



The Corporation of the City of St. Catharines
CITY COUNCIL AGENDA
Regular, Monday, January 28, 2019
Council Chambers, City Hall, 6:30 PM

His Worship Mayor Walter Sendzik takes the Chair and opens the meeting

Page

- 1. Mayor's Report**
- 2. Adoption of the Agendas**
- 3. Adoption of the Minutes (Council and General Committee)**
 - 3.1 Regular Meeting of Council, [January 14, 2019](#)
 - 3.2 General Committee Meeting of [January 14, 2019](#)
- 4. Declarations of Interest**
- 5. Public Meetings Pursuant to Planning Act and Notice By-law**

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- 5.1 Public Meeting Pursuant to Planning Act
Planning and Building Services, Planning Services
Planning Act Public Meeting – Information Report
Applications for Official Plan Amendment, Zoning By-law Amendment,
and Draft Plan of Condominium Subject Lands: 102 and 104 Lakeport
Road and 3 and 5 Avalon Place Owner: John Maletta and John Ciocca
Agent: Better Neighbourhoods Inc. (Dan Romanko)
*(At the meeting of November 12, 2018, this item was deferred the new
term of Council)*

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- 5.2 Public Meeting Pursuant to Planning Act
Planning and Building Services
Planning Act Continuation of Public Meeting - Recommendation Report
Application for Draft Plan of Vacant Land Condominium; 125 Rykert
Street; Owner: 2575115 Ontario Inc.; Agent: Better Neighbourhoods Inc.

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- 5.3 Public Meeting Pursuant to Planning Act
Planning and Building Services, Planning Services
Planning Act Continuation of Public Meeting - Recommendation Report:
Application to Amend Zoning By-law 2013-283 Subject Lands: 10
Benfield Drive Owner: Mountainview Belmont Ridley Heights Inc.
Additional Subject Lands: located opposite the intersection of Louth

Street and Crestcombe Road (no assigned municipal address) Owner:
Ministry of Transportation of Ontario Agent: Zelinka Priamo Ltd.

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5.4 Public Meeting Pursuant to Notice By-law

(Staff presentation to precede discussion)

Financial Management Services, Financial Management Services,
Director / City Treasurer; and Transportation and Environmental
Services, Director / City Engineer

2019 Capital Budget, Program Report and Four Year Forecast

6. Delegations

- 6.1 Colleen Beard, Lee Anne Losier, Barb Munroe, Natalie Millhiser, and
Robert Hazra

Re: 102-104 Lakeport Road and 3 & 5 Avalon Place

(see Regular Council Agenda, January 28, 2019, Item 5.1)

[Addenda]

- 6.2 Henry Ostaszewicz, Roberto Vergolito, Tanya Orba, Tom Skrzyszewski,
and Derek Kedzierski

Re: 2019 Capital Budget, Program Report and Four Year Forecast
(Facer Street Project)

(see Regular Council Agenda, January 28, 2019, Item 5.4)

7. Presentations

8. Motions

- 8.1 Port Pier Stakeholder Liaison Task Force

Councillor Williamson will present the following motion:

WHEREAS the Port Dalhousie piers are owned by the Government of
Canada, and managed through the Department of Fisheries and
Oceans (DFO) – Small Craft Harbours; and

*WHEREAS the DFO – Small Craft Harbours is responsible for
overseeing the repair of the Port Dalhousie piers and harbour assets;
and*

*WHEREAS on July 9, 2018, Council agreed to accept ownership of the
Port Dalhousie piers and harbour assets upon successful completion of
the rehabilitation of the assets and the successful negotiation of a
mutually acceptable divestiture agreement; and*

*WHEREAS on December 10, 2018, Council met in-camera and received
a verbal update of the project's status from the Deputy-CAO at which
time Council directed staff to work with DFO and the Contractor as part
of a liaison committee for the duration of the project up to, and including
the divestiture agreement; and*

THEREFORE BE IT RESOLVED that Councillor _____ be appointed to a Stakeholder Liaison Task Force on the Rehabilitation of Harbour Infrastructure.

[Addenda]

8.2 Budget Standing Committee Meeting Schedule

Councillor Siscoe will present the following motion:

That the Budget Standing Committee meeting scheduled for February 11, 2019, be cancelled; and

That a Budget Standing Committee meeting be scheduled for March 6, 2019, at 4:00 p.m.

[Addenda]

9. Call for Notices of Motion

10. Resolve into General Committee

11. Motion Arising from In-Camera Session

12. Motion to Ratify Forthwith Recommendations

13. By-laws

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13.1 Reading of By-Laws

14. Agencies, Boards, Committee Reports

14.1 Minutes to Receive:

- Recreation Master Plan Advisory Committee, [November 20, 2018](#)
- Heritage Permit Advisory Committee, [November 29, 2018](#)
- Green Advisory Committee, [December 12, 2018](#)

15. Adjournment



CITY OF
ST. CATHARINES

At the meeting of November 12, 2018, this item was deferred the new term of Council. **AGENDA ITEM #5.1**

Corporate Report

Report from Planning and Building Services, Planning Services

Date of Report: October 20, 2019

Date of Meeting: November 12, 2018

Report Number: PBS-235-2018

File: 60.30.334, 60.35.1033
and 60.46. 445

Subject: *Planning Act Public Meeting – Information Report*
Applications for Official Plan Amendment, Zoning By-law Amendment, and
Draft Plan of Condominium Subject Lands: 102 and 104 Lakeport Road and
3 and 5 Avalon Place
Owner: John Maletta and John Ciocca
Agent: Better Neighbourhoods Inc. (Dan Romanko)

Recommendation

That Council consider this Information Report as part of the Public Meeting; and,

That Council consider a Recommendation Report regarding the Applications for Official Plan Amendment and Zoning By-law Amendment and Draft Plan of Condominium for lands municipally known as 102 and 104 Lakeport Road and 3 and 5 Avalon Place at a future meeting of Council, pursuant to the two-step process for planning applications. FORTHWITH.

Summary

This Information Report provides Council and the public with information regarding the applications for Official Plan Amendment, Zoning By-law Amendment and Draft Plan of Condominium approval for 102 and 104 Lakeport Road and 3 and 5 Avalon Place, to be considered at a Public Meeting, in accordance with the Planning Act, tentatively scheduled for November 12, 2018.

A summary of the applicant's request is as follows.

5 Avalon Place (a portion of the site)

The lands are designated in the Official Plan and zoned for residential purposes. The proposal is to amend the Official Plan and change the zoning of the lands to permit the proposed residential/commercial mixed use building. Specifically, the applicant seeks to re-designate the lands from Low Density Residential to Community Commercial in the Official Plan and rezone the lands from Low Density Residential – Traditional Neighbourhood (R2) to Community Commercial (C2).

102 and 104 Lakeport Road and 3 Avalon Place (a portion of the site)

The lands are designated in the Official Plan and zoned for commercial purposes and permit the proposed residential/commercial mixed use building. No changes are proposed to the land use designation and zoning category. The existing Community Commercial designation and the Community Commercial (C2) zoning are to be retained.

102 and 104 Lakeport Road and 3 and 5 Avalon Place (the entire site)

The applicant is proposing that certain site specific zoning provisions apply to the entirety of the lands to allow the intended uses. The draft plan of condominium application applies to the entirety of the lands, for (ownership) tenure of the residential and commercial units.

These requests are intended to facilitate the construction of a mixed use building on the land, with apartment dwelling units and commercial space. The tenure of the premises is intended to be condominium ownership. Details of the proposed development and planning change requests will be set out in this report.

A Recommendation Report containing planning analysis of how the applications relate to Provincial, Regional and City land use policies, addressing any outstanding concerns raised at the Public Meeting, will be considered by Council at a future meeting.

Background

The subject lands are bounded by Lakeport Road, Avalon Place, Bernhardt Street, and Embassy Avenue. There are existing detached dwellings on 3 and 5 Avalon Place. 102 and 104 Lakeport Road are vacant (see appendix 1).

**Report
Proposal**

The applicant proposes to construct a six-storey mixed use building on the lands. Appendices 2 and 3 outline the proposed site plan and building elevations for the proposed residential/commercial building. The proposed mixed-use development will be located on the edge of a commercial node at Lakeshore and Lakeport Roads. The proposed building includes 63 apartment dwelling units and 100m² of commercial space on the ground floor. Conceptual plans submitted with the application show a total of 64 parking spaces. Forty-eight of those parking spaces are to be provided underground on two levels within the building with the remaining 16 spaces at grade outside of the building.

The proposed building is “L shaped” with the bulk of the massing oriented towards Lakeport Road and commercial uses to the east. The applicant has advised that it is their intention to sell the apartment dwellings and commercial space as condominium units. Draft plan of condominium approval also is being requested. Further application for site plan approval will be needed if the proposed amendments and draft plan of condominium are approved by Council.

The proposed preliminary site plan and elevations are attached to this report as Appendices 2 and 3, respectively.

Location and Site Description

The combined lands (102 and 104 Lakeport Road and 3 and 5 Avalon Place) have a total area of approximately 0.4 hectares with frontage on both Avalon Place and Lakeport Road. Specifically, the lands are located on the south side of Lakeport Road, east of Avalon Place, in the City's North Planning District. The lands have 67 metres of frontage on Avalon Place and 75 metres of frontage on Lakeport Road. A location map is attached as Appendix 1.

Surrounding land uses include:

North:	Detached dwellings commercial uses;
South:	Detached dwellings fronting onto Bernhardt Street;
East:	Commercial uses;
West:	Detached dwellings fronting onto Avalon Place and townhouses to the west of Avalon Place.

Planning Policy Context

Provincial planning legislation requires Council's decision on a planning application to be consistent with the Provincial Policy Statement (2014), and must conform to Provincial plans, upper-tier official plans and lower-tier official plans. Accordingly, planning staff will evaluate these Applications for Official Plan Amendment and Zoning By-law Amendment against the policies of the Provincial Policy Statement, the Provincial Growth Plan for the Greater Golden Horseshoe (2017), the Regional Official Plan, and the Garden City Plan, all of which apply to this application: and staff recommendations will be formulated accordingly.

For Council's reference, the land use planning framework in Ontario, as it relates to these applications, is outlined in Appendix 4.

Provincial Policy Context

The subject lands are located within a Settlement Area, as defined by the Provincial Policy Statement (2014) and within the Built-up Area as identified by the Provincial Growth Plan for the Greater Golden Horseshoe (2017). These documents contain policies that support all forms of residential intensification and urban area regeneration, directing growth to Built-up Areas. Provincial policies place an emphasis on intensification and infill to foster the development of complete communities which efficiently use land, resources, infrastructure and public service facilities.

Provincial Policy Statement (2014)

The following provides an overview of the Provincial Policy Statement (PPS) themes applicable to these applications for Official Plan Amendment and Zoning By-law Amendment and Draft Plan of Condominium.

- Accommodate an appropriate range and mix of residential, employment (industrial and commercial), institutional (including places of worship), recreation, park and open space, and other uses (including commercial) to meet long-term needs;
- Settlement areas are to be the focus of growth and development, and their vitality and regeneration is to be promoted;
- Establish a land use plan and pattern that supports financial sustainability and cost effective development and provision of services;
- new development should have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities;
- Promote the wise use and management of land and resources;
- Support an accessible, connected and complete community;
- Establish appropriate development standards to promote/facilitate intensification, redevelopment and compact built form;
- Improve accessibility for persons with disabilities and older persons by identifying, preventing and removing land use barriers which restrict their full participation in society
- Ensure protection of natural features and areas for the long term

Relevant PPS policies will be considered and analyzed in the future Planning Recommendation Report.

Provincial Growth Plan for the Greater Golden Horseshoe (2017)

Similarly, guiding principles within the Growth Plan stress compact and well-designed development that prioritizes intensification. The Plan supports the achievement of complete communities that ensure the development of high quality compact built form, an attractive and vibrant public realm, including public open spaces, through site design and urban design standards. Policies ensure that a range and mix of housing options are available including affordable housing.

The following provides an overview of the Provincial Growth Plan themes applicable to these applications for Official Plan and Zoning By-law Amendment.

- Promote compact built form and transit supportive development, a diversity and mix of uses and opportunities, and the efficient use of land, infrastructure and service capacities to support development, redevelopment and growth;
- Foster safe, interactive, active transportation and connected communities;
- Ensure viability for long term development and community investment;

- Maintain, enhance vitality of main streets, and conservation of neighbourhoods;
- Encourage sense of place by promoting well designed built form and cultural planning to conserve features that help define the character, built heritage resources and cultural heritage landscapes of the area;
- Adapt to climate change through compact and nodal development;
- Promote high quality built form, attractive/vibrant public realm through site design and urban design standards;
- Support complete communities by ensuring that lands are adequately zoned and that development is designed to meet this policy;
- Support multi modal transportation, including active transportation and transit

Relevant Growth Plan policies will be considered and analyzed in the future Recommendation Report.

Regional Official Plan

The subject lands are located within the St. Catharines Urban Area, as defined by the Regional Official Plan (ROP). The St. Catharines Urban area is considered as a Settlement Area by the 2014 Provincial Policy Statement (PPS).

The ROP, 2014 PPS and 2017 Growth Plan for the Greater Golden Horseshoe (Growth Plan) together direct development to take place in urban areas and delineated built-up areas to support intensified development and to build upon existing servicing and infrastructure. Both Regional and Provincial policy place an emphasis on intensification and infill to foster the development of complete communities that have mix of diverse land uses and housing choices, improved social equity and quality of life, new and expanded access to multiple forms of transportation, and the provision of spaces that are vibrant and resilient in their design.

Given the site specific nature of the application, the Official Plan amendment is exempt from Regional approval in accordance with the Memorandum of Understanding with Area Municipalities, and Regional Official Plan Policy 14.E7.

It should be noted that the Region of Niagara is undertaking a comprehensive review (MCR) of their Official Plan to bring it into conformity with the PPS and the Provincial land use plans. Once completed, local area municipalities will be required to update their Official Plans accordingly.

Official Plan (Garden City Plan)

The Official Plan (Schedule D1, Appendix 5) designates the lands at 5 Avalon Place as Neighbourhood Residential and the lands at 102 and 104 Lakeport Road and 3 Avalon Place as Commercial. The North Planning District of the Plan (Schedule E1, Appendix 6) further designates the lands at 5 Avalon Place as Low Density Residential and the lands at 102 and 104 Lakeport Road and 5 Avalon Place as Community Commercial.

The Low Density Residential Designation of the Plan provides for a range of residential uses including detached, semi-detached, duplex, triplex, quadruplex, fourplex and townhouse dwellings at a density range generally between 20 and 32 units per hectare of land. Height of buildings will generally not exceed 11 metres. Apartment dwelling units are not allowed.

The Community Commercial designation of the Plan (Section 9.2.2 (a)) provides for a range of commercial uses, excluding auto dealerships, adult oriented uses and hotels. Other uses permitted include institutional, civic, cultural, indoor recreation, entertainment and residential apartment dwellings units.

The Official Plan identifies the lands generally bounded around the intersection of Lakeport Road and Lakeshore Road as being designated Community Commercial. 102 and 104 Lakeport Road and 3 Avalon Place are currently within that designation. The City's previous Official Plan (approved by the Region of Niagara on March 12, 1999) similarly designated these lands for commercial purposes. As such, 102 and 104 Lakeport Road and 3 Avalon Place have long established permissions for commercial/apartment uses with no height limit and do not represent a new or freestanding Community Commercial Centre designation.

Community Commercial Centres are to be located at the intersection of arterial roads and well served by public transit. They are generally between 2 and 12 hectares in size and shall not exceed a total gross leaseable floor area of 30,000 square metres for all uses except residential, and non-retail/service commercial uses should be limited in size and scale to protect the primary function of the centre for the provision of shopping and service commercial facilities. The size of individual uses shall be limited in a manner that supports a diversity and mix of uses in the Centre. Residential uses shall be located in freestanding buildings or in upper storeys of commercial buildings, providing adequate outdoor amenity space for residents (Section 9.2.2 (b and c)).

The proposal is to re-designate 5 Avalon Place to Community Commercial (Appendix 7) and to merge with the remainder of the site which is already designated Community Commercial in the Official Plan.

The subject lands also form part of a Mixed Use Intensification 'Special Study Area' which is intended to "establish an appropriate range of transit supportive medium and higher density housing opportunities, with a mix of commercial, employment, institutional and recreational uses" (15.1.1.d). While special detailed study has not yet been undertaken, the GCP does acknowledge that this corridor is anticipated to transition into more of a higher density mixed use corridor.

The Plan (9.2.2 (d)) provides that an application to establish a new centre or to expand an established centre beyond existing property limits will require an amendment to the Plan, and may be subject to studies as set out in Part F, Section 16.16 of the Plan including but not limited to, market analysis, land use compatibility, and traffic (see Appendix 10 to this report).

Section 16.1 of the GCP details specific matters that must be evaluated when considering applications of an Official Plan Amendment. Every application for amendment is to be evaluated on the basis of the following considerations:

- i) “the degree of conformity of the proposed amendment to the general intent, purpose and philosophy of the Plan, particularly the vision, planning principles and general policies of the Plan;
- ii) consistency with Provincial and upper tier government plans, policies and legislation;
- iii) the availability and suitability of land already designated for the proposed use, and the need for, and the feasibility of the proposed use;
- iv) the compatibility of the proposal, or the adequacy of proposed mechanisms for achieving compatibility, with adjacent and planned uses;
- v) the potential of the proposal to cause instability within an area intended to remain stable;
- vi) the ability of the City’s infrastructure and public service facilities to accommodate the proposal without costly expansion, upgrading, or required deferral of other planned infrastructure and public service facility improvements in other areas of the City;
- vii) the financial implications, both costs and revenues, to the City; and
- viii) the degree to which approval of the amendment would establish an undesirable precedent”.

Relevant Official Plan policies will be addressed and analyzed in the future Recommendation Report.

All relevant City Official Plan policies that apply to this Application have been excerpted and included in the table attached as Appendix 10.

Zoning By-law

Zoning By-law 2013-283 zones the lands at 5 Avalon Place Low Density Residential – Traditional Neighbourhood (R2). The R2 Zone permits a broad range of residential uses; however, apartment dwellings are not permitted in this zone. The lands at 102, 104 Lakeport Road and 3 Avalon Place are zoned Community Commercial (C2) The C2 zone permits a broad range of commercial uses including apartment dwelling units in association with a commercial use (see Appendix 8). The proposal is to change the zoning for 5 Avalon Place to the Community Commercial (C2) zoning to enable these lands to be developed as proposed (Appendix 9).

The applicant is also requesting Site Specific C2 Provisions applicable to the entirety of the lands (ie.102 and 104 Lakeport Road and 3 and 5 Avalon Place) as follows:

Provision	Required	Proposed
Minimum exterior side yard setback from building to Lakeport Road	3 metres	1.4 metres
Minimum exterior side yard setback from balconies to Lakeport Road	3 metres	0.4 metres
Minimum rear yard setback of balconies to commercial uses to the east	4.5 metres	4.1 metres
Minimum landscape buffer abutting a public road – Avalon Place	3 metres	2.1 metres

Affordable Housing

In June 2017, Council approved Phase I of the City's Housing Action Plan in recognition of rising market values, a low vacancy rate, and a low supply of affordable housing in the City. Currently, Staff are in the process of implementing priority recommendations that were outlined in the Housing Action Plan. While the City's Official Plan recognizes the need to provide for affordable housing, actual mechanisms to technically require and sustain an affordable housing supply in the City as per the definition in the Provincial Policy Statement is part of the ongoing work in implementing the Action Plan. This includes consideration for Inclusionary Zoning which is a land use planning tool that a municipality may use to require affordable housing units to be included in proposed residential developments of 10 units or more. This tool is typically used to create affordable housing for low and moderate income households. In Ontario, this means families and individuals in the lowest 60 per cent of the income distribution for the regional market area, as defined in the Provincial Policy Statement (2014). Generally, housing is considered "affordable" when households do not pay more than 30 per cent of gross income on annual accommodation costs, or when the purchase price for housing is at least 10 per cent below average market value or when households do not pay more than 30 per cent of gross annual household income for rent or when the rent is at or below the average market rent.

In accordance with Provincial legislation, in order to implement inclusionary zoning, a municipality is required to undertake a series of steps including an assessment report to understand local housing supply and demand, as well as potential impacts which might arise from the inclusionary zoning requirement and then implement official plan policies and pass a zoning by-law guiding where and how Inclusionary Zoning will be implemented. Once implemented, the municipality is responsible for monitoring the affordable housing units and reporting outcomes.

This study requires considerable detail and, to date, as far as Staff are aware, no Ontario municipality has fully studied or implemented this tool to require such availability in the approval of development applications. The work to undertake this study and implement the mechanism is subject to discussion and approval of the department's

draft Operating Budget for 2019. Given Council budget guidelines, provision is unable to be made to undertake this work in 2019 because of the lack of staff and financial resources.

The applicant has not indicated that any of the proposed units meet the definition of affordable housing as defined by Provincial policy.

Draft Plan of Condominium

The proposed Draft Plan of Condominium is attached as Appendix 11. Section 51(24) of the Planning Act directs the matters to be considered in recommending approval for a Draft Plan of Condominium, include:

- The development does not have a detrimental effect on matters of provincial interest, it is not premature and is in the public interest.
- The proposal complies with the Official Plan, and the lands are suitable for development as proposed.
- The suitability of the land for residential purposes.
- Adequacy of any public streets and how they are designed and how they link with adjacent streets.
- The adequacy of the proposed size and shape of lots and blocks.
- Any restrictions on the subject lands or adjoining lands.
- Flood control measures, if any.
- Adequacy of existing utilities and municipal services.
- Adequacy of school sites in the vicinity.
- Any land considered for dedication for public purposes.
- Any considerations regarding the subdivision of the land and matters relating to site plan approval.

An analysis of the proposed Draft Plan of Condominium, as well as any recommended conditions of draft plan approval will be provided in the Recommendation Report.

Circulation of Applications

The applications were circulated to all relevant departments and agencies in accordance with the Planning Act. No department or agency has objected to the proposed Official Plan and Zoning By-law Amendments and Draft Plan of Condominium. The following feedback has been received:

Region of Niagara

The proposed development has frontage along Lakeport Road (Regional Road 87). A road right-of-way widening will be required to be dedicated to the Region through the site plan process should these applications be approved. The applicant has incorporated the Region's requirement in this regard into the proposed site plan (Appendix 2). Given the site specific nature of the application, the Official Plan Amendment is exempt from Regional approval in accordance with the Memorandum of Understanding with Area Municipalities, and Regional Official Plan Policy 14.E7. Region of Niagara requirements (e.g. archaeology, road right-of-way dedications, storm water

management, encroachments, and waste collection) will be appropriately dealt with as part of the future site plan approvals process.

Parks, Recreation and Culture Services (PRCS)

Landscape strips should be maintained at a minimum of 3.0m. A future application for Site Plan Agreement shall include necessary tree protection measures to ensure the preservation of healthy trees, where possible. Additional boulevard trees shall be required along Avalon Place through the Site Plan Agreement.

Development Engineering - Planning and Building Services

As part of the submission requirements for these applications the proponent has provided a servicing report concerning capacity of existing infrastructure to support the proposed development and any requirements that might be generated to support it. Staff has reviewed the submission and is satisfied that the local infrastructure can accommodate the proposed development. The proponent will be required to convey to the City a road right-of-way widening of 1.52m across the entire frontage of the subject property along Avalon Place. A sidewalk will be required along the Avalon Place frontage.

Traffic

Traffic staff has no objections or comments concerning these applications.

Site Plan

An application for Site Plan Approval will be required to be submitted should these applications be approved. The City's Site Plan Control By-law requires that mixed use developments (in this case residential and commercial) be subject to site plan control. Site plan approval regulates the placement of buildings, architectural elevations of the buildings, parking, grading, drainage and storm water management, landscaping, fencing and similar details of site design.

The conceptual site plan (see Appendix 2) shows the siting of the building, access, landscaping, etc. Conceptual elevations and renderings are shown in Appendix 3.

An additional open house relating to the detailed design of the site will be hosted by City staff as part of the site plan process should these applications be approved.

Public Consultation

A public open house was hosted by Planning and Building Services on October 3, 2018. The purpose of the open house was to present the applicant's proposal and provide an opportunity for attendees to ask questions and provide comments before staff recommendations are presented to Council for consideration. There were approximately 45 attendees, including the ward councilors, owner, and his agent. Matters raised at the Open House are summarized below, with staff responses accordingly.

Public submissions have been received and are attached as Appendix 12.

Official Plan compliance

Comment: Will the proposal be evaluated with regard to Official Plan policies?

Response: The future Recommendation Report will provide an analysis of the proposal based on the Official Plan (Garden City Plan (GCP)), together with provincial policies (Provincial Policy Plan and Growth Plan), and the Regional Official Plan. See relevant Official Plan policies in Appendix 10 to this report.

Avalon Place status

Comment: Is Avalon Place a collector road or an arterial road?

Response: Avalon Place is a local road.

5 Avalon Place uses

Comment: Can semi-detached dwellings be built on 5 Avalon Place? Further, can townhouses be constructed on the entire lands under consideration?

Response: The Official Plan and Zoning By-law currently permit semi-detached dwellings at 5 Avalon Place. Town houses are not currently a permitted use on the remainder of the lands. The proposal will be evaluated based on the applications and proposal presented for consideration.

Condominium unit sales/configuration/construction

Comment: What amount of pre-sale is required for the condominium to proceed? How high will interior ceilings be? How long will construction take if the proposal is approved?

Response: The owner's agent addressed these queries. Depending on the requirements of the financial institution who are financing the project, typically 50% pre-sale of units is required for development to obtain financing. The interior ceiling heights of the condominium units will be approximately 3 metres. The estimated time frame for construction is approximately one year.

Parking

Comment: How is parking being provided for?

Response: The Zoning By-law requires one parking space per dwelling unit plus 5 parking spaces (at the rate of one space per 20 m²) for the commercial use. The by-law waives the first four required commercial parking spaces in mixed uses developments. As such, one parking space is required for the commercial component of the proposed development and 63 spaces are required for the residential component. The rationale is that typically commercial uses demand parking during daytime business hours when residential demand for parking is the lowest.

Conceptual plans submitted with the application show a total of 64 parking spaces. Forty-eight of those parking spaces are to be provided underground on two levels within the building with the remaining 16 spaces at grade outside of the building.

Traffic

Comment: The proposed development will result in increased traffic flows and will compromise safety.

Response: Lakeport Road (Regional Road 87) is a Regional road of Niagara. Avalon Place is a local road and is under the jurisdiction of the City of St. Catharines. Circulation of these applications did not generate traffic related concerns or requirements for a traffic impact study (TIS). Increased traffic flows can be expected should the development be approved and constructed. Surrounding roads can accommodate this development.

Adequacy of infrastructure

Comment: Are there adequate services (i.e. sewer (storm/sanitary) and water) available to accommodate the proposed development?

Response: The proponent has submitted a servicing report as part of the application requirements. Staff has reviewed the submission and is satisfied that the local infrastructure can accommodate the proposed development.

Impact of construction on adjacent residences

Comment: The construction of the proposed building will result in damage to adjacent residences.

Response: Prior to any development or site works beginning the applicant is required to conduct preconstruction assessments of neighbouring properties when there is any chance that damage may occur. It is in the Owner's interest and diligence to protect their interests against potential claims.

Compatibility/Overdevelopment

Comment: The proposed development is too much/too high for the site and as it relates to the neighbourhood.

Response: The Official Plan (Garden City Plan) contains policies/criteria that address compatibility and scale of development, as do applicable senior government level plans (Provincial Policy Statement, Places to Grow, and the Region of Niagara Official Plan). The Recommendation Report will provide analysis of these matters in accordance with these policy plans.

It should be noted that the majority of the site already permits the proposed use. The applications request the expansion of the development site to include the additional

lands at 5 Avalon Place, which is a residential lot with a frontage of 16.8 metres and depth of 27.4 metres.

Loss of privacy

Comment: The proposed development will result in the loss of privacy. Has there been a shadow study done?

Response: A shadow study has been submitted in support of the application. The study indicates that shadow effects will be limited primarily to the season(s) or parts thereof in which amenity areas are generally not in use. Should these applications be approved, the site plan approval process will provide an opportunity to address privacy issues through a number of means (eg. landscaping and fencing etc). Council has directed that staff host a public open house upon submission of site plan applications. As such residents will be invited to view more detailed plans and provide input in the future.

Location of Property – Community Commercial Centre

Comment: The property is 0.4 hectares in size. The Official Plan provides that the size of a Community Commercial Centre shall generally range from 2 hectares to 12 hectares of land.

Response: The Official Plan identifies the lands generally bounded around the intersection of Lakeport Road and Lakeshore Road as being designated Community Commercial. 102 and 104 Lakeport Road and 3 Avalon Place are currently within that designation. The overall size of the lands identified as being within this Community Commercial designation is approximately 3.7 hectares.

The City's previous Official Plan (approved by the Region of Niagara on March 12, 1999) similarly designated these lands for commercial purposes. As such, 102 and 104 Lakeport Road and 3 Avalon Place have long established permissions for commercial/apartment uses with no height limit and do not represent a new or freestanding Community Commercial Centre designation.

The proposal is to add 5 Avalon Place, with an area of 460 square metres to this designation.

Further public submissions received on November 2, 2018, are attached as Appendix 13.

Public Notice

In accordance with established procedures, notice for the public meeting has been circulated.

Financial Implications

All costs associated with this development will be borne by the developer. No direct financial impacts to the City are triggered by these approvals. If approved and constructed, additional tax revenues would be generated by the development.

Relationship to Strategic Plan

The relationship of the proposal to the City's Strategic Plan will be evaluated as part of the future recommendation report on this matter.

Conclusion

In summary, this report provides the planning context and explains the applications for Official Plan Amendment, Zoning By-law Amendment, and Draft Plan of Condominium for 102, 104 Lakeport Road and 3 and 5 Avalon Place. The amendments would facilitate land use designation and zoning for 5 Avalon Place that is consistent with the rest of the site (102, 104 Lakeport Road and 3 Avalon Place). The Zoning By-law Amendment also proposes Special Provisions applicable to the entirety of the lands as outlined in this report. A Recommendation Report will be presented to Council at a future meeting, to be scheduled.

Notification

It is in order to advise Dan Romanko, Better Neighbourhoods Development Consultants, 190A Ontario Street, St. Catharines, Ontario, L2R 5K9, the Owner's agent.

Prepared by:

Kevin Blozowski, MCIP, RPP, CAHP
Heritage Planner

Submitted by:

Judy Pihach, MCIP, RPP
Manager of Planning Services

Approved by:

James N. Riddell, M.Pl, MCIP, RPP
Director of Planning and Building Services

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Aerial Photo Surrounding Area



Niagara Regional Air Photo (April, 2015)

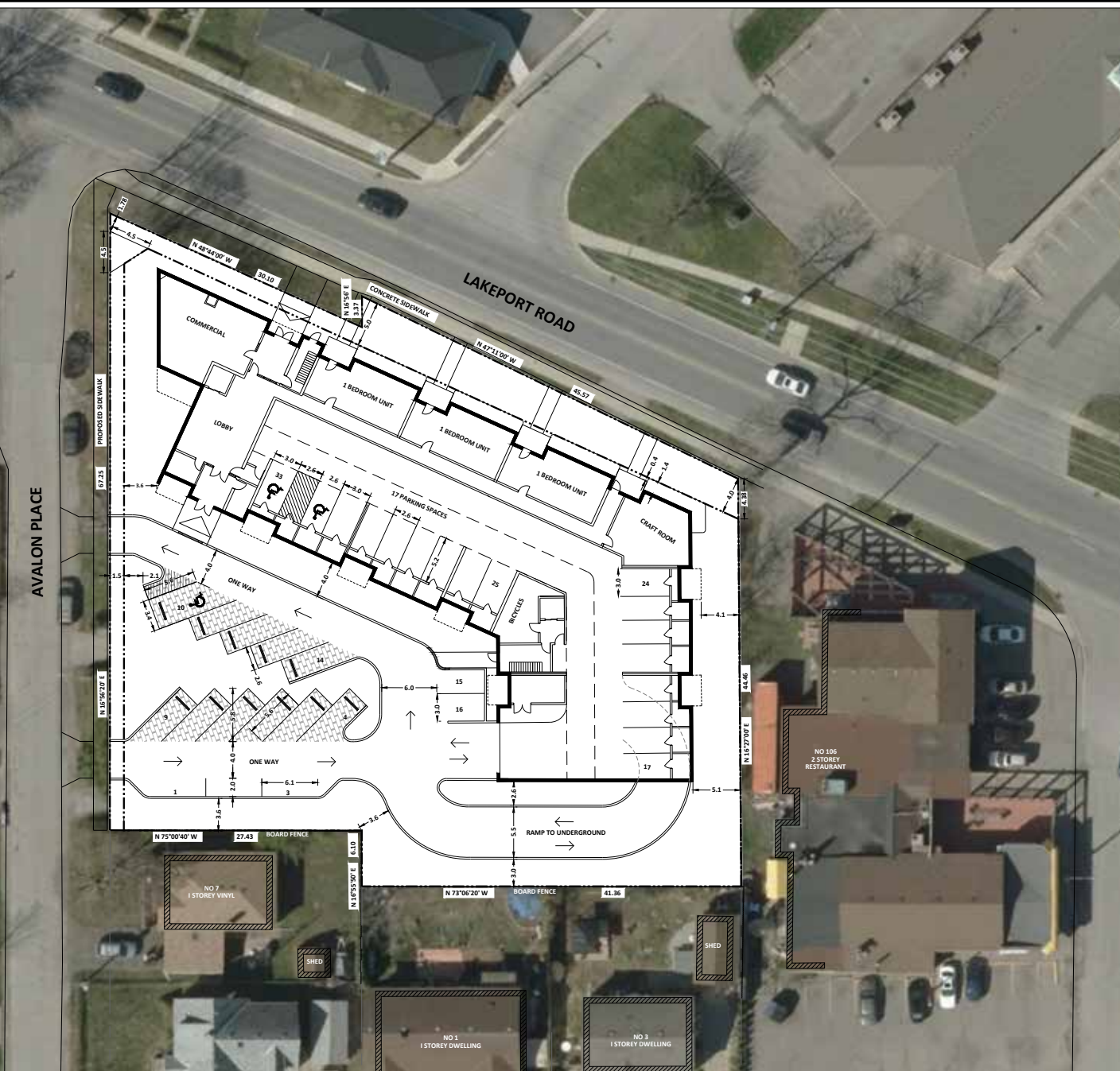


102 & 104 Lakeport Road; 3 Avalon Place



5 Avalon Place

File: 60.30.334 & 60.35.1033





NORTH ELEVATION (LAKEPORT RD)



WEST ELEVATION (AVALON PLACE)



EAST ELEVATION



SOUTH ELEVATION



REVISIONS	
NO.	DESCRIPTION

LAKEPORT CONDOMINIUM APARTMENTS
104 LAKEPORT RD.
ST. CATHARINES, ONTARIO

Parker Architects Inc.

156 St. Paul Street
St. Catharines, ON, L2R 9M2
tel: 905-687-6931
email: info@parkerarchitects.ca

A3.1

drawing	BUILDING ELEVATIONS
drawn	DTP
checked	-
scale	1 : 200
NL 16-110	date JUNE 11, 2011



VIEW FROM CORNER OF LAKEPORT RD. & AVALON PLACE



VIEW FROM AVALON PLACE AT LAKEPORT RD. INTERSECTION



REVISIONS	
NO.	DESCRIPTION

LAKEPORT CONDOMINIUM APARTMENTS
104 LAKEPORT RD.
ST. CATHARINES, ONTARIO

Parker Architects Inc.
156 St. Paul Street
St. Catharines, ON, L2R 9M2
tel: 905-687-6851
email: info@parkerarchitects.ca

A4.1	
drawing	3D IMAGES
drawn	DTP
checked	-
scale	1 : 200
NL 16-110	date JUNE 1, 2017



VIEW FROM AVALON PLACE



VIEW LOOKING NORTH ON LAKEPORT RD.



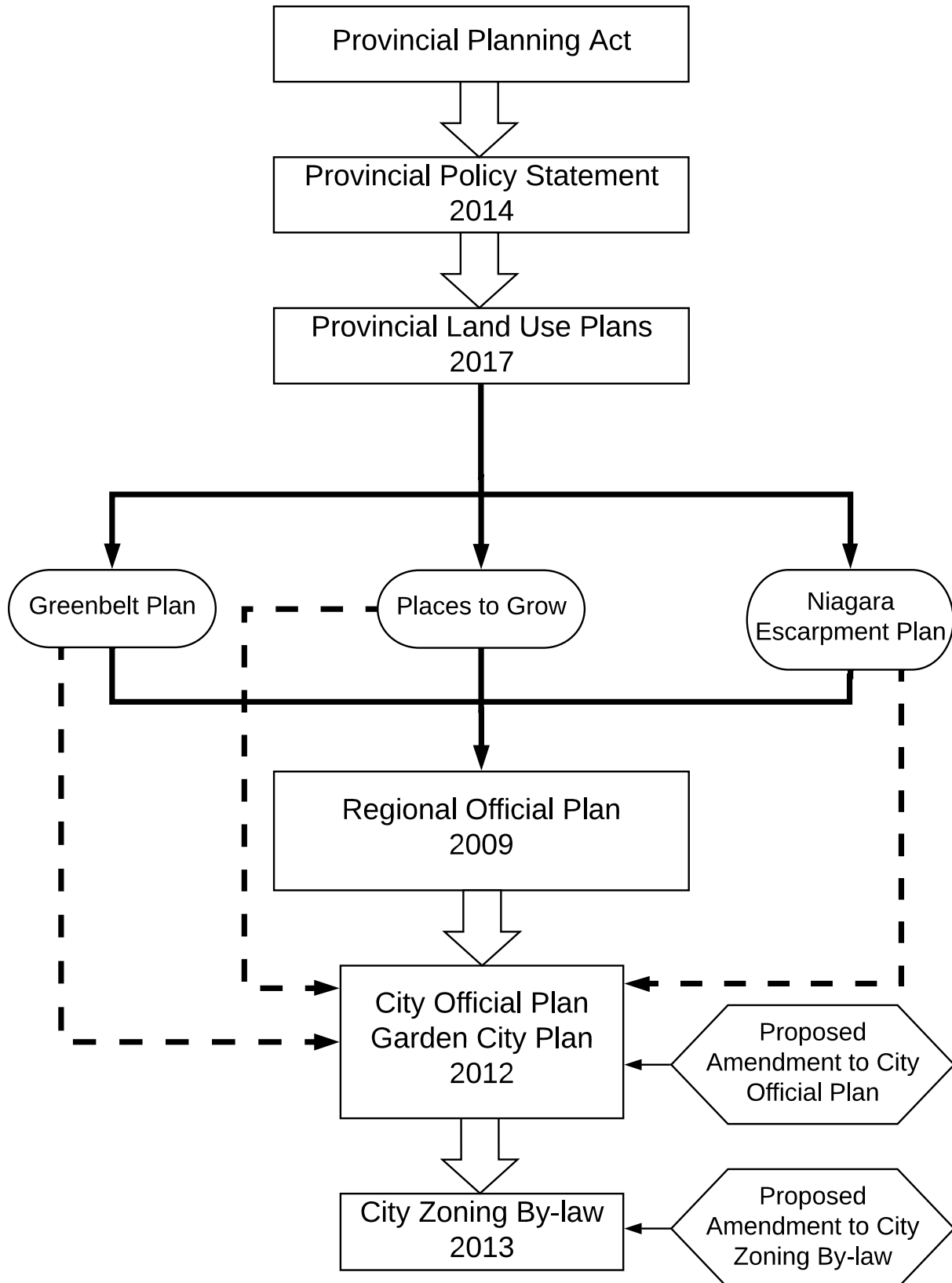
REVISIONS	
NO.	DESCRIPTION

LAKEPORT CONDOMINIUM APARTMENTS
104 LAKEPORT RD.
ST. CATHARINES, ONTARIO

Parker Architects Inc.
156 St. Paul Street
St. Catharines, ON, L2R 9M2
tel: 905-687-6951
email: info@parkerarchitects.ca

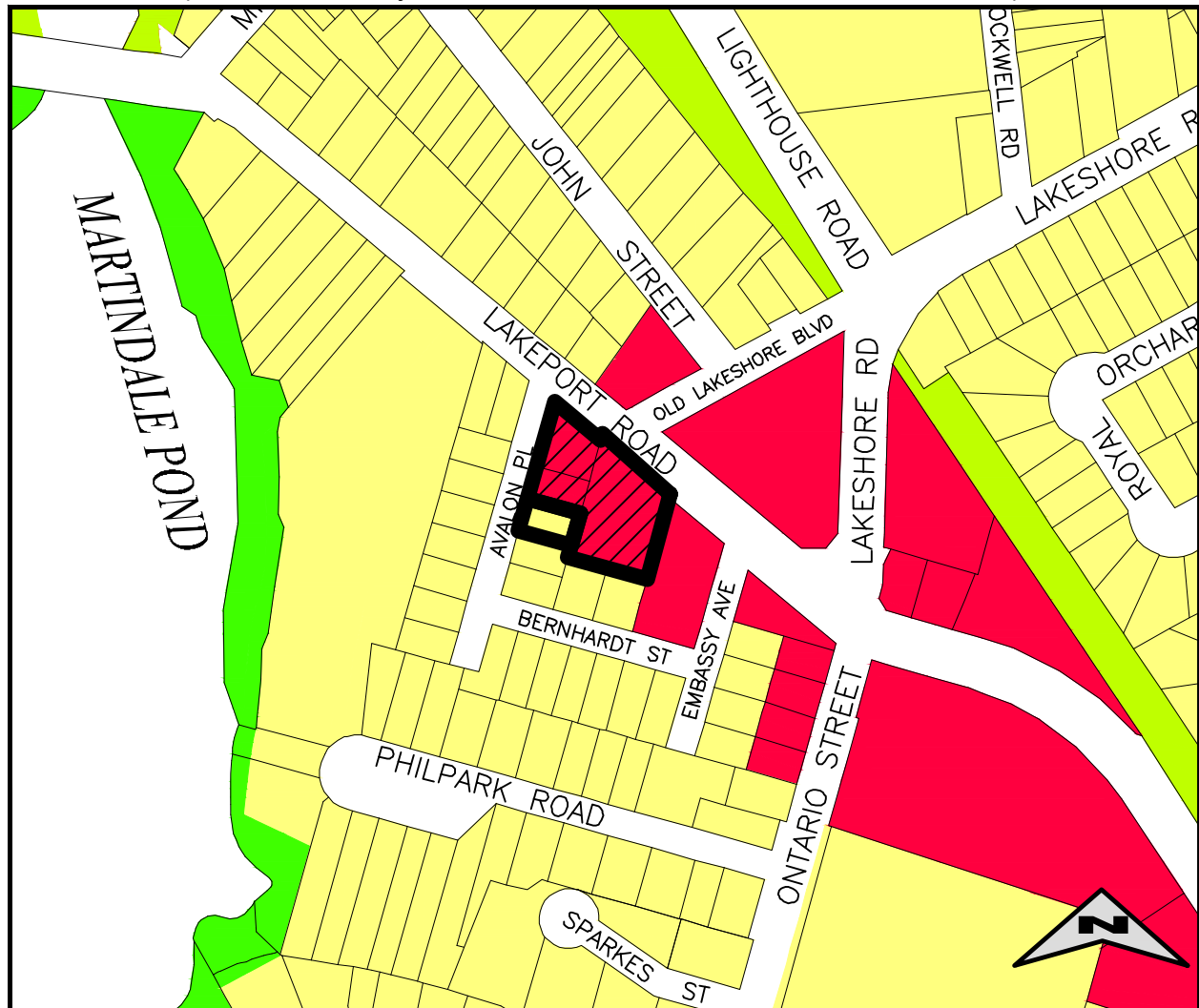
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NL 16-110	date JUNE 1, 2017

Ontario Land Use Planning Framework




Existing Land Use Designation

(The Garden City Plan - General Land Use Plan, Schedule D1)





 Area A (102 & 104 Lakeport Road; 3 Avalon Place)

 Area B (5 Avalon Place)

Land Use Designations

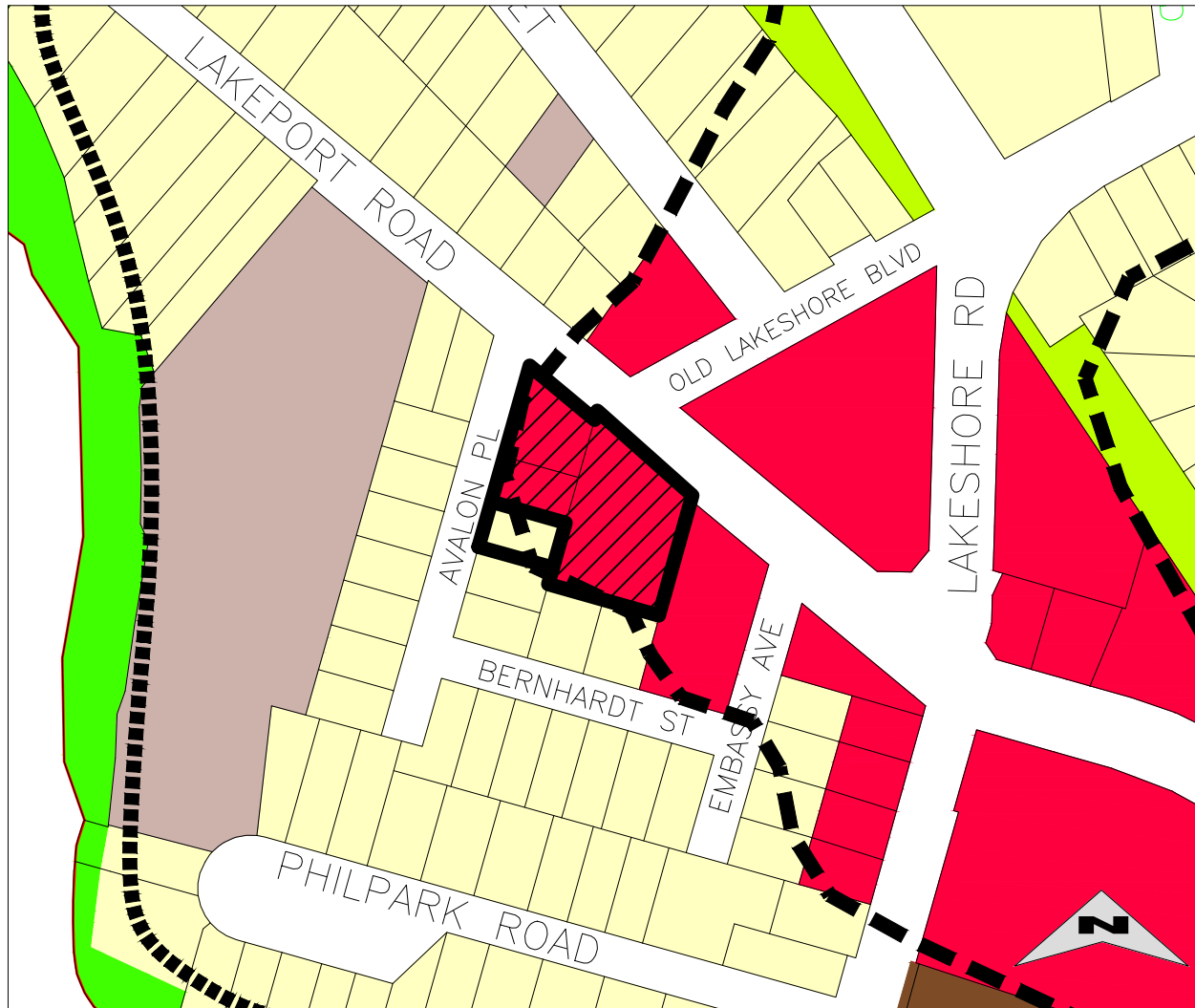
 Neighbourhood Residential
 Commercial

 Natural Areas
 Parkland & Open Space


Files: 60.30.334 & 60.35.1033

Existing Land Use Designation

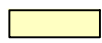



(The Garden City Plan - North Planning District, Schedule E1)



 Area A (102 & 104 Lakeport Road; 3 Avalon Place)

 Area B (5 Avalon Place) - Lands to be re-designated from Low Density Residential to Community Commercial

Land Use Designations

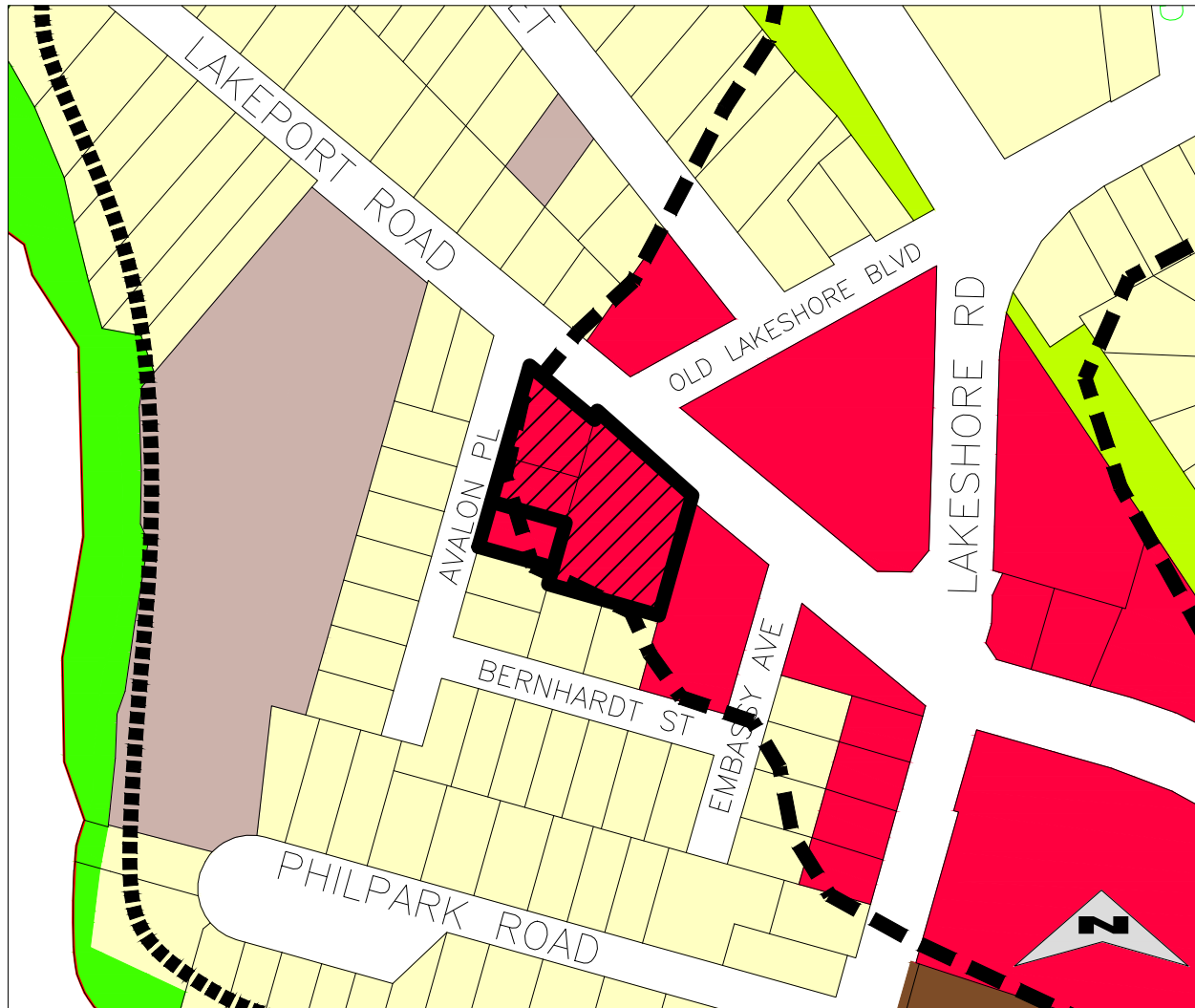
 Low Density Residential
 Medium Density Residential
 High Density Residential
 Community Commercial

 Natural Areas
 Parkland & Open Space
 Natural Area Extent Line
 Special Study Area


File: 60.30.334 & 60.35.1033

Proposed Official Plan Amendment









(The Garden City Plan - North Planning District, Schedule E1)



 Area A (102 & 104 Lakeport Road; 3 Avalon Place)

 Area B (5 Avalon Place) - Lands to be re-designated from Low Density Residential to Community Commercial

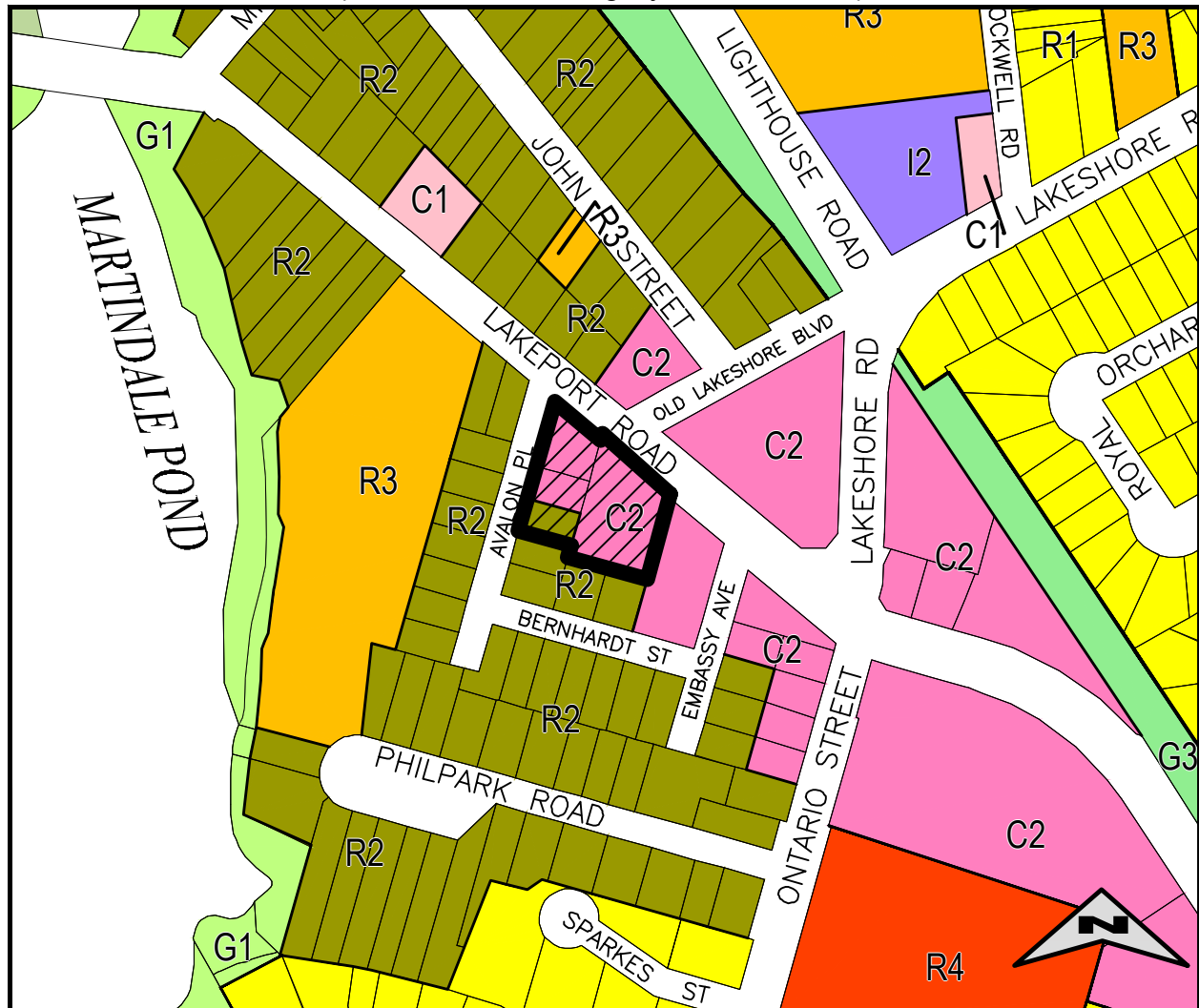
Land Use Designations

	Low Density Residential		Natural Areas
	Medium Density Residential		Parkland & Open Space
	High Density Residential		Natural Area Extent Line
	Community Commercial		Special Study Area

File: 60.30.334 & 60.35.1033

Existing Zoning

(Schedule A - Zoning By-law 2013-283)



Subject Lands

102 & 104 Lakeport Road; 3 & 5 Avalon Place

Zones

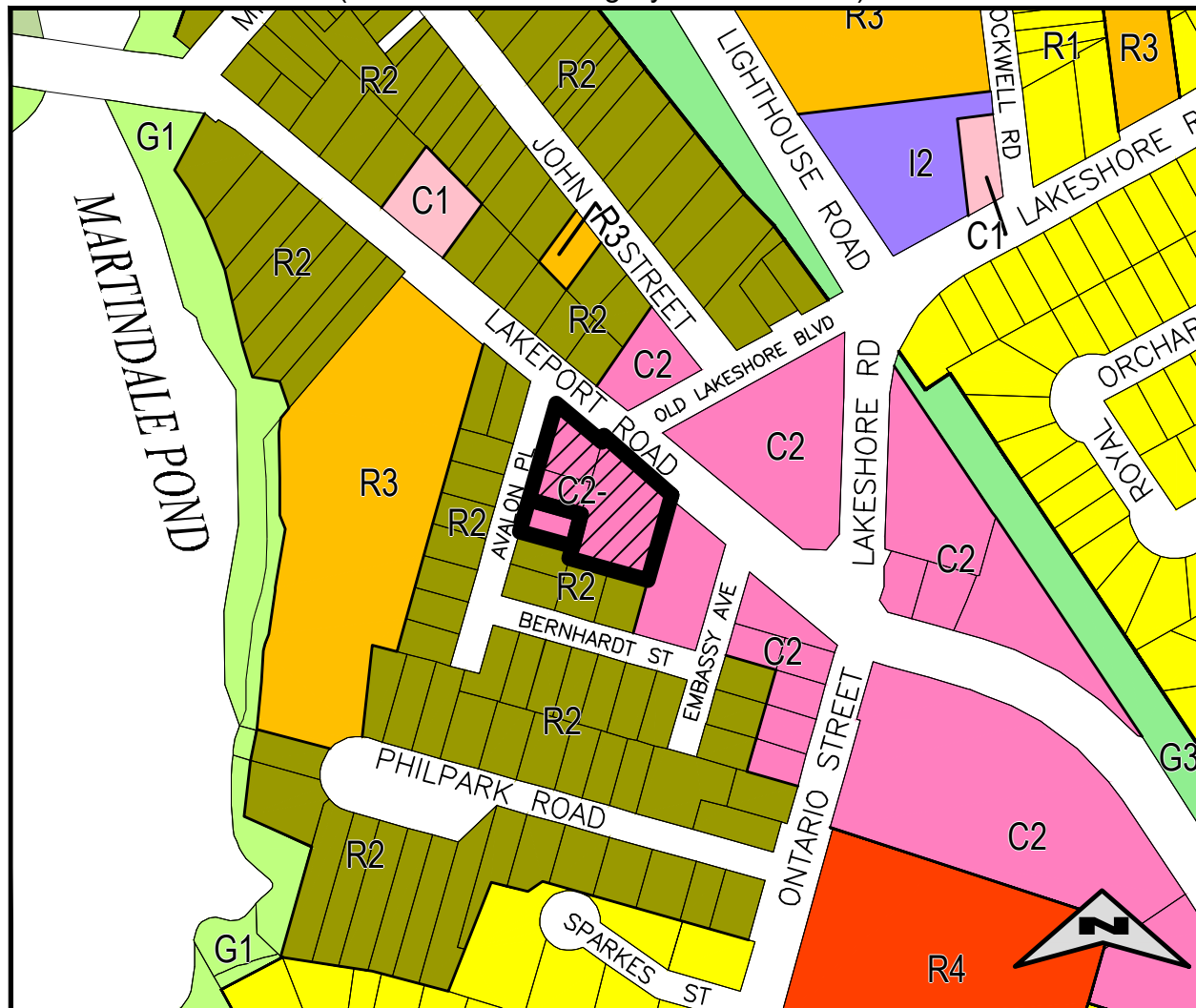
- R1** Low Density Residential
- Suburban Neighbourhood
- R2** Low Density Residential
- Traditional Neighbourhood
- R3** Medium Density Residential
- R4** High Density Residential

- C1** Local Convenience Commercial
- C2** Community Commercial
- G1** Conservation / Natural Area
- G2** Minor Green Space
- G3** Major Green Space
- I2** Community Institutional

Files: 60.30.334 & 60.35.1033


Proposed Amendment to Zoning By-Law 2013-283

(Schedule A - Zoning By-law 2013-283)







 Area A (102 & 104 Lakeport Road; 3 Avalon Place)

Lands to be rezoned from Community Commercial (C2) to
Community Commercial with Special Provision No. (C2-)

 Area B (5 Avalon Place)

Lands to be rezoned from Low Density Residential - Traditional Neighbourhood (R2) to
Community Commercial with Special Provision No. (C2-)

Zones

-  R1 Low Density Residential
- Suburban Neighbourhood
-  R2 Low Density Residential
- Traditional Neighbourhood
-  R3 Medium Density Residential
-  R4 High Density Residential

-  C1 Local Convenience Commercial
-  C2 Community Commercial
-  G1 Conservation / Natural Area
-  G2 Minor Green Space
-  G3 Major Green Space
-  I2 Community Institutional

Files: 60.30.334 & 60.35.1033

Applicable Official Plan Policies
City of St. Catharines Garden City Plan

Policies
PART B: VISION AND GUIDING PRINCIPLES
2.3. Managing Growth and Change
2.3.1 Municipal Planning Structure
3. Planning Areas
The City is divided into two primary planning areas – the Urban Area and the Agriculture Area.
i) Urban Area
The Urban Area is established by the Region of Niagara Policy Plan and is set out on Schedule D ‘Municipal Structure’ of this Plan. This area is intended to provide urban development opportunities on full municipal services to accommodate the majority of the City’s projected future population and employment growth.
2.3.3. Accommodation of Growth
1. Projected population, housing and employment will primarily be accommodated within the Urban Area as defined on Schedule D ‘Municipal Structure’.
2. The Plan recognizes a finite Urban Area, and within it, a diminishing vacant land supply and a finite occupied land base to accommodate projected population and employment growth.
3. Growth will be accommodated by:
i) the efficient usability of vacant and occupied lands;
ii) more compact built form and density of development;
iii) the reuse, rehabilitation, regeneration, intensification and enhancement of the built environment;
iv) redevelopment and build out of underutilized properties
v) a more integrated, interactive mix of uses, activity and functions;
vi) service, infrastructure, energy, transportation sustainable, accessible, efficient and supportive development;
vii) design initiatives to support:
<ul style="list-style-type: none">• compatible, innovative, efficient and sustainable building and site design;• enhanced natural and cultural heritage protection, preservation and conservation;• greening;• an accessible, integrated, interactive, usable and connected public realm within and between different use and activity areas, functions, landscapes and identities;
viii) recognizing the preferred mix and interaction of uses contemplated, the City’s Urban Design Guidelines shall be emphasized in the evaluation of all development, redevelopment and intensification opportunities;
ix) the upgrading of existing infrastructure and public service facilities, and where required, the provision of new infrastructure and public service facilities to ensure adequate service capacity to support development, redevelopment and intensification.
x) providing a balance of housing and employment opportunities to maximize the opportunities for people to live and work in St. Catharines, and to maintain a sustainable healthy tax base to support a balanced and complete community, and sustainable long term growth and development opportunities.
4. This Plan establishes a land use planning framework consistent with and supporting the growth and intensification strategy and the achievement of minimum density targets set out in the Provincial Growth Plan for the Greater Golden Horseshoe, 2006, (referenced as the Provincial Growth Plan) as follows:
i) Built Boundary – the Provincial Growth Plan establishes a Built Boundary which represents the limits of the developed Urban Area of the municipality, as defined by the Ministry of Infrastructure Renewal. The Built Boundary for St. Catharines is defined on Schedule D ‘Municipal Structure’ of this Plan.
Intensification Areas are set out on Schedule D ‘Municipal Structure’, and are to be planned to support:
i) minimum housing densities that are generally higher than surrounding areas together with a range and mix of commercial, employment, institutional, recreational and cultural uses primarily intended to serve the local area neighbourhood;

- ii) context sensitive building, site and streetscape design to support an accessible, connected, interactive and vibrant public realm, and compatible and appropriate transition of built form with adjacent areas;
- iii) public transit service and active transportation networks and opportunities.

Specific parameters for development and redevelopment within Intensification Areas are set out in Part E ‘District Plans’.

vii) General Intensification – notwithstanding Intensification Areas set out in Part B, Section 2.3.3.4 vi) above, this Plan also recognizes and provides opportunities for housing intensification within the Urban Area, as follows:

- on all Local Neighbourhood Convenience Commercial, Major Commercial and Community Commercial Centres in a manner not to detract from the intended primary commercial use of such Centres as set out in Part D, Sections 8.3.3, 9.2.1 and 9.2.2 of this Plan.
- on all designated Residential and Mixed Use properties subject to the policies and density standards set out in Part D, Section 8 and 12 of this Plan.

5. Housing

- i) If past trends towards development of primarily low density housing were to continue, the vacant residential land supply is not adequate to accommodate all of projected future population and housing needs. To accommodate projected future growth requires evolution from traditional development patterns and trends.
- ii) This Plan emphasizes the provision of new housing through the efficient use of vacant and occupied lands; higher density of development; small lot infill; intensification; mixed use development; and reuse, rehabilitation and redevelopment of the built form, including brownfield and greyfield sites.
- iii) All types of new housing will be provided in a manner not to compromise, but to accentuate and enhance the character, structure, function and accessibility of established residential neighbourhoods and other activity areas; cultural and heritage landscapes, buildings and identities; and the City’s network of parks, open space and natural areas, and public realm opportunities.
- iv) All types of new housing will be encouraged to support housing diversity for a range of life stages, special needs, incomes and ages to promote social inclusion, reduce isolation, and promote affordable housing opportunities.
- v) This Plan establishes higher housing density standards and opportunities for a greater mix of housing types than has traditionally been directed by the municipality. While it is recognized that new housing development and intensification may occur in all residential neighbourhoods, the primary emphasis for housing intensification is directed towards the Urban Growth Centre and Intensification Areas as set out on Schedule D ‘Municipal Structure’, along major road corridors, at commercial centres and mixed use areas. The range and scale of housing permitted is further defined in Part D ‘Land Use Policies’ and in Part E ‘District Plans’.

6. Employment

- i) The land use plan provides opportunities for a diverse range, mix, scale and location of economic activities to accommodate projected employment growth forecasts.

To accommodate projected employment growth forecasts, and support long term sustainable employment opportunities, economic development and competitiveness, this Plan promotes and emphasizes:

- the provision of an adequate, available, suitable, variable and competitive choice of vacant employment lands to attract and accommodate a diverse range of new economic activities.
- reuse, rehabilitation, redevelopment and enhancement of occupied employment lands to accommodate new or expanded economic activity and employment opportunities;
- transit supportive, public realm, design and greening initiatives to provide compatible, efficient, functional, connected, accessible, interactive, environment friendly and sustainable development.
- clustering of like activities to support compatible development and encourage integrated, interactive and connected business and employment opportunities.

- iv) Population serving commercial, institutional, recreation, and cultural uses generate significant employment opportunities outside the Urban Growth Centre and Employment Lands.

- ii) This Plan will ensure opportunities for commercial, institutional, recreational and cultural activities to help build complete communities adequately serve and support local resident needs and employment opportunities, and to support and attract tourist related activity and employment.

v) Work Live accommodation and home based business are encouraged to enhance accessible and transit supportive business and employment opportunities.
Work Live accommodation will generally be directed to the Urban Growth Centre, Intensification Areas and mixed use areas, and is most desirable to be developed on a larger scale to support business or research clusters with shared support services.

PART C: GENERAL POLICIES

4. URBAN DESIGN

4.1. Urban Design Principles

Good urban design is a key planning tool to create sustainable communities. Sustainable design involves the holistic design of communities and buildings for long-term economic prosperity, social harmony and stability, minimized environmental impact, and strengthened cultural identity.

The development of St. Catharines will be based on the following sustainable design principles:

- a) a strong sense of identity and place as the “Garden City”;
- b) innovative and sustainable design;
- c) a stimulating, attractive, and safe public realm;
- d) compatibility of new development with established areas;
- e) universal access to public areas;
- f) conservation of heritage buildings and structures;
- g) protection of the natural environment and processes;
- h) a compact, walkable, bikeable and well connected community;
- i) a range of choices for housing and transportation ;
- j) mixed-use nodes and corridors;
- k) complete streets for all users;
- l) building design to support street life.

4.3. The Natural Environment

St. Catharines is located between Lake Ontario and the Niagara Escarpment, with numerous creek systems and environmentally sensitive natural areas. In the urbanized areas, the relationship to these natural features and areas will be enhanced through a thoughtful approach to their conservation, visibility, and accessibility.

1. The natural features and areas of the City will be connected to the surrounding environment by improving physical and visual access from adjacent public spaces and by designing these to create connectivity and enhance a comprehensive, interconnected and safe open space network.

2. The enjoyment of the valleys and ravines will be protected and enhanced by ensuring that adjacent development, particularly from building height and massing, will protect for views and vistas.

3. Development and redevelopment activities will be designed to be compatible in scale and character with natural features and areas, and enhance the visual qualities and public enjoyment of those features and areas.

4. Site design and planning will consider the existing topography and the preservation and enhancement of vegetation, natural features and areas, open space, and naturalized areas. This includes:

- a) Ensuring that existing naturalized open space on lands proposed for development/redevelopment is retained to the maximum extent possible, and where retained, is allowed to regenerate with minimum intervention;
- b) Enhancing ecological stability by supporting the use of low maintenance landscape features and materials;
- c) Supporting the use of native plant species when creating new plant communities or when adding to existing native plant communities;
- d) Applying development standards designed to maximize retention of all woodlots and other natural features and areas;

5. Where they remain, the pleasant tree-lined streets of the older areas will be protected and where trees have to be removed, they will be replaced as soon as possible.

6. A program of tree planting, preservation, and landscaping will be undertaken so that all areas are provided with trees and other vegetation to maintain a high standard of amenity and appearance, with specific emphasis given to the Urban Growth Centre and Intensification Areas, as set out on Schedule D ‘Municipal Structure’, at the time of infrastructure renewal and reinvestment.

7. In all public works, trees should be retained and when trees must be lost to accommodate the works, they will be replaced as soon as possible by other trees of sufficient maturity and in sufficient numbers to enhance the appearance of the public works.

8. Where development or redevelopment may necessitate the loss of existing trees or vegetative planting on a public right-of-way, they will be replaced and relocated on the public right-of-way in the immediate vicinity of the affected lands, to the satisfaction of the City or the Region of Niagara, and at the cost of the proponent.

4.4. The Public Realm

The public realm – the streets, parks, squares, public buildings, and open spaces, is a critical component of city life, drawing people together and providing the setting for festivals, cultural events, and recreational and leisure activities, as well as for daily casual contact. They create strong social bonds and help establish our public identity.

1. Urban design opportunities to enhance the quality of the public realm shall be encouraged as part of the design of all municipal undertakings, including public parks and buildings, public streets, natural areas, and all municipal engineering projects related to public spaces. The design of such projects will consider:

- a) the introduction or improvement of linkages where existing areas are inadequately connected to each other or between public places;
- b) improvements to streetscapes, including pedestrian-scale lighting, bicycle facilities, street furniture, signage, landscaping, and enhanced active transportation wherever road improvements occur;
- c) naturalization opportunities including the use of native species of trees in development of open spaces;
- d) the placement of a broad range of art works in publicly accessible and visible locations;
- e) roadway design to maximize the preservation and maintenance of natural features and the development of complete streets;
- f) the protection and enhancement of major vistas, gateways, and scenic routes;
- g) measures to mitigate light trespass and glare to minimize the effects of sky glow.

2. Public buildings will be located and designed to promote their public status on prominent, accessible sites. Public space associated with public buildings will be designed to accommodate a variety of public functions.

3. Where feasible, major public and private development projects will be required to incorporate public spaces, bicycle facilities and sidewalks to support connections to the City’s pedestrian, bike and transit network.

4. Wherever feasible, utilities will be placed underground and/or designed to minimize negative impacts, maintain existing area character, and enable further aesthetic improvements such as boulevard trees planting.

Consideration shall be given to the location of utilities within the public rights of way as well as on private property. Utilities shall be clustered or grouped where possible to minimize visual impact. The City encourages utility providers to consider innovative methods of containing utility services on or within streetscape features such as gateways, lamp posts, transit shelters, etc. when determining appropriate locations for large utility equipment and utility cluster sites.

5. The City’s Urban Design Guidelines shall identify certain locations as design priority areas given their importance as growth areas, major activity areas, or prominent focal points. These areas will include:

- a) Downtown and the Urban Growth Centre
- b) Intensification Areas as set on Schedule D ‘Municipal Structure’.
- c) City and neighbourhood nodes, gateways, and traditional main streets as identified in the City’s Urban Design Guidelines.
- d) major activity centres and mixed use areas.
- e) major vistas, landscapes

6. Design features for the design priority areas identified in Part C, Section 4.4.5 above will be established through the City’s Urban Design Guidelines and may include but are not limited to decorative lighting, public art, distinct signage, wider sidewalks, boulevard and median treatment and planting, traffic calming, entrance features, and enhanced pedestrian surfaces to support the achievement of complete streets.

4.5. Built Form

Built form refers to the arrangement of buildings and their relationship to each other, and to the natural environment. It plays a large role in defining the character of an area. New

development in St. Catharines will primarily be by way of intensification and redevelopment, and urban design will have a significant impact on how the new development will fit with established areas. Compatibility of development does not necessarily mean the same as or similar to existing buildings in the vicinity, but rather that new development respects and enhances the existing character and context of an area. Where there is no discernable character, new development will set the standard for future development.

1. As a basis for evaluating compatibility and for achieving design excellence, development/redevelopment will be designed in a manner that reflects the principles established in Part C, Section 4.1 of this Plan and that maximizes compatibility with the surrounding area in terms of the following matters:

- a) Building scale, height, gradation of height, and massing.
- b) Spacing of buildings.
- c) The level and visibility of the ground floor relative to exterior grade
- d) Roof form and pitch, together with any other structures on the roof.
- e) The placement, number, type and proportion of doors and windows.
- f) Use of materials, textures, and colours.
- g) Continuity in the provision, location, and linkage of amenity, open space, and green corridors.
- h) Preservation and enhancement of significant views and vistas.
- i) Retention of natural vegetation and other distinctive landscape or streetscape features.
- j) The overall scale of the development as it relates to the surrounding area. In this regard, factors contributing to compatibility include avoiding long unbroken expanses of walls; creating relief in walls; the use of varied colours, textures, types, qualities and patterns of finish materials; roofline articulation.
- k) Mitigating light trespass and glare and to minimize the effects of sky glow.
- l) Mitigating wind, sun shadow impacts.

2. Development/redevelopment will locate parking, service areas, and utilities to minimize the impact on the property, surrounding area, and the environment by:

- a) Consolidating and minimizing the width of driveways and curb cuts.
- b) Providing underground parking or parking structures where possible.
- c) Limiting surface parking between the front and flanking face of a building and the public street.
- d) Providing perimeter landscaping as well as landscaped interior islands.
- e) Using permeable pavement systems or other low impact development practices.
- f) Connecting parking areas to the street through safe, landscaped pedestrian walkways.
- g) Integrating utilities as part of the building and site design, where feasible.
- h) Ensuring safety for active transportation.

3. Development/Redevelopment may be required to provide amenities for adjacent streets and open spaces such as street furniture, bicycle parking facilities, trees, signage, and lighting to ensure they are fully integrated into the surrounding neighbourhood.

4. Major development/redevelopment proposals shall be required to prepare an urban design scheme (site plans, building elevations, context plans, and other studies) to determine impacts on the physical compatibility of the surrounding area and on the long term development or redevelopment potential of the area.

4.6. Universal Access

Universal access means providing a broad range of users with varying needs the opportunity to move through the City independently with as few obstacles, physical and psychological, as possible.

1. New buildings, the retrofitting of existing municipal buildings and public spaces shall be designed to foster and support accessibility by all citizens. The full range of abilities of all ages and users shall be considered in the design of the built environment.

2. The travelled portion of pedestrian and bicycle routes shall be kept free from obstructions or projections of street furniture, fixtures, or buildings.

3. Barrier-free features shall be well-integrated within the functional and aesthetic design composition, including but not limited to:

- i) ensuring appropriate driveway and sidewalk height to length ratios to support safe accessibility and travel for all;
- ii) requiring sidewalk curb cuts at all intersections;
- iii) requiring paving changes be incorporated at grade changes and intersections;
- iv) requirements for accessibility parking spaces;

v) encouraging other features appropriate to ensure that barrier free design is provided.

The City may prepare specific design guidelines for accessibility to ensure that accessible design initiatives are considered in the evaluation of development and redevelopment proposals.

4.7. Personal Safety

Design, through sensitive site layout, location of public spaces and road design, can have a significant influence on the safety of an environment based on the “eyes on the street” philosophy.

- 1. Publicly accessible spaces shall be located near public roads, transit stops, or other public spaces in order to allow public surveillance.
- 2. Adequate lighting shall be provided in concealed areas or those areas where visibility may be poor.
- 3. Views shall be provided into, out of, and through publicly accessible spaces.
- 4. Access points from public and public accessible spaces shall be clearly identified.
- 5. Public spaces shall be designed to encourage a variety of activities on a daily, weekly, and seasonal basis.

4.8. Landscaping

Landscaping has a number of significant roles to play in creating interesting and functional places.

- 1. Landscaping design shall:
 - a) enhance the visual appeal of development.
 - b) buffer unsightly areas or uses that are incompatible.
 - c) promote the use of native species.
 - d) provide seasonal variation in form, colour, texture, and representation.
 - e) promote human scale and create an attractive environment for pedestrian movement.
 - f) assist in energy conservation.
 - g) mitigate the effects of inclement weather.
 - h) preserve and compliment existing natural landscape.
- 2. Appropriate buffering will be required between all uses of land where there may be a conflict and where one use may detract from the enjoyment.

6. ACHIEVING A SUSTAINABLE CITY

6.1. General Policies

- 1. The City shall develop an Integrated Community Sustainability Plan that will provide guidance for the development or alignment of all municipal plans, policies and decisions under one integrated decision-making framework.
- 2. The City shall support sustainable development through land use and development patterns that:
 - a) Promote compact form and a structure of nodes and corridors, and which directs development, redevelopment and intensification primarily to the Downtown Urban Growth Centre and Intensification Areas as set out on Schedule D ‘Municipal Structure’.
 - b) Promote the use of public transit and other alternative transportation modes among residential, employment and commercial areas, and other activity centres;
 - c) Promote development/redevelopment throughout the City’s Urban Area on sites that are or will be well served by public transit, with particular emphasis on the Downtown Urban Growth Centre and Intensification Areas as set on Schedule D ‘Municipal Structure’.
 - d) Promote design and orientation that maximizes the use of alternative or renewable energy sources, such as solar and wind, and the mitigating effects of vegetation.
- 3. All of the City’s sustainable development principles shall be considered when reviewing applications for development, public works, and capital expenditures.
- 6. In making decisions about planning, development, and conservation, within the sustainability framework, the City shall employ an ecosystem approach

addressing:

- a) The interrelationships among air, land, water, plant and animal life, and human activities;
- b) The health and integrity of the broader landscape, including impacts on the natural environment in neighbouring jurisdictions;
- c) The long term and cumulative impacts on the ecosystem.

7. Development should maintain, enhance, or restore ecosystem health and integrity.

8. The City will collaborate with Niagara Region, the Niagara Peninsula Conservation Authority and other appropriate stakeholders, in developing and maintaining an environmental data base and monitoring program to assess ecosystem health and integrity and recommend improvements, where necessary, to this Plan.

6.2. Corporate Policies

1. The City shall require all new major municipal buildings to achieve a LEED silver rating or equivalent, where feasible.

2. The City shall establish an environmental sustainability policy and related procedures such as a sustainable green purchasing procedure and a sustainable green fleet procedure.

3. The City shall require all municipal renovation and retrofitting, including the adaptive reuse of heritage buildings for public use, to incorporate sustainable building techniques wherever feasible.

4. The City shall consider the installation of green or light reflecting roofs in the feasibility and design phase of all new municipal buildings.

5. The City shall consider the installation of solar panels, geothermal heating/cooling systems and other sustainable energy practices in the feasibility and design phase of all new municipal buildings.

6.3. Energy Efficiency/Generation

3. The City shall promote innovative subdivision and site plan design that minimizes energy consumption through road design and lot layouts which maximize passive solar energy opportunities and other alternative energy sources.

6.5. Water Resources

1. Development and site alteration shall only be permitted if it will not have negative impacts, including cross-jurisdictional and cross-watershed impacts on:

- a) The quantity and quality of surface and ground water;
- b) The functions of ground water recharge and discharge areas, aquifers and headwaters;
- c) The natural hydrologic characteristics of water courses such as base flow;
- d) The natural drainage systems, streams, forms and shorelines;
- e) Flooding or erosion;
- f) Surface or ground water resources adversely impacting on the ecological function of natural hazards and heritage as set on Schedule F2 'Natural Hazard Lands' and Schedules F3 and F4 'Natural Heritage' of this Plan.

2. Development and site alteration shall be restricted in the vicinity of vulnerable surface and ground water features of importance to water supplies so that the safety and quality of drinking water will be protected and improved.

4. A Stormwater Management Plan prepared by a qualified engineer, and based on sediment and erosion control guidelines from the Niagara Peninsula Conservation Authority (NPCA), as amended from time to time, may be required with an application for development, redevelopment or site alteration depending on:

- a) The scale and nature of the proposal.
 - b) Site specific environmental conditions.
- The City, in conjunction with the Region, the Niagara Peninsula Conservation Authority (NPCA) and other authorities will determine the need for such a plan as part of the complete application process. Stormwater Management Plans will be required to consider alternative approaches to traditional stormwater management such as Low Impact Development practices as noted in Part D, Section 7.1 g) of this Plan.

6.6. The Urban Forest

4. Planning applications shall integrate natural features and natural vegetation,

<p>including the planting of native species, into development plans.</p> <p>5. The City shall ensure that appropriate space for tree protection and tree planting within road rights-of-way are included in the design of new roads and road improvements.</p>
<p>PART D: LAND USE POLICIES</p> <p>7. GENERAL POLICIES</p> <p>7.1 Development / Redevelopment Development and redevelopment within the Urban Area shall be evaluated having regard for the following:</p> <p>a) Support for safe, accessible and connected active transportation linkages within and between residential neighbourhoods, other activity centres, uses, and the City's public realm and open space network.</p> <p>b) Opportunities to create or enhance public places, active transportation linkages, facilities and gateways.</p> <p>c) Building, site and streetscape context sensitive design to ensure:</p> <p> i) Integration of compatible building form, scale, massing, height, setbacks, spacing, siting, orientation, facades and architectural materials with adjacent buildings, properties and the surrounding neighbourhood;</p> <p> ii) Adverse impacts on adjacent properties are minimized in regard to grading, drainage, location and design of service utilities and areas, access and circulation, parking, transition in height, privacy, views, vistas, microclimatic conditions, and protection of the natural features, functions and hazards.</p> <p> iii) Possible negative impacts such as noise, odour, and emissions are not excessive in relation to the predominant land use character and function of the area;</p> <p> iv) Adequacy of lot size, access, on-site facilities and outdoor amenity areas to accommodate use;</p> <p> v) Provision of parking areas that do not dominate the site physically or visually, and maximize opportunities for perimeter and internal landscaping.</p> <p>d) The preservation, conservation, enhancement and integration of natural and cultural heritage features, landscapes and identities.</p> <p>e) Opportunities to support and improve connectivity and accessibility to public transit and active transportation, including carpooling and carsharing.</p> <p>f) Treatment of existing topographic and vegetative features, and greening and landscaping opportunities.</p> <p>g) The best use of existing resources, infrastructure and service capacity. Development/redevelopment will only be permitted where there is adequate municipal water and wastewater service, waste management, and public service facilities. Compatible and practical on-site renewable water, waste and energy management techniques and design are encouraged, including Low Impact Development (LID) practices.</p> <p>h) Surface parking areas should be minimized and shared parking areas, standards and access shall be encouraged where compatible, and adequate and appropriate for intended level of service. Parking areas and access should be designed to optimize greening opportunities, and to support reduction of heat islands, sustainable storm water management, limited access points to public roads, and provide for safe and friendly active transportation circulation.</p> <p>i) Development will only be permitted where the sustainable transportation system is adequate to accommodate anticipated traffic volumes, generation, circulation and use.</p> <p>j) Land assembly and configuration will not detract from the potential development or redevelopment on adjacent properties, or create isolated parcels which may otherwise have future development or redevelopment potential.</p> <p>k) Alternative and innovative lottage patterns are supported provided that compatible street, building and site context sensitive design with adjacent properties and the surrounding neighbourhood can be achieved; and adequate and maintained access to a public road is provided and ensured.</p> <p>l) Direct vehicular access from individual properties to the Welland Canals Parkway is not permitted.</p> <p>7.2 Contaminated Sites</p> <p>ii) On all lands known or suspected of potential environmental hazards, the City shall require, prior to consideration of approval for development /redevelopment or site alteration, appropriate studies to assess potential hazards. Protective or mitigating measures may be applied to the subject lands, subject to study evaluation and Provincial standards. A record of site condition may be required.</p>

- iii) On all lands subject to Official Plan or Zoning By-law amendment or Subdivision approval, an environmental site assessment from the proponent will be required as a condition of approval, identifying that the site has been investigated and found to be free from contamination subject to Provincial standards. A record of site condition may be required.
Where hazards are identified, they shall be subject to appropriate studies, and protective or mitigating measures may be applied.
- iv) The City may, in accordance with City procedure, require for all lands conveyed or dedicated to the City, or purchased by the City, the submission of documentation from a qualified professional indicating the lands have been investigated for contaminants and are in accordance with Provisional standards for the intended use. A record of site condition may be required.
- v) All environmental and legal requirements, including but not limited to soil or water studies, audits, and satisfactory mitigating and engineering measures, are the sole responsibility of the developer.

7.6 Special Study Areas – Notwithstanding the general land use policies of this Plan, certain lands may be set out as a ‘Special Study Area’ in accordance with Part F, Section 16.20. A ‘Special Study Area’ is subject to future study by the City or by development proponents to determine and implement appropriate land use designations and/or policies to guide future development, redevelopment or intensification in the affected area(s). Special Study Areas are set out in Part E, District Plans. District Plans may also set out the general vision and parameters as the basis of future study. Until such time as revised land use designations and/or policies have been implemented by amendment to this Plan, the underlying land use designations and applicable policies established in this Plan will apply.

8. NEIGHBOURHOOD RESIDENTIAL

8.1. Residential Use Designations

1. Low Density

The Low Density residential designation permits detached, semi-detached, duplex, triplex, quadruplex, fourplex and townhouse dwellings at a density range generally between 20 and 32 units per hectare of land. Height of buildings will generally not exceed 11 metres.

2. Medium Density

The Medium Density residential designation permits detached, semi-detached, duplex, triplex, quadruplex, fourplex, townhouse and apartment dwellings at a density range generally between 25 and 99 units per hectare of land. Height of buildings will generally not exceed 20 metres.

3. High Density

The High Density residential designation permits triplex, quadruplex, fourplex, townhouse and apartment dwellings at a density generally 85 units per hectare of land or greater.

8.2. General Policies

1. Residential development, redevelopment and intensification will be integrated within Low, Medium and High Density residential designations having regard for Part D, Section 7 and all other policies of this Plan.

2. New Medium and High Density development sites may be permitted in the Neighbourhood Residential designation by way of zoning by-law amendment, provided:

- a) the permission represents only a change from low to medium density residential, or medium to high density residential, as defined in Part D, Section 8.1 above;
- b) the subject lands are located on arterial or collector roads, in close walkable proximity and accessibility to commercial centres, community facilities and parks, are well served by public transit and should most appropriately be located adjacent to or in close proximity to existing medium or high density residential sites;

Any proposal for new medium or high density residential development will be evaluated having regard for urban design principles and policies set out in this Plan to ensure building, site and streetscape design will support compatible and context sensitive development with adjacent properties and those in close proximity to the subject lands; and, may be subject to studies as set out in Part F, Section 16.16 of this Plan.

Through regular monitoring of this Plan, the subject lands will be appropriately designated in Part E 'District Plans', and subject to the applicable policies established for that designation.

3. Apartments are permitted on arterial roads in low density residential designations, subject to zoning by-law amendment, having regard for the density and height provisions established in Section 8.1.1. Any such proposal will be evaluated having regard for urban design principles and policies set out in this Plan to ensure building, site and streetscape design will support compatible and context sensitive development with adjacent properties and those in close proximity to the subject lands, and may be subject to studies as set out in Part F, Section 16.16 of this Plan.

4. New residential development may be subject to Provincial guidelines to ensure adequate minimum distance separation from designated employment lands.

8.3 Ancillary Uses

The Neighbourhood Residential designation also permits elementary and secondary schools, places of worship and other local public serving institutional uses, neighbourhood commercial uses, home based business, parks and open space uses.

1. Parkland and open space uses are permitted by way of zoning by-law amendment where they are compatible with the residential environment.

2. Elementary and Secondary Schools, places of worship and other local public serving institutional uses are permitted subject to zoning by-law amendment where:

- a. Compatible with the surrounding residential environment, and where potential negative impacts such as noise, odours, emissions, litter, and traffic generation are not excessive in relation to the predominant use;
- b. Located in areas appropriate and accessible for the intended level of service; In close walkable and bikeable proximity to public transit service;
- c. The transportation system is adequate to accommodate anticipated traffic volumes and circulation.

i) Low, medium or high density residential standards are set out in Part E 'District Plans' to establish residential development parameters on existing school and place of worship sites where redevelopment for residential purposes may be proposed. Medium density residential standards will generally apply to those properties located on arterial or collector roads.

ii) Policy 8.3.2 i) above should not be construed as the City supporting the redevelopment or relocation of existing elementary and secondary school sites. This Plan supports the retention of existing elementary and secondary school sites recognizing the integral role of these schools in providing essential and accessible community and neighbourhood facilities and resource, and in providing neighbourhood focus and identity.

3. a) Local Neighbourhood Convenience Commercial Centres cater to the day to day convenience needs of nearby local residents and may include small scale retail and service commercial, office, institutional and indoor recreation uses, and residential dwelling units. New Local Neighbourhood Convenience Commercial Centres, or the expansion of existing Centres onto adjacent properties may be permitted subject to zoning by-law amendment and will be evaluated based on the following criteria:

- i) compatible with surrounding residential environment;
- ii) total site size of the centre should not exceed 0.4 hectares of land;
- iii) total gross leaseable floor area for commercial uses should not exceed 930 square metres;
- iv) total gross leaseable floor area per individual commercial use should not exceed 370 square metres;
- v) the centre shall be located at the intersection of arterial or arterial and collector roads, central to it's intended service area, have an intended service radius of not more than 0.8 kilometres and should not be located within this distance from a Commercial land use designation.
- vi) residential units should be provided where gross leaseable commercial floor area exceeds 370 square metres, and in a manner not to detract from the primary convenience commercial function of the centre.
- vii) auto related uses, excluding a gas bar, are not permitted.

b) Residential intensification of local neighbourhood convenience commercial centres is encouraged where it does not detract from the primary convenience commercial function of the centre.

Any application to increase the size of a centre beyond 0.4 hectares of land and/or 930 square metres in gross leaseable floor area will only be considered where such application includes development of residential dwelling units.

<p>c) Notwithstanding Section 8.3.3 b) above, minor boundary adjustments to a local neighbourhood convenience commercial centre may be permitted where deemed to be consistent with, and not contrary to the intent of Section 8.3.3 a) and b) above.</p> <p>4. Small scale local convenience commercial uses are permitted in higher density residential apartment and long term care complexes where sized, designed and intended to primarily serve the residents of the complex. The combined size of all convenience commercial uses should not exceed 5% of the total residential gross floor area of the complex, and shall not include auto related uses.</p>
<p>9. COMMERCIAL</p> <p>9.1. It is expected that the commercial areas designated in this Plan are sufficient in number, size and location to serve the commercial needs of area residents throughout the Plan time horizon. The expansion of commercial designations is discouraged. Intensification, redevelopment and reuse of existing commercial designations is intended to be the primary means of accommodating new or expanded commercial facilities. The City may require a commercial market study and other studies as set out in Part F, Section 16.16 of this Plan in consideration to expand or establish new commercial designations.</p> <p>9.2. Commercial Use Designations</p> <p>1. Major Commercial</p> <p>Major Commercial Centres are primarily intended to provide for major concentrations of commercial facilities to support shopping opportunities to serve and attract a city wide and broader regional population base and market.</p> <p>a) Permitted Uses A range of commercial uses are permitted, excluding auto dealerships and adult oriented uses. Other uses permitted include institutional, civic, cultural, indoor recreation, entertainment, and residential apartment dwelling units.</p> <p>b) Major Commercial Centres:</p> <ul style="list-style-type: none">i) are to be located on arterial roads with close proximity and adequate and accessible access to 400 series Provincial highways;ii) shall be developed and designed in campus and nodal format, occupying one or more properties functioning as a singular site or node, and may include lands on other quadrants of an intersection;iii) shall be designed to ensure:<ul style="list-style-type: none">a) integrated, common and shared access and parking, where feasible;b) strip or linear development and access points along arterial roads are minimized;c) adequate on-site parking is provided to accommodate all uses;d) safe internal vehicular traffic circulation, and to minimize traffic impacts on adjacent roads and uses;e) safe, connected, convenient, accessible and barrier free pedestrian and bike networks within and adjacent to the site development;f) common landscaping and design features;g) on- site and internal parking area landscaping and greening opportunities, and in support of pod parking design;h) that loading areas, outside storage not intended for retail display and sale of merchandise, and waste management facilities will be located away from, and screened from view from on-site pedestrian and vehicular activity areas; and screened from view from other on-site uses, internal and external roads, pedestrian use activity areas, and adjacent properties and uses not part of the site development;i) context sensitive building, site and streetscape design to support compatible development between on-site uses, and with adjacent properties and uses not part of the site development, including building form, scale, massing, height, setbacks, spacing, siting, orientation, facades, architectural materials, buffering, screening, landscaping;j) well defined and clearly articulated street edges.iv) are to be well served by public transit and will incorporate convenient and accessible transit transfer, arrival and departure facilities to serve as a major transit depot.<ul style="list-style-type: none">v) outside storage areas intended for retail display and sale of merchandise will be regulated through the implementing zoning by-law to ensure ancillary use, minimize negative impacts on pedestrian, bike, transit and vehicular circulation, and to enhance building, site and streetscape context sensitive design. <p>c) To support the primary function for city wide and regional population based shopping opportunities, the following applies:</p> <ul style="list-style-type: none">i) the Major Commercial Centre shall be at least 25 hectares of land in size

- and have a minimum total gross leaseable retail commercial floor area of 47,000 square metres;
- ii) non-retail uses should be limited in size and scale to protect the primary function of the Centre for shopping purposes;
- iii) residential uses shall be located in freestanding buildings or in upper storeys of commercial buildings, and will provide adequate exclusive outdoor amenity space for residents;
- iv) local office uses should not exceed 20 % of total gross leaseable retail commercial floor area.

d) The full range and scale of uses permitted in a Major Commercial Centre may not be allowed in all locations so designated. The precise range and scale of permitted uses on individual sites within the Major Commercial designation may be stipulated in District Plans or the implementing zoning by-law, and may be subject to impact studies as set out in Part F, Section 16.16 of this Plan including, but not limited to, use compatibility, traffic, parking, market analysis, and building and site design.

e) Any application to establish a new centre or to expand an established centre beyond existing property limits will require an amendment to this Plan, and may be subject to studies as set out in Part F, Section 16.16 of this Plan including, but not limited to, market analysis, land use compatibility and traffic.

2. Community Commercial

Community Commercial Centres are to be spatially dispersed throughout the Urban Area, primarily intended to provide concentrations of commercial facilities to support day to day and weekly shopping and service needs for the local surrounding community population.

a) Permitted Uses

A range of commercial uses are permitted, excluding auto dealerships, adult oriented uses and hotels. Other uses permitted include institutional, civic, cultural, indoor recreation, entertainment and residential apartment dwelling units.

b) Community Commercial Centres:

- i) are to be located at the intersection of arterial roads and well served by public transit service;
- ii) are subject to Section 9.2.1 b) ii), iii) and v);
- iii) shall generally not exceed a total gross leaseable floor area of 30,000 square metres for all uses except residential, and non retail/service commercial uses should be limited in size and scale to protect the primary function of the Centre for the provision of shopping and service commercial facilities;
- iv) shall limit the size of individual uses in a manner to support a diversity and mix of uses within the Centre;
- v) residential uses shall be located in freestanding buildings or in upper storeys of commercial buildings, and will provide adequate exclusive outdoor amenity space for residents;

c) In recognizing the diversity in size of established and designated Community Commercial Centres, the permitted size of centres shall generally range between 2 hectares and 12 hectares of land.

The full range and scale of uses permitted in Community Commercial Centres may not be allowed on all properties so designated. The precise range and scale of uses permitted on properties designated Community Commercial may be stipulated in District Plans or the implementing zoning by-law, and may be subject to studies as set out in Part F, Section 16.16 of this Plan including, but not limited to, use compatibility, traffic, parking, market analysis, and building and site design.

d) Any application to establish a new centre or to expand an established centre beyond existing property limits will require an amendment to this Plan, and may be subject to studies as set out in Part F, Section 16.16 of this Plan including, but not limited to, market analysis, land use compatibility and traffic.

3. Arterial Commercial

Arterial Commercial designations are primarily intended to provide a range of service commercial uses, and uses to serve the travelling public and the automobile using consumer, and will be established only in a very limited number of suitable locations along arterial roads.

a) Development of Arterial Commercial properties is encouraged through campus format, occupying one or more properties functioning as a singular site, and will be evaluated pursuant to Section 9.2.1 b) iii).

b) Permitted Uses
Permitted uses include retail and service commercial uses, excluding large scale retail food stores and adult oriented uses. Other uses permitted include institutional, office, civic, cultural, and indoor recreation uses.

i) Office uses shall generally not exceed a maximum 10 % of permitted total gross leaeable floor area on the property.
ii) Outside storage areas intended for retail display and sale of merchandise will be regulated through the implementing zoning by-law to minimize negative impacts on adjacent or adjoining properties, on pedestrian, bike, transit and vehicular circulation, to enhance building, site and streetscape design, landscaping opportunities, and to support well defined and clearly articulated street edges.

c) The full range of arterial commercial uses permitted may not be allowed in all locations so designated. The precise range and scale of permitted uses on individual sites within the Arterial Commercial designation may be stipulated in District Plans or the implementing zoning by-law, and may be subject to studies as set out in Part F, Section 16.16 of this Plan including, but not limited to, use compatibility, traffic, parking, market analysis, and building and site design.

Any application to expand an existing, or establish a new Arterial Commercial designation will require an amendment to this Plan, and may be subject to studies as set out in Part F, Section 16.16 of this Plan including but not limited to use compatibility, traffic, parking, market analysis, and building and site design.

12. MIXED USE

12.1. General Policies

Lands currently designated mixed use, or to be developed or redeveloped for mixed use, are subject to the following:

i) Are to be developed to support minimum densities of 100 people and jobs per hectare of land;
ii) Work Live Accommodation is permitted, and encouraged to locate in campus format to support integrated and shared support services and business incubation opportunities.
iii) Retail and service commercial uses will only be permitted on the ground floor;
iv) The development or redevelopment for commercial, institutional and recreational buildings generally in excess of 930 square metres of gross floor area shall only be permitted in combination with residential units. Commercial, institutional and recreation buildings shall generally not exceed 1860 square metres in gross floor area;
v) Height will be restricted to ensure street animated development, and to recognize the character of surrounding neighbourhoods. Building height will be stepped back to support street animated development and protect adjacent neighbourhoods from intrusive development. The height of buildings will generally not exceed 20 metres;
vi) Parking requirements may be minimized, and shared parking and access is encouraged in order to reduce street front parking areas and support transit friendly development;
vii) All development will be of high quality design that considers the integration of new and existing buildings, connected building façade treatments and streetscape initiatives to ensure pedestrian first design principles and greening;
viii) Prohibited uses in the mixed use designation include vehicle sales and auto related service facilities. Gas bars and car washes may be permitted subject to zoning bylaw amendment;
ix) Recognizing the preferred mix of uses contemplated, the City’s urban design guidelines should be emphasized in the evaluation of all development, redevelopment and intensification opportunities.

13. GREEN SPACE

13.1. PARKLAND AND OPEN SPACE

4. Parkland Dedication

a) The City will, as a condition of development, redevelopment or subdivision of land, require that land in an amount not exceeding, in the case of land proposed for commercial and employment purposes, 2 % and in all other cases 5 % of the land proposed for development, redevelopment or subdivision be conveyed to the municipality for parks or other recreational purposes, pursuant to the provisions of the Planning Act.
b) In the case of development, redevelopment or subdivision of land proposed

- for mixed use purposes, the conveyance of land shall be calculated at 2% for commercial or employment uses, and 5 % for all other uses.
- c) The City will use alternative requirements where dedication of parkland is greater than a) and b) above, pursuant to the Planning Act, as follows:
- i) for residential development:
- up to 40 units per hectare of land, park dedication will be calculated based on 1 hectare of land for every 300 dwelling units;
 - between 40 units and 80 units per hectare of land, parkland dedication will be calculated based on 1 hectare of land for every 400 dwelling units;
 - exceeding 80 units per hectare of land, parkland dedication will be calculated based on 1 hectare of land for every 500 dwelling units.
- d) Notwithstanding a) to c) above, not more than 30 % of any lands proposed for development or redevelopment shall be required for parkland dedication.
- e) Land conveyed to the City as parkland dedication will be expected to meet standards for location, size, drainage, grading, topography, and other criteria as outlined in the Parks Policy Plan and the policies of this Plan.
- f) The municipality will generally not accept the conveyance of lands for parkland dedication that are designated Natural Area or zoned for environmental protection, or that are to be designated Natural Area or zoned for environmental protection as part of the development approval process.
- g) Land conveyed to the City as parkland dedication in the Downtown will be taken, where suitable, to increase landscaped areas along the street front to enhance pedestrian circulation, opportunities for streetscape amenities, and to extend or establish special urban parks and active transportation routes.
- h) Lands conveyed to the City as parkland dedication on properties adjacent to the Lake Ontario waterfront will be taken, where suitable, as the lands on that portion of the property adjacent to the waterfront.
- i) The municipality encourages the provision of indoor and outdoor recreation and amenity space in the design of medium and high density residential developments. The provision of private open space and recreational and amenity space in medium and high density developments will be in addition to the requirements for parkland dedication.
- j) Cash-in- lieu of parkland dedication as set out in Section 13.1.4 a) to c) may be accepted by the municipality, pursuant to the Planning Act, where:
- i) required dedication of land would render the remainder of the site unsuitable or impractical for development or redevelopment;
 - ii) required dedication of land fails to provide an area of suitable shape, size, location or other criteria, including priority for land acquisition as set out in Section 13.1.3 of this Plan, and in the Parks Policy Plan.

PART E: DISTRICT PLANS
NORTH DISTRICT

15.1. NORTH DISTRICT

The following specific or special policies apply to lands located on the following schedules:

1. Schedule E1

- a) Auto dealerships shall not be permitted on lands designated Business Commercial Employment fronting on Secord or Nihan Drive.
- b) Notwithstanding Part D, Section 9.2.2 of the Plan, the following applies on the lands designated Community Commercial located to the west of Lakeport Road and north of Gertrude Street, and generally known as Port Dalhousie Commercial Core:
- i) a) an appropriate range and mix of medium or higher density residential housing, commercial, employment, institutional and indoor recreation uses are permitted, to support regional based tourist facilities to enhance the economic vitality of the area;
 - b) no ground floor residential dwelling units are permitted;
 - c) office uses which do not generate a significant amount of pedestrian traffic or require a ground floor location are encouraged to locate on upper floors;
 - d) retail functions are permitted on upper floors only where in conjunction with a ground floor commercial use.
- ii) In addition to Section 15.1.1 b) i) above, the following applies to the lands designated Community Commercial west of Lakeport Road, north of Lock Street, and specific to the lands described as Port Place lands and

subject to the Ontario Municipal Board Final Orders issued on May 14, 2010, and August 3, 2010 with respect to Case No. PL060850:

a) a multi-use commercial/residential building is permitted having a 17 storey height limit to a maximum of 62 metres, provided that:

- i) the residential component is part of a multi-use commercial redevelopment scheme including retail, office, restaurant, hotel and theatre uses, and
- ii) appropriate development standards are established in the related zoning amendment, site plan agreement, heritage easement agreement and any other approval or applicable development agreement, and
- iii) a Holding (H) designation shall be applied to the residential component; and buildings and structures may be erected on the subject lands prior to the removal of the specific Holding (H) designation, however, the residential component shall not be used for residential purposes until such time as the provisions of the zoning by-law are met, including all conditions related to the Holding (H) conditions.

c) Lands located on the east side of Lakeport Road, west of Port Dalhousie Harbour, the following applies:

- i) Notwithstanding the Community Commercial land use designation, the development or redevelopment of these lands permit an appropriate range and mix of medium and higher density residential housing, commercial, employment, institutional and indoor recreation uses to support regional based tourist facilities to enhance the economic vitality of the area.
- ii) prior to development or redevelopment of the subject lands, an urban design study shall be undertaken by the proponent(s), to the satisfaction and approval of the City, to ensure implementation of appropriate design standards to support a connected public realm, protection for public access, vistas and views to the waterfront, and protection of cultural heritage amenities.

d) Mixed Use Intensification ‘Special Study Area’ as set out in Part F, Section 16.20

- Lakeshore Road between Ontario Street and Geneva Street.
- primary basis/focus for study: to establish an appropriate range of transit supportive medium and higher density housing opportunities with a mix of commercial, employment, institutional and recreation uses, pursuant to Part D, Section 12 of the Plan; and to establish urban design strategies to guide redevelopment and intensification.

e) Harbour industrial marine uses, excepting boat slips and administrative office, shall not be permitted on the west side of Port Dalhousie Harbour.

f) Port Dalhousie Residential Neighbourhood - Notwithstanding Part D, Section 8.1 of the Plan, for the applicable lands subject to Special Provision 7 in Zoning By-law No. 2013 -283, height of buildings should not exceed 9 metres.

g) Port Dalhousie Residential Neighbourhood - Notwithstanding Part D, Section 8.1 of the Plan, on lands bounded by Lake Ontario, Bayview Drive, Christie Street, and the rear lot line of properties fronting Considine Avenue, height of buildings should not exceed 7.5 metres.

h) Notwithstanding the Low Density Residential designation, business offices are permitted on lands known as 1-3 Lakeshore Road.

i) Notwithstanding the Low Density Residential designation, a medium density residential development consisting of one single detached dwelling and a 15 unit apartment building, for a maximum of 16 dwelling units, is permitted on lands known municipally as 6 and 10 Dalhousie Avenue.

j) Rowing facilities and associated uses are permitted on lands known municipally as 72 Henley Island Drive, and 60 Lakeport Road, subject to the policies in Part D, Section 13 of the Plan.

k) Notwithstanding Section 9.2.3 b) of this Plan, on lands known municipally as 300/302 Lake Street, office uses are permitted to a maximum 50 % of total gross leaseable floor area, and residential apartment units are permitted on the upper floor of the two storey building.

PART F: IMPLEMENTATION AND INTERPRETATION

16: IMPLEMENTATION

16.1 Official Plan

Every application for amendment to this Plan will be evaluated on the basis of the following general considerations and any others that are pertinent to the particular

application:

- i) the degree of conformity of the proposed amendment to the general intent, purpose and philosophy of this Plan, particularly the vision, planning principles and general policies of the Plan;
- ii) consistency with Provincial and upper tier government plans, policies and legislation;
- iii) the availability and suitability of land already designated for the proposed use, and the need for, and the feasibility of the proposed use;
- iv) the compatibility of the proposal, or the adequacy of proposed mechanisms for achieving compatibility, with adjacent and planned uses;
- v) the potential of the proposal to cause instability within an area intended to remain stable;
- vi) the ability of the City's infrastructure and public service facilities to accommodate the proposal without costly expansion, upgrading, or required deferral of other planned infrastructure and public service facility improvements in other areas of the City;
- vii) the financial implications, both costs and revenues, to the City;
- viii) the degree to which approval of the amendment would establish an undesirable precedent.

16.2. Zoning

1. Zoning by-laws shall be used to implement the policies of the Plan by regulating the use of land, buildings or structures in accordance with the Planning Act, and may be more restrictive than the provisions of the Plan. By-laws will establish the following criteria:

- a) Land use zones which will permit the type of development specified in the Plan
- b) Development and performance standards appropriate to each type of use
- c) Any regulations needed to implement the intent of the Plan

2. Notwithstanding, in the area of the Niagara Escarpment Plan, certain lands are subject to Niagara Escarpment Development Control, as identified in Part F, Schedule F5. In these areas of Development Control, City zoning by-laws have no force or effect. Subject to Niagara Escarpment Regulations, permits are required from the Niagara Escarpment Commission for all new development in the Development Control Area.

16.3. Holding By-Laws

1. The City may, in a zoning by-law, affix an "H" in conjunction with any zoning category in order to limit or prevent the use of certain lands which are considered inappropriate or premature for immediate development.

2. Where a holding designation is in effect, the lands shall generally be limited to existing uses or uses which will not prejudice the future development of these lands. In addition, no building or structure may be erected on the site until such time as an application has been approved by Council to remove the "H" symbol. The holding symbol shall not apply to minor accessory buildings and structures such as sheds, garages, or pools.

3. Prior to enacting a by-law to remove the holding provision in accordance with the Planning Act, Council shall be satisfied that:

- a) Adequate servicing, such as water supply, sewage disposal facilities, storm water drainage, solid waste collection and disposal, and roads can be provided;
- b) All necessary requirements of the city have been satisfied;
- c) All necessary development agreements have been entered into, and that the conditions of these agreements have been, or will be met;
- d) Development satisfies all other relevant policies of the Plan.

16.7. Site Plan Control

1. All lands within the limits of the City shall be designated a proposed Site Plan Control Area.

2. The City shall, by by-law, identify specific types of development subject to site plan control. It may also deem certain types of development exempt from site plan control.

3. Unless an exemption is granted under Part F, Section 16.7.2, site plan approval shall be required for all of the following:

- a) All lands used, zoned or to be zoned for residential purposes, save and except lands used, zoned or to be zoned for residential buildings having less than 4 dwelling units.
- b) All lands used, zoned or to be zoned for commercial, employment, institutional, public or recreational purposes.

- c) All lands used, zoned or to be zoned for mixed-use purposes.
- d) All lands within the “Agriculture Area” which are used, zoned or to be zoned for:
 - i. Existing and expanded non-agricultural uses, except residences, where there is an enlargement of existing uses, as set out in Part D, Section 14.2.10.
 - ii. Wineries as set out in Part D, Section 14.2.2.
 - iii. Agriculture farm related commercial and industrial uses directly related to and serving the surrounding agricultural uses as set out in Part D, Section 14.2.4.
 - iv. Agri-tourism and value-added uses and activities greater than 93 square metres, as set out in Part D, Section 14.2.1, with the exception of bed and breakfast accommodation and uses set out in Part D, 14.2.1.b).
- e) All lands used for the purpose of public service by the Corporation of the City of St. Catharines and any Local Board or Commission thereof, as defined by the Municipal Affairs Act, and the Regional Municipality of Niagara and any Local Board or Commission thereof as defined by the Municipal Affairs Act.

4. No development shall be undertaken within a site plan control area designated in accordance with Section 16.7.3, until the City has approved drawings and/or agreements sufficient to ensure the matters set out Subsections 41(4) and 41(5) of the Planning Act as amended, including but not limited to:

- a) Matters relating to exterior design, including without limitation, the character, scale, appearance and design features of buildings and their sustainable design.
- b) Sustainable design elements within and adjoining a City right-of-way, including, without limitation, trees, landscaping, permeable paving materials, street furniture, curb ramps, waste and recycling containers, and bicycle facilities.
- c) Matters relating to the protection and continued function of natural areas, features and hazard lands.

5. As a condition of approval of the plans and/or drawings required in this section, the City may require the owner of the land to:

- a) Provide and maintain, to the satisfaction of and at no expense or risk to the City, the facilities, works, or matters set out in Section 41(7) of the Planning Act as amended.
- b) Enter into one or more agreements with respect to the provision and maintenance of such facilities, works, or matters.
- c) Enter into one or more agreements to ensure that the development proceeds in accordance with the approved plans and drawings as required by this Section.

16.10. Subdivision Control

1. All lands within the City shall be subject to subdivision control pursuant to the Planning Act. The provisions of the Planning Act, including subdivision agreements, will be used to ensure that the land use designations and policies of the Official Plan are complied with, and that a high standard of design is maintained in all development.

2. Only those plans of subdivision which conform to the following criteria will be recommended for approval.

- a) The plan of subdivision conforms with the policies of this Plan.
- b) Adequate servicing, such as water supply, sewage disposal facilities, storm water drainage, solid waste collection and communications/ telecommunications infrastructure, and access can be provided.
- c) The City is able to provide necessary community services without imposing undue increases in taxation on all residents.

16.11. Lot Creation

1. Creation of lots shall only be effected through consent or plans of subdivision,

2. Consents to sever parts of lots as an alternative to a registered plan of subdivision will be discouraged and will only be permitted when such a plan would clearly not be needed to ensure the intent of applicable policies in this Plan are followed.

3. Consents to sever individual parcels of land will only be given where:

- a) It is clearly apparent that no development could take place which would lead to significant expense by the City for public works or which would lead to further development leading to such expenses.

- b) They contribute to the infilling of areas that are already substantially developed.
- c) The size and shape of the parcel is appropriate for the use proposed and in terms of the optimum development of the surrounding area.

4. Provisions for severing individual parcels of land within the Agriculture Area are found in the Agriculture policy section of the Plan.

5. It is the general intent of this Plan to provide for the severance of land or the creation of easements for public parks, open space and trails, where such consents do not result in the creation of additional building lots.

16.12. Non-Conforming Uses

1. A land use that does not conform to the zoning by-law, but which lawfully existed prior to the approval of the zoning by-law, is a non-conforming use. If such nonconforming uses ceases, then the rights derived from the non-conforming use shall terminate.

2. Non-conforming uses that are deemed to comply with the intent of the Plan may be zoned in accordance with their present use provided:

- a) The zoning does not permit any significant change of use or performance standards that will result in or aggravate any situation detrimental to adjoining land uses.
- b) It does not interfere with desirable development in adjacent areas that are in conformity with the Plan.

3. It is the intention and expectation that non-conforming uses shall eventually cease to exist and be replaced by uses that conform with the intent of the Plan and the zoning by-law.

4. In special circumstances, it may be appropriate to consider the extension or enlargement of non-conforming uses provided:

- a) the expansion or enlargement will not jeopardize the possibility of future development/redevelopment in their vicinity that may comply more closely with the intent of this Plan.
- b) special efforts are made to enhance the compatibility of the uses and to improve the amenity and design, more particularly buffering, landscaping, parking, and active transportation and vehicular circulation.
- c) the expansion or enlargement is directed to areas outside natural heritage and natural hazard lands.

5. Notwithstanding the provisions on extensions or enlargements of non-conforming uses, nothing will prevent the rebuilding or repair of any building or structure that is damaged or destroyed by fire or natural disaster subsequent to the adoption of this Plan provided:

- a) The dimensions of the building or structure that was destroyed are not increased.
- b) The use of the building is not altered.
- c) The building or structure is not located in a flood plain, unstable slope or erosion area.

6. However, where the area of the lot is sufficient, the new buildings or structures shall be located in conformity with the requirements of the zoning by-law.

Further, nothing shall prevent the upgrading to a safe condition any such building or structure provided such repair or alteration does not increase the size or height of the building or structure.

- a) Inadequacy of some or all hard services, including water, sanitary sewers, storm sewers, roads, lighting and sidewalks;
- b) A significant portion of the housing stock and other buildings are in need of maintenance, rehabilitation or redevelopment;
- c) An inadequate mix of housing types; inadequate affordable housing;
- d) The presence of non-compatible land uses;
- e) Inadequacy of social, recreational, open space and cultural facilities and services;
- f) Within commercial areas, there is deterioration in the appearance of building facades, inappropriate signage, inadequate parking facilities or inadequate pedestrian accessibility;
- g) Within employment areas, the inefficiency of employment sites, access to, from and within the employment areas, inadequacies of parking, loading and outside storage areas, conflicts with neighbouring uses or the presence of obsolete buildings;
- h) Areas where there is significant environmental contamination and areas of potential environmental contamination;
- i) Areas exhibiting environmental, social or community economic development problems

- j) Areas exhibiting natural hazard land or natural heritage deterioration or degradation;
- l) Improvement of housing and/or employment opportunities to support density targets established in this Plan for residential neighbourhoods, employment areas, mixed use areas, intensification areas and the Urban Growth Centre.

16.15. Committee of Adjustment

The Committee of Adjustment for the City of St. Catharines shall be guided by the general intent and purpose of this Plan and the implementing zoning by-law in making decisions on applications for minor variances.

16.16. Pre-Consultation and Complete Application Submission Requirements

1. Consultation with the City is required prior to the submission of an application for an Official Plan amendment, zoning by-law amendment, draft plan of subdivision, draft plan of condominium, consent or site plan control, unless the Director of the Planning and Development Services Department or designate determines that preconsultation is not necessary based on the scale of the development or the complexity of planning issues associated with the proposed application.
2. Pre-consultation will determine what is required to be submitted for a complete application and will provide the opportunity to discuss the nature of the application, development and planning issues, the need for additional information and/or report(s) to be submitted with the application, and the planning and approval process including the appropriateness of concurrent applications, where applicable.
3. Pre-consultation may also involve the Region of Niagara, the Niagara Peninsula Conservation Authority, or other agencies and adjacent municipalities that may have an interest in the application as determined by the City.
4. Pre-consultation shall be considered a requirement for the submission of a complete application.
5. Mapping, drawings, reports and technical studies shall be required to support any application requiring a Planning Act approval. The supporting information and materials required shall be determined by the City in consultation with the Region of Niagara, Niagara Peninsula Conservation Authority, other appropriate agencies, adjacent municipalities, and the applicant, prior to the submission of the application as part of the pre-consultation process.
6. In addition to the information and materials required under the Planning Act and any other legislation or regulation, additional information in the form of studies or assessments identified in Section 16.16.5 may be required to consider a planning application complete.
7. The additional information will be required as part of a complete application and shall be provided along with the prescribed information required under the Planning Act in order to ensure that all the relevant and required information pertaining to a development application is available at the time of submission to enable the City to make informed decisions within the prescribed time periods. It also ensures that the public and other stakeholders have access to all relevant information early in the planning process.
8. The additional information or material that may be required includes, but is not limited to the following:
 - a) Planning
 - justification report
 - land use needs assessment
 - economic study
 - market study
 - green space/trail needs assessment
 - financial impact assessment
 - b) Environmental Assessment
 - environmental assessment
 - environmental impact statement/study
 - soil, environmental audit, record of site condition
 - slope stability study
 - tree inventory and preservation study
 - energy assessment report
 - air quality report
 - natural features restoration plan
 - noise vibration, shadow, wind study
 - agriculture impact assessment
 - minimum distance separation
 - health impact assessment
 - hazard lands study
 - c) Servicing/Infrastructure

- servicing study
- grading and drainage plan
- stormwater management study
- hydrology, soils and/or geotechnical study
- sub-watershed study

d) Transportation

- traffic impact study
- parking demand analysis
- active transportation circulation plan
- transportation impact study
- transportation demand management plan

e) Urban Design/Culture

- archaeological impact assessment
- streetscape design study
- building elevations
- built form/site design
- cultural heritage study
- cultural heritage impact assessment
- landscape plan

9. The information and material submitted shall be in an electronic format along with a hard copy to the City to make this information readily available to the public and commenting agencies, and shall be prepared by a qualified professional to the satisfaction of the City, retained by and at the expense of the applicant. The City shall review all reports and studies and may also require a peer review by an appropriate public agency or by a professional consultant retained by the City at the applicant's expense.

10. The Director of Planning and Development Services or their designate shall be responsible for determining whether a planning application is complete. If an application is submitted without pre-consultation, adequate supporting information and/or reports, and any application review fees required by the local municipality, the Region of Niagara, Niagara Peninsula Conservation Authority or any other public agency, the application may be deemed to be incomplete.

16.17. Processing Times

1. The City endeavors to process all development applications as expeditiously as possible. However, it is recognized that certain development proposals are complex and may require considerable time to fully review. As such, it is difficult to predict a consistent time frame for all proposals. Notwithstanding, the City of St. Catharines will attempt to process applications within the time frames provided for in the Planning Act as amended from time to time, provided all the necessary information has been provided by the applicant.

2. The City shall establish target timeframes for processing other types of planning applications for which the Province has not established time frames.

16.20. Special Study Areas

1. Certain lands or areas in the City may be set out as a 'Special Study Area'.

2. A 'Special Study Area' is subject to future study by the City or by development proponents to determine and implement appropriate land use designations and/or policies to guide future development, redevelopment or intensification in the affected area(s).

3. The general boundaries of a 'Special Study Area' will be identified on District Plans in Part E of this Plan, and may be more specifically identified at the commencement of the study process.

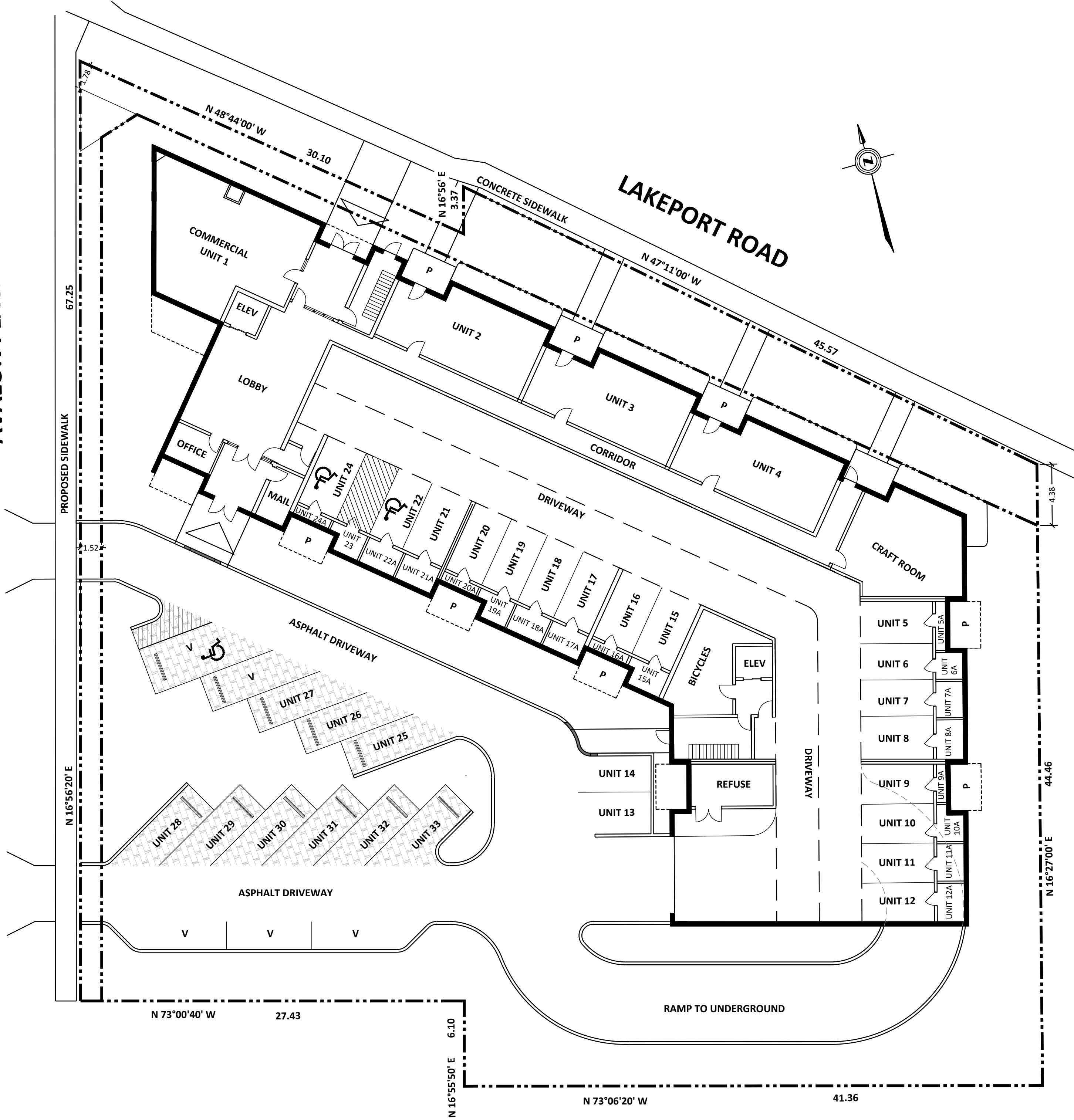
The general vision and parameters for study of the 'Special Study Area' will also be set out in Part E 'District Plans' of this Plan.

4. The Special Study will commence subject to an approved municipal works program or at such time as a major Official Plan Amendment, Zoning By-law Amendment or development application is brought forward affecting any lands within the Special Study Area.

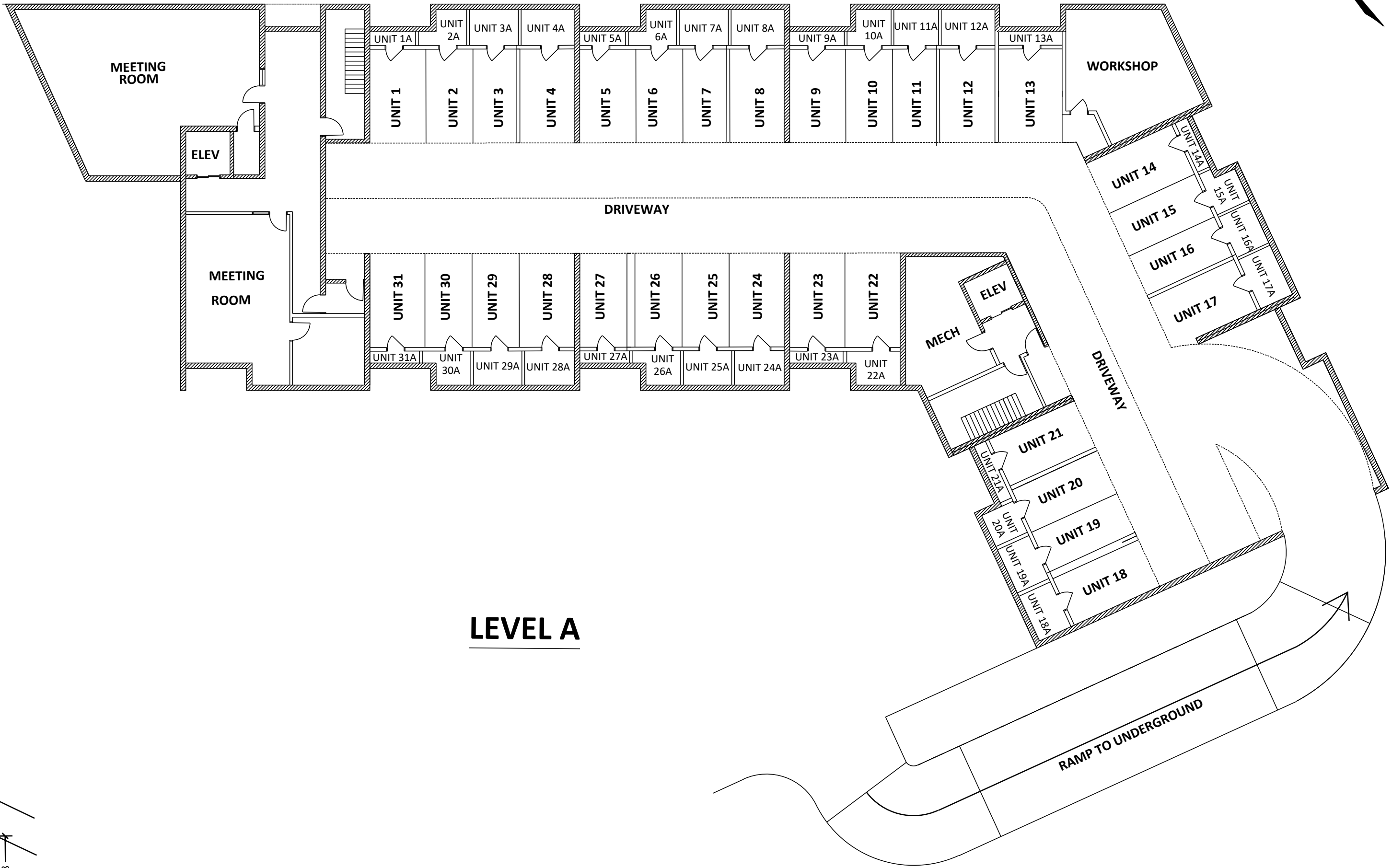
The municipal works program will set out a priority phasing schedule for undertaking study of the Special Study Areas, to be reviewed on an annual basis. The priority phasing schedule will be based, in part, on the need to assess infrastructure and public service facility requirements and improvements necessary to support the general study vision and parameters set out for the specific study areas, and the overall growth strategy established in this Plan.

<p>5. Terms of Reference for the study of the Special Study Area will be prepared and are subject to the approval of City Council.</p> <p>6. The findings of the Special Study shall be implemented by Amendment to this Plan considered in relation to Part F, Section 16.1.</p> <p>Until such time as revised land use designations and/or policies have been implemented by amendment to this Plan, the underlying land use designations and applicable policies established in this Plan will apply.</p> <p>16.21. Exemption Status</p> <p>i) Where site plan approval, plan of subdivision or plan of condominium final approval, or draft plan of subdivision or condominium approval has been granted by the City prior to the final adoption of this Plan, such approval is exempt from the policies of this Plan only insofar as to the policies of this Plan that are directly applicable to the plan or draft plan approval, and where those applicable policies for approval are more restrictive in this Official Plan than policies in effect at time of approval.</p> <p>This policy does not apply to draft plan approvals granted prior to the final adoption of this Plan that have lapsed.</p> <p>ii) Where draft plan of subdivision or condominium approval has been granted prior to final adoption of this Plan, the City will generally not grant an extension of lapsing draft plan approval beyond 6 years from the date of final adoption of this Plan, unless it can be demonstrated that such draft plan meets the growth management strategy and policies of this Plan.</p>
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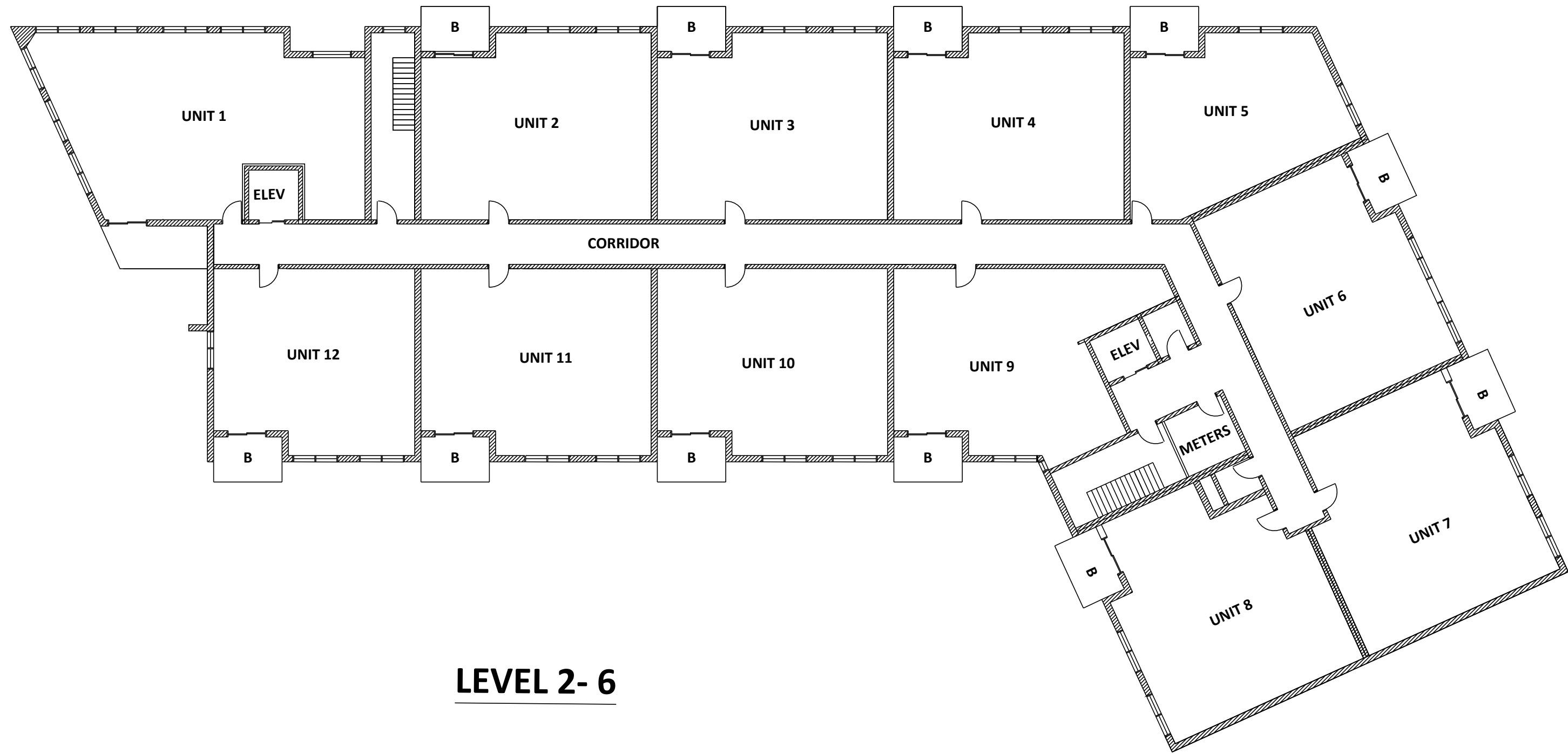
AVALON PLACE



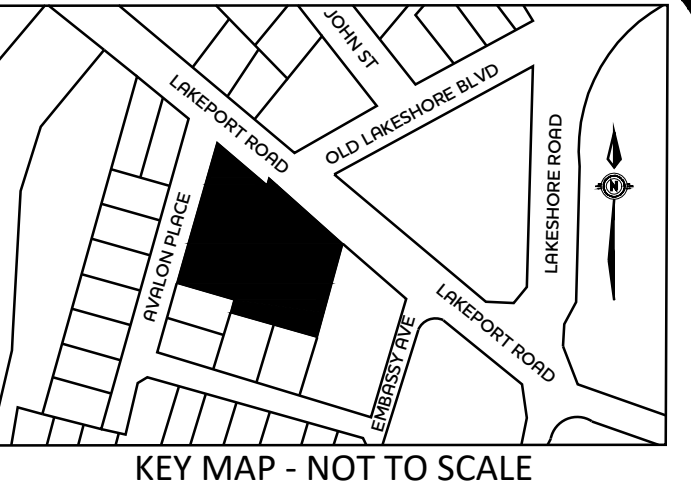
LEVEL 1



LEVEL A



LEVEL 2- 6



REQUIREMENTS OF SECTION 51(17) OF THE PLANNING ACT

- | | |
|-------------------|---------------|
| (A) SEE PLAN | (B) SEE PLAN |
| (C) SEE PLAN | (D) SEE PLAN |
| (E) SEE PLAN | (F) SEE PLAN |
| (G) SEE PLAN | (H) MUNICIPAL |
| (I) SILTY CLAY | (J) SEE PLAN |
| (K) FULL SERVICES | (L) SEE PLAN |

METRIC NOTE
DISTANCE SHOWN ON THIS PLAN ARE IN METERS AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048

- LEGEND**
- V - VISITOR PARKING
 - P - PORCH
 - B - BALCONY

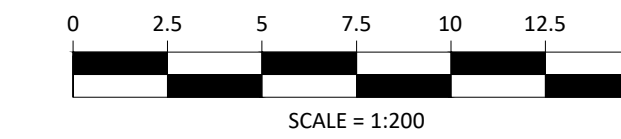
LEGAL DESCRIPTION
LOTS 19, 20, 21, & 22 REGISTERED PLAN No. 214 PART OF LOT 20, CONCESSION 2 AND PART OF ROAD ALLOWANCE BETWEEN CONCESSION 1 & 2 GEOGRAPHIC TOWNSHIP OF GRANTHAM, CITY OF ST. CATHARINES, REGIONAL MUNICIPALITY OF NIAGARA.

OWNER'S CERTIFICATE
BEING THE REGISTERED OWNER, I HEREBY AUTHORIZE BETTER NEIGHBOURHOODS INC. TO PREPARE AND SUBMIT THIS DRAFT PLAN TO THE PLANNING DEPARTMENT OF THE CITY OF ST. CATHARINES.

SIGNATURE	DATE
SIGNATURE	DATE
SIGNATURE	DATE

SURVEYOR'S CERTIFICATE
I HEREBY CERTIFY THAT THE BOUNDARIES OF THE LANDS TO BE SUBDIVIDED AND THEIR RELATIONSHIP TO THE ADJACENT LANDS ARE ACCURATELY AND CORRECTLY SHOWN.

2018 09 05
WILLIAM A. MASCOE DATE
ONTARIO LAND SURVEYOR



better neighbourhoods
DEVELOPMENT CONSULTANTS
190A Ontario Street, Ontario L2R 5K3
Office: 905.684.8585 betterneighbourhoods.ca

PROJECT TITLE:
**104 LAKEPORT ROAD
St. Catharines, Ontario**

DRAWING TITLE:
**DRAFT PLAN OF
CONDOMINIUM**

DATE OF ISSUE: 2018 09 05

DRAWING No: 0241 DPC1	REV. NO: 0
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102 and 104 Lakeport Road and 3 and 5 Avalon Place

Public Submissions

Blozowski, Kevin

From: Colleen Beard [REDACTED]
Sent: Tuesday, October 09, 2018 8:36 AM
To: Mayor's Administration; Blozowski, Kevin; Sendzik, Walter
Cc: Williamson, Bruce; Garcia, Carlos
Subject: Attention Mayor, City Council and Staff - Re: 104 Lakeport Rd. Proposal
Attachments: 104Lakeport_Rd-proposal_response-BEARD.pdf

Good morning,

As a home owner on Lakeport Rd., please accept my response regarding the 102 & 104 Lakeport Road development application (see pdf attached).

I trust this will be forwarded to appropriate City staff and Council for their review as part of the application process.

Thank you,
Colleen Beard
91 Lakeport Rd.
St. Catharines

Click [here](#) to report this email as spam.

Colleen Beard
91 Lakeport Rd. (owner)
St. Catharines, ON
[REDACTED]

October 9, 2018

TO: Mayor of the City of St. Catharines, City Councillors, and City Planning staff

RE: 102 & 104 Lakeport Road Development Application

Although most of us in the immediate neighbourhood of Avalon Pl., Bernhardt St., Lakeport Rd., and surrounding area do not oppose development on this site, we do strongly oppose the magnitude of the proposed development mainly for the reason that it is **NOT** compliant with the permitted uses of a C2 Community Commercial as outlined in the *Garden City Plan* and the *Zoning By-Law 2013-283*.

I trust our City planning staff and councillors will be informed and consider the following:

ISSUE 1: Size of Building Property

According to Official Plan, Section 9.2.2 c) **Community Commercial Centres** "...the permitted size of centres shall generally range between 2 hectares and 12 hectares of land."

Comment: Please note that the proposed development is on a property of 0.4 hectare!
This leads to the following concern ...

ISSUE 2: This is a Zone R4 proposal, NOT a C2.

Typically, Zone C2 Community Commercial centres "... are primarily intended to provide concentrations of commercial facilities to support day to day and weekly shopping and service needs for the local surrounding community population." (*Garden City Plan, Section 9.2.2*)

AND, should a C2 complex include residential occupancy, these units "Must be located on the same lot as commercial uses, to a **maximum 15% lot coverage**" AND/OR "Dwelling units are **only permitted above**, to the **rear and/or below** non-residential uses." (*Zoning by-law 6.2.1*)

Comments: The proposed residential space occupies much beyond 15% of the property (41%), and IS the main function of the building, not a commercial centre.

Is this the intent of the proposal? I think not! The application is suspicious!

In fact, the density of residential occupancy in this proposal meets the definition of a **high density** residential (R4) zone.

The **High Density** residential designation (R4) permits "... apartment dwellings at a density generally 85 units per hectare of land or greater." (*Garden City Plan Section 8.1.3*)

THIS PROPOSAL HAS A DENSITY OF 160 UNITS PER HECTARE! ... and therefore is regarded as a "high density" residential complex!

Comments: This proposal is for a high density residential development. No question! It includes a small commercial space in its attempt to call it a "Mixed Use Building", **AND** to avoid the parking requirement of 1.25 spaces per unit (required for an apartment). Let's not be fooled. Is the City this desperate to jeopardize the integrity of our city

Proposed Mixed Use Building		
Units	63	
Commercial Space	100 sqm	
Site Area	0.4 ha	
Density (unit/ha)	160	
Building Coverage	41%	
Pavement Coverage	23%	
Landscaping Coverage	36%	
Parking		
	Required	Provided
Residential	59	59
Commercial	5	5

planners and councillors? This development has no benefit to our neighbourhood other than profiting the developers.

In a recent Mayor debate, Mayor Sendzik identified the availability of affordable housing as an issue the City is currently facing (**St. Catharines Standard, October 5**). Is this complex, (and the four others in Port Dalhousie that have been approved by City Council, with amendments) going to be “affordable” to the average citizen? Again, I think not!

ISSUE 3: Local street access

According to Official Plan, Section 9.2.2 b) i) for Community Commercial states:
“CC Centres are to be located at the intersection of arterial roads”.

Comments: There is a good reason for this. I assume it is that arterial roads can accommodate the flow of in—out traffic, and not rely on a small residential side street, like Avalon Place.

City staff should be challenged to identify an example of where, within the North planning district E1, there is a CC of equal proportion on this size a footprint (.4 ha) that uses a low residential dead-end side street to accommodate its traffic flow.

IF there is – what has the impact been on the neighbourhood?

IF there is not – this is setting a serious and dangerous precedence, and it won’t be long before the rest of Lakeport Rd north will be C2.

ISSUE 4: Re-zoning of 5 Avalon from R2 to C2

Comments: the current resident at #7 Avalon grew up and then purchased this house, in a quiet residential neighbourhood. She is now facing the possibility of her neighbouring property transform into a thoroughfare for 6-stories of condo traffic. Pause for a minute, and put yourself in her shoes! No doubt, the value of her property will plummet, not to mention the overall quality of life.

The City is chipping and eroding away low density residential and affordable housing to accommodate vertical growth. Let’s do this in areas that are already zoned for this kind of development and abide by the rules. Enough of the amendments!

ISSUE 5: Traffic/Parking impact study

Garden City Plan, Section 9.2.2 c) *“The full range and scale of uses permitted in Community Commercial Centres **may not be allowed** on all properties so designated. The precise range and scale of uses permitted on properties designated Community Commercial may be stipulated in District Plans or the implementing zoning by-law, and may be subject to studies as set out in Part F, Section 16.16 of this Plan including, but not limited to, use compatibility, **traffic, parking**, market analysis, and **building and site design**.”*

Comments: In the last few years City Council has approved four residential developments in Port Dalhousie, all in proximity to one another, where none of them have reached completion so as to evaluate their impact on traffic (Port Tower complex, Harbour Club, 57 Lakeport, The Beaches). But yet the Niagara Region and City staff feel there is no need to conduct a traffic impact study for a fifth proposal, so we’ve been told. Traffic flow has already increased considerably over the last several year – to levels where crossing the street and backing out of a driveway have become unsafe. After hearing repeated concerns at the Open House meeting of Oct. 3rd, I strongly suggest City staff to consider a traffic impact study in this area, especially for Lakeport Road.

The neighbourhood is already experiencing the effects of inadequate parking from the Dental Office complex at 95 Lakeport Rd. At any given time of day cars are parked along Old Lakeshore Rd. and jam the rear of the adjacent plaza parking spaces. This proposal dictates the same scenario. It may

meet the requirements of a C2 complex (1 parking space per unit) BUT NOT a high density residential condo complex (1.25 spaces per unit). And it omits visitor parking all together!

Also, does this development meet the building and site design that will "better" the neighbourhood?; Is it compatible with the adjacent low-residential properties?


General Comments:

Clearly this submission has demonstrated that the 104 Lakeport Rd. proposal is not compliant with a Zone C2 development. The applicant is not only requesting Site Specific C2 provisions, but also to ignore the guiding principles that should be applied when considering a Community Commercial development, as set out in the *Garden City Plan* and *Zoning By-law*.

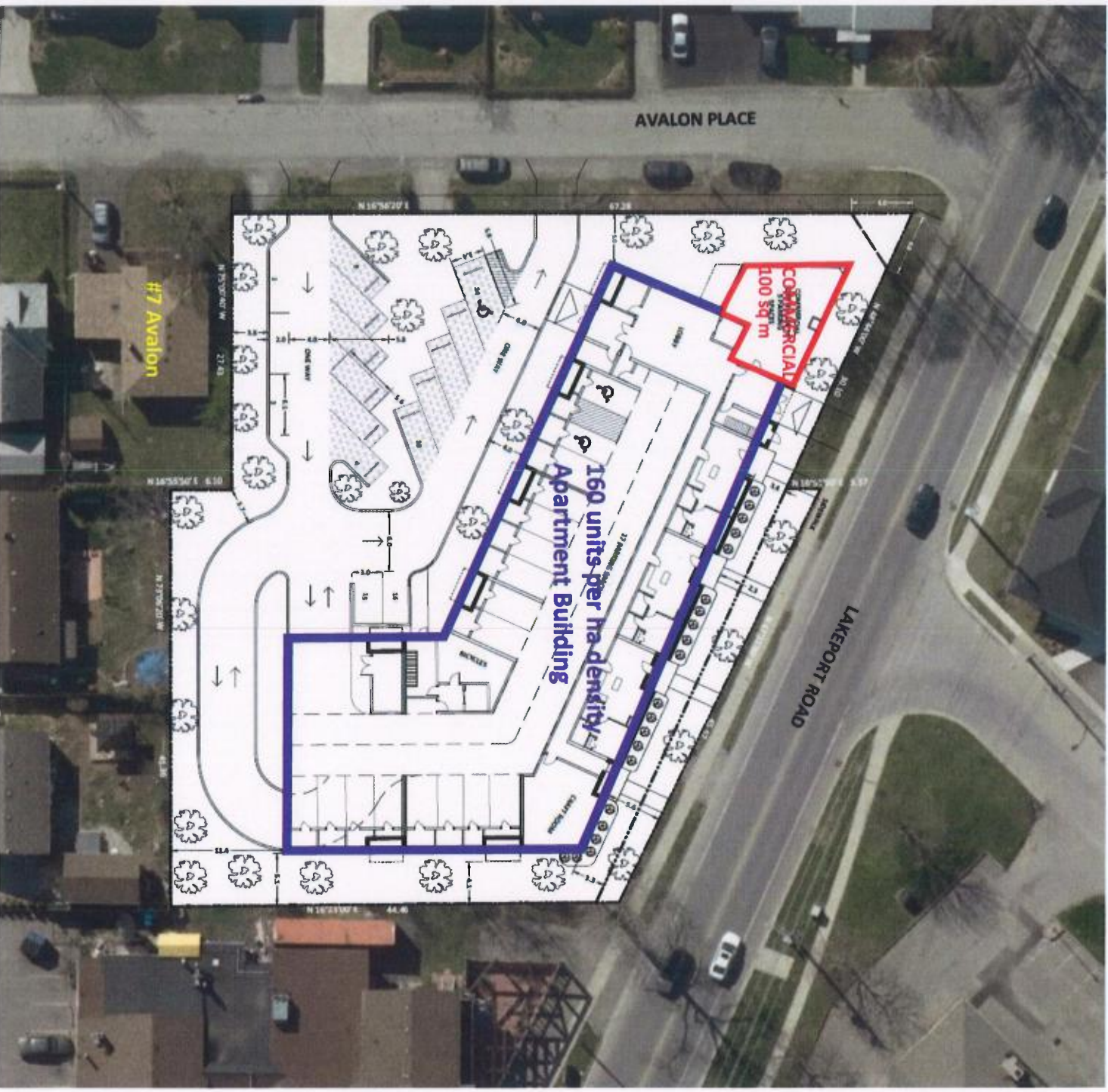
City staff make great efforts to include the general public to establish our land use planning policies that govern our future growth. Why should we take an interest and participate in these processes when it appears that amendments can be made so easily? (we have witnessed this many times of late with approved projects in Port Dalhousie.)

We have heard no argument from local residents that support this proposal. This was evident at the recent Open House meeting of October 3rd. I urge City councillors and planners to deny approval of this application and ensure this property is developed for the betterment of the neighbourhood, and that it adheres to the city planning principles they were established to achieve.

Respectfully submitted,



Colleen Beard



Blozowski, Kevin

From: Sue Fountain [REDACTED]
Sent: Monday, October 08, 2018 12:16 PM
To: Blozowski, Kevin
Cc: Mayor's Administration
Subject: Lakeport 102 /104, 3/5 Avalon Building Proposal

I live at 92 Lakeport Rd. and spoke at the Open House about my concern about traffic, safety and the infusion of even more vehicles in a small residential area. I question the use of Avalon as the entrance and exit for all vehicles visiting or parking there. There is only pedestrian accessibility from the major arterial road in the proposal.

How is it that this proposed high density 6 story is exempt from following the rules which require car accessibility from the only major artery, Lakeport Rd?

The fact that this project is in a quad of 3 dead end streets fronted by a major artery and NOT at an intersection should stop the whole project as outlined.

Snowplowing does not happen on Avalon often for at least 2 days after a storm. Access to Avalon is dangerous and unsafe for pedestrians and cars slipping on ice and snow. I am certain that the reason for the artery rule is in recognition of the need for safety in accessibility.

Emergencies happen during storms and rescue and services should be considered and addressed as part of any proposal of a high density complex.

Respectfully submitted
 Sue Fountain

Blozowski, Kevin

From: Hughes, Julie
Sent: Thursday, October 04, 2018 11:18 AM
To: Lesley Hirst
Cc: Blozowski, Kevin
Subject: RE: 5 Avalon Place rezoning

Good Morning Ms. Hirst,

On behalf of Mayor Sendzik, I'd like to acknowledge your email.

I'm copying in Kevin Blozowski, the planner assigned to 104 Lakeport Road/ Avalon. Mr. Blozowski is collecting all comments from the public for consideration prior to the staff report coming to Council.

The Mayor and council appreciate you making the effort to be engaged and forward your comments with respect to this development.

Warm regards,
 Julie

Julie Hughes
Administrative Assistant - Mayor's Office
Email: jhughes@stcatharines.ca
Tel: 905.688.5601 x1542

From: Lesley Hirst [REDACTED]
Sent: Wednesday, October 03, 2018 6:31 PM
To: Mayor's Administration
Cc: Stu Hirst; Garcia, Carlos; Williamson, Bruce
Subject: 5 Avalon Place rezoning

Hello
 Unfortunately neither my husband or I were able to attend tonight's public meeting re the rezoning of 5 avalon place. Therefore we wish for it to be recorded that we strongly oppose the rezoning.

The plaza at lake and lakeshore is unable to maintain vendors with shops empty so this proves there is no need for more commercial properties. There are other commercial properties empty along lakeshore and Ontario streetm again not viable and demonstrates the lack of need for this type of zoning.

The impact to the community is not acceptable with likely 6 to 8 story buildings which are out of character as well as the impacts: traffic, light, noise.
 There is already extensive development in Port with the Ranking legion and Lincoln fabrics buildings. This is money for the developers and not in the interest of the community.
 Please accept our opposition to this proposal and keep us informed of further developments.
 Thank you.

Lesley and Stewart Hirst

Click [here](#) to report this email as spam.

Blozowski, Kevin

From: Hughes, Julie
Sent: Friday, October 05, 2018 1:37 PM
To: mike sullivan
Cc: Blozowski, Kevin
Subject: RE: Concerning 102&104 Lakeport Road 3&5Avalon Place

Good Afternoon Mr. Sullivan,

On behalf of Mayor Sendzik, I'd like to acknowledge your email. I apologize if this is the second acknowledgement but I was away from the office and I want to ensure that your comments are in possession of the assigned planner, Kevin Blozowski to incorporate into the staff report on this proposed development.

Kevin is collecting all public feedback which will form part of the staff report which will come to council for consideration.

Many thanks for connecting.

Julie

Julie Hughes
Administrative Assistant - Mayor's Office
Email: jhughes@stcatharines.ca
Tel: 905.688.5601 x1542

From: mike sullivan [REDACTED]
Sent: Thursday, September 06, 2018 5:20 PM
To: Mayor's Administration
Subject: Concerning 102&104 Lakeport Road 3&5Avalon Place

Avalon has always been a quiet ,quaint single family residential neighbourhood .Our house is directly across from the proposed entrance .We aren't against the development of this property just against the immense size that a six storey condo would be on this parcel of land .Everybody on Avalon and Bernhardt would be in shade for a good portion of the day .We have parking on both sides of the street now would this change and would the street be overrun with cars parked from residents of this development because a lot of people have more than one car.Traffis is bad now on lakeport it is near impossible to turn left onto Avalon now you want another 50 to 100 cars to do just that .These are just some of our concerns

Mike &Val Sullivan 6Avalon Place St Catharines

Sent from my iPad

Blozowski, Kevin

From: Hughes, Julie
Sent: Friday, October 05, 2018 1:06 PM
To: Natalie Millhiser
Cc: Blozowski, Kevin
Subject: RE: For the attention of The Mayor and City Council

Good Afternoon Ms. Millhiser,

On behalf of Mayor Sendzik, I'd like to acknowledge your email and thank you for providing comment on this project.

I'm copying in Kevin Blozowski, the planner assigned to 104 Lakeport Road/ Avalon application. Mr. Blozowski is collecting all comments from the public for consideration prior to the staff report coming to Council.

The Mayor and Council sincerely appreciate you making the effort to be engaged and forward your comments with respect to this proposed development. All feedback will be considered prior to Council making their decision.

Warm regards,
 Julie

Julie Hughes
 Administrative Assistant - Mayor's Office
 Email: jhughes@stcatharines.ca
 Tel: 905.688.5601 x1542

From: Natalie Millhiser [REDACTED]
Sent: Thursday, October 04, 2018 3:52 PM
To: Sendzik, Walter
Subject: For the attention of The Mayor and City Council

Let me start by saying that we are not opposed to the development, just the size and driveways.

- As everyone knows Port Dalhousie is in a unique location. We have the beauty of the lake and beach and also Lakeside Park.
- To get down to Port there is Lakeport Rd and Main St. With all the new developments proposed there will be excessive traffic on those roads. Already trying to cross Lakeport is a task. At times on a weekday it could take someone almost 10 minutes to cross. Trying to get onto or off Lakeport in a car is just as futile. Cars

just won't stop to let you out. It hasn't gotten any better since school has started, the buses tie up the traffic at every stop.

- With this proposed development Avalon and Bernhardt will become traffic congested. Already we have delivery trucks traveling through our streets from the Rib Crib. There have been instances in the past where the truck has knocked down wires. Where will our friends and families park when they come to visit? I don't want to widen my driveway and destroy my gardens. These streets have no sidewalks or curbs, which we all agree works for us. Will there be road improvements such as curbs? At whose expense? Our taxes will go up and our property values will drop.

- Avalon and Bernhardt are not by definition collector or arterial roads they are local roads as outlined in The City of St. Catharines Official plan re- road Classification section 5.2

- Because road improvements (including road widening) can have a significant impact on the character of a neighbourhood, the City shall ensure that the design process will minimize the impact on the livability of affected residential areas by: a) Reducing the use of local roads by through traffic; b) Minimizing conflicts between local and through traffic. c) Minimizing any adverse impacts on the social and natural environments of adjacent lands.

- Character of the neighbourhood should be taken into consideration. Style design, scale and spacing should be uniform to the area. This proposed development will impact the style and spacing in the area. I fear that home values will drop but our taxes will increase. Children will have nowhere to play or ride their bicycles. Our privacy will be depleted along with the community feeling we now have.

- A six storey building will shade my house and my neighbours. At this moment there are trees which are only about two stories high and they shade my yard for the most of the morning. What will six stories do?

Also, why must all the traffic be routed down Avalon and Bernhardt, with the exit driveway right at my living room window. My privacy will be eliminated with cars constantly going out. This is not what I asked for when I purchased my home.

Please take into consideration our concerns. We are taxpayer and therefore we pay your salaries to look out for OUR interests. I have lived in my house since 1980 and my late husband and I had loved living here. I was hoping one of my children would buy it and raise their children here. Alas, none of them want to live across the street from a 6 storey condo which towers over them.

Thank You

Natalie Millhiser

4 Avalon Pl

Click [here](#) to report this email as spam.

Blozowski, Kevin

From: Hughes, Julie
Sent: Friday, October 05, 2018 8:47 AM
To: Yvan Seguin
Cc: Blozowski, Kevin
Subject: RE: Got the Mayor and City Council

Good Morning Mr. Seguin,

On behalf of Mayor Sendzik, I'd like to acknowledge your email and thank you for providing comment on this project.

I'm copying in Kevin Blozowski, the planner assigned to 104 Lakeport Road/ Avalon. Mr. Blozowski is collecting all comments from the public for consideration prior to the staff report coming to Council.

The Mayor and council appreciate you making the effort to be engaged and forward your comments with respect to this proposed development.

Warm regards,
 Julie

Julie Hughes
 Administrative Assistant - Mayor's Office
 Email: jhughes@stcatharines.ca
 Tel: 905.688.5601 x1542

From: Yvan Seguin [REDACTED]
Sent: Thursday, October 04, 2018 4:36 PM
To: Sendzik, Walter
Subject: Got the Mayor and City Council

I live at 4 Bernhardt which is a very small residential dead end street consisting of only 7 bungalows, on the south side of the proposed development of 102 and 104 Lakeport Rd.
 My main concern is the density of the proposed 6 story development with cars, parking and traffic impacting and changing the quality and nature of living on our quiet street.
 These issues need to be seriously addressed by Council . Please consider and approve a much smaller, less invasive proposal.

Yvan Seguin
 4 Bernhardt St.

Sent from my iPad

Blozowski, Kevin

From: Hughes, Julie
Sent: Friday, October 05, 2018 1:39 PM
To: Blozowski, Kevin
Cc: Councillors
Subject: Resident comment:
Attachments: lakeport, avalon pl dev't ap, losier.pdf

Hi Kevin,

Please see the attached letter from resident, Lee Anne Losier, 7 Avalon Place.

These comments are with respect to the proposed development for Lakeport Road/Avalon Place.
Please incorporate them into your staff report.

I'm copying Council as per the resident's request.

Julie

Julie Hughes

Administrative Assistant - Mayor's Office

Email: jhughes@stcatharines.ca

Tel: 905.688.5601 x1542

My name is Lee Anne Losier and live at 7 Avalon Place

RECEIVED
MAYOR'S OFFICE

Just want to give a quick overview of our neighbourhood.

OCT 05 2018
annal

On Avalon Place we currently have 13 homes. 9 bungalows ~~3 2-storey~~ and 1 2 -storey (deemed community commercial)

On Bernhardt we have 7 bungalows.

Our neighbourhood is small, established, affordable, and for the most part quiet, and people have moved here for that reason. We are in Port Dalhousie leading down on Lakeport Road to the core. We have several working families and retired and semi-retired folks amongst us.

The neighbourhood was well aware that 104/102 Lakeport and 3/5 Avalon were purchased. Some got letters asking if interested to sell, some were visited by the developer and some received annoying phone calls.

We are not opposed to progress, however seeing the proposal of a 63 unit "monster footprint plan" we had grave concerns as you have heard this evening with traffic, parking, elevation of building, privacy of adjacent buildings.

This proposed "monster footprint plan" is an oversized building on an undersized lot. They chose to put this "Footprint" to include one lot specifically #5 Avalon Place which is currently Residential. That is why we are here this evening as they want it changed to Commercial to reflect their proposed development.

Our City is always looking for affordable housing for our citizens so why would one think to tear down an affordable residential home to be replaced with a driveway and a concrete parking lot on a small established street?

We did speak with the Planning Department and it was confirmed 5 Avalon would be driveway and parking lot/spaces.

I want to read an excerpt from Niagara this Week in February Re: Stakeholders have their say on Infills

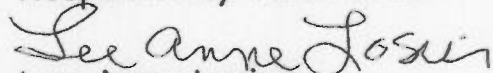
The concern from Policy Bruce Bellows was "some of the builds do not fit in well with the neighbourhoods they're going up in "

The interim bylaw passed on December 5 temporarily put a stop to multi unit development and creation of new lots on properties zoned R1 or low density residential.

Mayor Sendzik says "we have a job as elected officials to listen to the concerns of residents living in these neighbourhoods."

In closing I will reiterate...we are not opposed to development, however it should be with compatibility of existing neighbourhood and therefore would prefer to keep 5 Avalon zoned Residential.

Respectively Submitted


Lee Anne Losier

7 Avalon Place

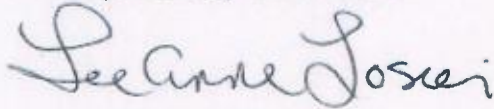
We have now participated in the Open House portion with the Planning Department & Developer. Currently, there is 1 empty seat, and 3 or 4 are running for Region on Council in the upcoming Municipal Elections October 22nd.

Based on fact that the current council is in a transitional mode, We believe it would only be fair to all concerned the Public Meeting be held in front of the new 4-year duly elected Council.

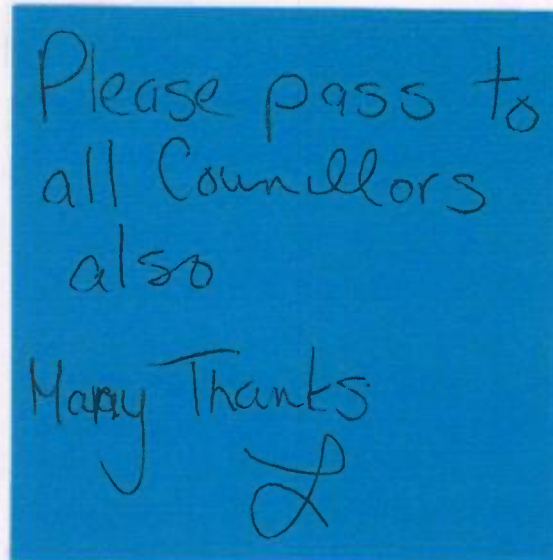
Regards to all seeking and holding a seat on Council!!!!

Thanking you in advance for your consideration.

Respectively submitted



Lee Anne Losier
7 Avalon Place



Please pass to
all Councillors
also
Many Thanks
L

Blozowski, Kevin

From: Dilts, Ann
Sent: Monday, October 15, 2018 9:37 AM
To: Blozowski, Kevin
Cc: Pihach, Judy; Riddell, Jim
Subject: FW: 104, 105 Lakeport Rd. 3 and 5 Avalon Place

Kevin - FYI

Ann Dilts

Office Manager

Email: adilts@stcatharines.ca

Tel: 905.688.5601 x1601

From: Nistico-Dunk, Bonnie
Sent: Monday, October 15, 2018 9:04 AM
To: Riddell, Jim ; Pihach, Judy ; Dilts, Ann
Subject: Fwd: 104, 105 Lakeport Rd. 3 and 5 Avalon Place

Sent from my iPhone

Bonnie Nistico-Dunk CMO, B.A.

City Clerk

Email: bdunk@stcatharines.ca

Tel: 905.688.5601 x1506

Begin forwarded message:

From: Pat [REDACTED]
Date: October 14, 2018 at 2:03:47 PM EDT
To: "mayorcouncil@stcatharines.ca" <mayorcouncil@stcatharines.ca>
Subject: 104, 105 Lakeport Rd. 3 and 5 Avalon Place

Dear Mayor and City Council,

I am a resident on Avalon Place and I am horrified at the proposal of the building that you are considering to build at the end of my street. This street is a quiet street with a lot of seniors. This quiet neighborhood is what drew me to this area. It is amazing that I can sit out on my front porch and if I close my eyes I would think I was in the country because of the lack of noise. Yes I do hear the traffic from Lakeport but very few cars come down the street unless they live here.

With this eyesore that you are so eager to approve the traffic on this street would increase. Not only that but the proposal also says that there are only enough parking for 1.5 cars per unit. You need to take a trip down Avalon and see that parking on the street is not really an option. If one person has a party on the street there would be no room to park. And with being that said you wouldn't be able to get an emergency vehicle down the street.

I also understand that there was traffic studies done to make sure that traffic wouldn't be an issue. All this was done when there is no other condos in Port Dalhousie. With the 3 being built now where is the correct traffic study?? I don't think that was taken in consideration was it? Have you ever tried to go down Ontario St. and try to make that turn onto Lakeport RD?? I have and I'll tell you it's frustrating. Your answer maybe certain times of the day! Yes when everyone is going to work or home or on the weekend. Heaven help us if it is in the summer or something is going on in Port.

So what I am asking is to show some consideration and hold this decision over to the next new elected council so that every avenue is discussed and considered. This building is out of place and too big on the site. Please don't make another mistake like the Port Tower and GM plant on Ontario. Let them decide if is then the right thing to do!

Pat and Art Hardy
11 Avalon Place

Sent from Mail for Windows 10

Click [here](#) to report this email as spam.

October 5, 2018

Mr. Kevin Blozowski, M.C.I.P, R.P.P, CAHP
Planning and Building Services
City of St.Catharines

Dear Mr. Blozowski:

102 and 104 Lakeport Road & 3 and 5 Avalon Place

As a follow up to the Public Meeting regarding the above-noted property on October 3, 2018, please accept this correspondence as my formal written comments.

To begin, the proposed use is too large for the site and does not meet the spirit and intent of the Official Plan; nor does it comply with the proposed zoning of the site. The proposal is essentially a high density residential proposal on an undersized lot.

A 6 storey building abutting Lakeport Road with minimal green space and setbacks will be incompatible with the neighbourhood and perhaps set a precedent for similar overdevelopment of small properties in the area. The architectural design of the building is also out of keeping with the surrounding neighborhood – the building looks institutional in nature.

This site - in particular - is unable to accommodate this scale of development because it is adjacent to two commercial uses that do not meet their parking requirements on site due to special exceptions approved by previous Councils.

The Rib Crib depends on parking on an adjacent lot, and the Dentist's office across the street does not meet the parking requirements of the zoning by-law and depends on considerable on street parking during the day.

In other words, the streets in this neighborhood are saturated and cannot accommodate the on-street parking the development will generate. The parking requirements for this proposal should align with the normal requirements for a multi-residential development – 1.25 spaces per unit, plus parking for the commercial component.

The very fact that this proposal cannot meet the performance standards of the zoning by-law with respect to setbacks confirms that this site is too small for this development; the density of the proposal is also well beyond what is appropriate for this small property, and the parking required by the proposed zoning of this site is not expected to be met on-site.

The preliminary site plan for the proposal suggests that the provision of on-site amenities and requirements such as garbage pickup, emergency access, children's play area, storm water drainage, etc. will be difficult, and at the expense of the limited green space on this site.

S. Robert Hazra, RPP

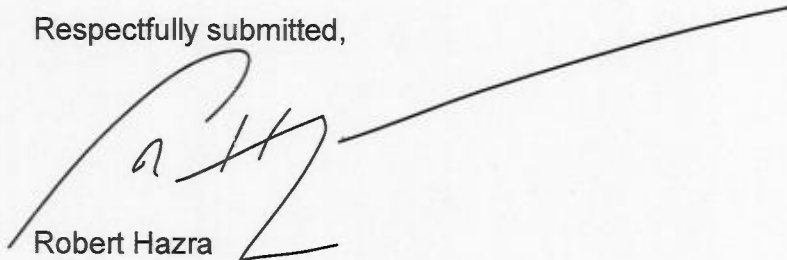
8 John Street, St. Catharines, Ontario L2N 4P1

If Council does choose to approve this development against the wishes of the neighbors, I would respectfully ask that Council seek a compromise and require a reduction in the number of units to reduce the lot coverage and increase the setbacks of this proposal, and also require the proponent to provide more parking on-site.

Neighbors should also be invited to participate in the site plan approval process as they are heavily impacted by this proposal.

Thank you for taking the time to consider these comments.

Respectfully submitted,

A handwritten signature in black ink, appearing to be 'R. Hazra', with a long horizontal line extending from the end of the signature.

Robert Hazra

Condo Proposal Concerns
September 06, 2018 Meeting
St. Catharines City Hall

*** Please note: This meeting was postponed until Wednesday, October 03, 2018 when I presented my concerns.

I have been a resident at 10 Bernhardt St. for 66 years. I was raised here. It has always been a very quiet neighbourhood and a great place to raise children.

The land, for this proposal, was previously owned by Mr. Long. It had a small orchard, a farmhouse and his business "Long Electric". All buildings were only 2 storey in height, like the surrounding neighbourhood. The thought of a 6 storey condominium building to be built here is outrageous. What are the developers and city councillors thinking? They certainly are not thinking of a peaceful neighbourhood.

My main concern, of many, is in regard to our ageing infrastructure and the potential harm it could cause, such as damage and flooding. Will it be upgraded prior to building? Who is financially responsible for the costs to upgrade and if there is flooding? (City or Developer)

The proposed property is presently a field which absorbs much of the rain. Environmentalists are telling us rain and winds are to increase in frequency and severity. This summer seems to be evidence of that fact. The proposal with all its concrete and asphalt will only compound the issue to our existing infrastructure.

What harm will be done to our existing roads and homes with the vibrations caused by the heavy equipment transporting dirt, cut trees, etc. when they dig the 2 floor underground garage? Our homes are made of plaster. What about the potential cracks caused from the vibrations? Who is financially responsible for the repairs? (Developer or Home owner)

There are already commercial buildings on each corner (Old Lakeshore Rd., Lakeshore Rd., Lakeport Rd. and Ontario St.). We do not need more commercial establishments in the area. Where would anyone park? In the proposal there does not appear to be access to the 5 parking spots at the corner of Avalon Rd. and Lakeport Rd.

The crisis right now in the city is affordable housing. As condos are built, the cost of buying one is **not** for the middleclass buyer. It is for the wealthier purchaser. Would the Developer consider building a Townhouse complex, which would be much more suitable for the area? It would give the middleclass buyer some hope. It would also maintain our quiet neighbourhood.

- "Shadowing" and "Loss of Privacy" concerns were listed on Pg. 31 of the 57A, 59A, 61, 63 Lakeport Rd. Proposal and they certainly apply here. Neighbours and residents, as far away as Philpark Rd. & John St., are concerned about privacy do to the height of the building proposed. There is no home or building in this neighbourhood that is higher than 2 storeys.
- Increased traffic impact is a concern listed on Pg. 39 of the 57A, 59A, 61, 63 Lakeport Rd. Proposal and they certainly apply here as well. Already it is difficult to enter or exit Lakeport Rd. from Avalon St. and Embassy Dr. What happens to accessibility when the two approved proposals in the Port Dalhousie core begin construction?
- **What is the hurry?** Would it not be prudent to wait until the other two approved proposals are built, so traffic impact, noise, street accessibility and any other issues which may arise can be determined and dealt with prior to approval of this proposal? Winter is coming, which only enhances these concerns. It may even be economically sound.
- Since the present Council is in a "lame duck" situation, because of the October 22, 2018 election, this proposal should be dealt with, in my opinion, by the new Council.

I thank you for allowing me this time to express a few of my concerns.

Respectfully submitted,

Barbara Munroe

beam@vaxxme.com

10 Bernhardt St.

**102 and 104
Lakeport Road and
3 and 5 Avalon
Place
Public Submissions
Received
November 2, 2018**

Blozowski, Kevin

From: Conrad Losier [mailto:conrad.losier@cityofvaldosta.com]
Sent: Friday, November 02, 2018 7:54 AM
To: Mayor & Council; Blozowski, Kevin
Cc: Sue Fountain
Subject: 104/102 Lakeport Road 3/5 Avalon

Mayor, Councillors and Kevin Blozowski

Our neighbourhood still remains concerned regarding the compatibility of this 6 storey condo footprint on a low density residential street.

The Planning Department will be releasing their report for the above proposal on November 5th, to be heard at City Hall November 12th. This is only a 1 week time span.

Our neighbourhood has been working hard and diligent on this proposal and are seeking a deferral in order to prepare full substantial responses to the report.

Thanking you in advance for consideration.

Respectively Submitted

Lee Anne Losier
7 Avalon Place

Sent from [Mail](#) for Windows 10

Click [here](#) to report this email as spam.

Blozowski, Kevin

From: Colleen Beard [REDACTED]
Sent: Friday, November 02, 2018 8:41 AM
To: Mayor & Council
Cc: Mayor's Administration; Blozowski, Kevin; Sue
Subject: 104 Lakeport Rd - request to postpone decision on application
Attachments: Petition_letter.pdf; Petition-signed.pdf

Dear Mayor Sendzik and City Council members,

Please accept this submission (see documents attached) on behalf of concerned residents in the Avalon/Lakeport roads neighbourhood to consider postponement of discussion and voting on the proposal for reasons outlined.

Colleen Beard
91 Lakeport Road

Click [here](#) to report this email as spam.

Nov. 1, 2018

TO: Mayor Sendzik, City of St. Catharines
CC: City Councillors, and City Planning staff

**RE: November 12th Council meeting agenda item: development application for
102/104 Lakeport Rd & 3 Avalon Place**

Please accept this petition as our request to consider postponing discussion of this proposal to: provide reasonable time for residents to prepare a response, AND until the newly elected council is in place.

The developers have been given ample time to consult with the City to formulate their proposal over the last 9 months. It is only fair that the citizens, whom are directly affected by this proposal, also have time to do their work and prepare a comprehensive response.

This proposed development was first presented to the general public at an Open House meeting on October 3. Since then, we have been informed that the City staff recommendation will be available November 5, and a meeting of Council has been scheduled to discuss the proposal as soon as November 12. The residents of Avalon, Bernhardt, Lakeport roads, and beyond, feel that seven (7) days (or 4 days for written submissions) is not a reasonable amount of time to prepare a well-informed response.

You have pointed out in an earlier response to our request (Oct. 12) the importance of the 180 day timeline for the City to make a decision, that is fast approaching. However, we were also informed at the Open House meeting that this deadline lapsed long ago. Therefore, the urgency to hold the public Council meeting on Nov. 12th to fulfill this mandate is a null factor.

Given that there are 26 documents to review (21 application docs; Official Plan; Zoning by-laws; and the City staff recommendation), there is a lot for the residents to digest and organize in order to formulate a comprehensive response to Council.

As well, 5 of the 12 Councillors are leaving council as of early December. It would be prudent to allow newly-elected councillors to participate in the process and to make a well-informed decision about the future growth of our city.

**PETITION TO City of St. Catharines Mayor and City Council to
Postpone voting on the development application for 102/104 Lakeport
Rd & 3 Avalon Place**

We, the undersigned, are opposed to the scheduling of the discussion Re: development application for 102/104 Lakeport Rd & 3 Avalon Place at the November 12th Council meeting. We call on St. Catharines City Council to please:

***Postpone discussion and voting on this application to provide
reasonable time for residents to prepare a response, AND until the
newly elected council is in place.***

This proposed development was first presented to the general public at an Open House meeting on October 3. We have been informed that the City staff recommendation will be available November 5, and a meeting of Council has been scheduled to discuss the proposal as soon as November 12. The residents in the immediate neighbourhood directly affected by this proposal are requesting the meeting be postponed since seven (7) days is not a reasonable amount of time to prepare a response.

NAME	ADDRESS	SIGNATURE
KAREN ALLEN	3 BERNHARDT	<i>K. Allen</i>
Marilyn Wallace 72 Philpark		<i>M. Wallace</i>
BARBARA MUNROE	10 BERNHARDT ST.	<i>Barbara Munroe</i>
<i>Catherine Champagne</i>	<i>6 Embury</i>	<i>Catherine Champagne</i>
<i>Keith Bradley</i>	<i>" no email</i>	<i>Keith Bradley</i>
YVAN SEGUIN	4 BERNHARDT ST.	<i>Yvan Seguin</i>
Diane Nay	585 Ontario St.	<i>Diane Nay</i>
Marjorie McCullough	585 Ontario St	<i>Marjorie McCullough</i>
MIKE DUHACHEL	2 PHILPARK RD.	<i>M. Duhack</i>
LESLIE Duhack	" "	<i>L Duhack</i>
W. Duhack 17 PHILPARK		<i>W. Duhack</i>
Alan Holder ⁰³⁶⁰ _{outlook.com}	8 Bernhardt St	<i>Alan Holder</i>
Sarah Douse ⁰³⁶⁰ _{outlook.com}	2 Bernhardt St	<i>S Douse</i>
John Keddy	"	

**PETITION TO City of St. Catharines Mayor and City Council to
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NAME	ADDRESS	SIGNATURE
SUE FOUNTAIN	92 LAKEPORT RD	Sue Fountain
GAIL FRAPPIER	92 LAKEPORT RD	Gail Frappier
CELENE BRAND	92 LAKEPORT RD	Celine Brand
URSULA MAUSBERG	89 LAKEPORT RD.	Ursula Mausberg
RICK BROUGHTON	87 LAKEPORT RD.	Rick Broughton
KRISTINE BROUGHTON	87 LAKEPORT RD	Kristine Broughton
VINCE BENVENUTI	88 LAKEPORT RD ^{UNIT 30}	Vince Benvenuti
BEN DE-JONG	3 OLD LAKESHORE BLVD	Ben De-Jong
DIANNE DEJONG	3 OLD LAKESHORE BLVD	Dianne DeJong
LIH DEJONG	50 OLD LAKESHORE BLVD	Lih DeJong
DIRK DEJONG	50 Old Lakeshore Blvd	Dirk DeJong
MIC NEUFIELD	109 OLD LAKESHORE RD	Mic Neufield
K. G. + L. L. L. L.	8 John St.	K. G. + L. L. L. L.
SHAMPA SENGUPTA	8 John St.	S. Sengupta

**PETITION TO City of St. Catharines Mayor and City Council to
Postpone voting on the development application for 102/104 Lakeport
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NAME	ADDRESS	SIGNATURE
Natalie Millhaer	4 AVALON PL	Natalie Millhaer
LeeAnne Hosier	7 Avalon Pl	PROXY: Natalie Millhaer
Conrad Hosier	7 Avalon Pl	PROXY: Natalie Millhaer
Gary Versluis	9 Avalon Pl	Gary Versluis
Kim Versluis	9 Avalon Pl	Kim Versluis
Ellaine G. Trottier	14 Avalon Pl	Ellaine G. Trottier
Frankie Brown	10 Avalon Pl	Frankie Brown
Pat Hardy	11 Avalon Pl	Pat Hardy
Art Hardy	11 Avalon Pl	Art Hardy
KIMILA WILLS	10 AVALON PL	Kimila Wills
CHIM WILLS	10 AVALON PL	Chim Wills
Kary SWIERCZ	10 JOHN ST	Kary Swiercz
LARRY SWIERCZ	10 JOHN ST	Larry Swiercz
KRISTY SHOCK	12 AVALON PL	Kristy Shock
Margaret Salisbury	11 Bernhardt St	Margaret Salisbury

PROXY PERMISSION

Sue Fountain

From: "Conrad Losier" [REDACTED]
Date: October-20-18 6:34 AM
To: "Sue Fountain" [REDACTED]; "Natalie M" [REDACTED]; "Colleen Beard"
Subject: Petition

I cannot sign this petition as it is delivered to my Cogeco address.....you certainly have our permission to sign for us both Conrad and myself.

Conrad Losier Lee Anne Losier

If you are uncomfortable signing, Natalie will!
You all have been working diligently!

Thanks guys.....WE CAN DO THIS!!!!!!!!!!

Sent from for Windows 10



20/10/2018

Blozowski, Kevin

From: Sarah Dowse [redacted]
Sent: Friday, November 02, 2018 10:22 AM
To: Mayor & Council; Blozowski, Kevin
Cc: [redacted]
Subject: Condo Development on Lakeport Road

To whom it may concern,

My family and I live at 2 Bernhardt Street in St. Catharines which we moved into 11 years ago. I am writing to show our concern for the large condo building being proposed for Lakeport Road.

The traffic turning onto Lakeport from Avalon and Embassy is already hectic. Turning left is almost impossible on a weekend, and the weekday traffic has also increased drastically over the past couple of years.

When that area gets congested, vehicles think they can skip the intersection and cut the corner by speeding down Avalon and along Bernhardt. With the added congestion and parked cars on those streets, due to not having enough onsite parking spaces in the proposed plan, someone is going to get hurt.

We are not opposed to something being built on that property, just the idea of adding something so drastic in size along with the amount of vehicles it will then bring to the area, will really negatively effect the quaintness of our little community that we have grown to love and call our home.

We are hoping there is some compromise in what is built on that lot to make it work for the residents and for the city.

Thank you for your time,

Sarah Dowse

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Corporate Report

Report from Planning and Building Services, Planning Services

Date of Report: January 18, 2019

Date of Meeting: January 28, 2019

Report Number: PBS-018-2019

File: 60.46.447

Subject: *Planning Act Continuation of Public Meeting – Recommendation Report*
Application for Draft Plan of Vacant Land Condominium; 125 Rykert Street;
Owner: 2575115 Ontario Inc.; Agent: Better Neighbourhoods Inc.

Recommendation

That Council approve the Draft Plan of Vacant Land Condominium for the lands described as Part Lot 21 CON 8 Grantham designated as Part 1 on 30R6366, municipally known as 125 Rykert Street, showing 10 townhouse dwelling units, together with common elements containing parking, landscaping, and servicing, as illustrated in Appendix 1 of this report, subject to the Conditions of Draft Plan of Vacant Land Condominium Approval, as outlined in Appendix 2 of this report; and

That the Notice of Decision required by the Planning Act, R.S.O. 1990, c.P. 13, as amended, be processed by staff; and

That the Notice of Decision include a statement that public input has been received, considered, and has informed the decision of Council; and

That upon expiration of the appeal period, staff be directed to forward any appeals to the Provincial Local Planning Appeals Tribunal (LPAT) (formerly the Ontario Municipal Board) for consideration and final approval; and

That, in the event of an appeal to the Local Planning Appeal Tribunal (LPAT), Council authorize the City Solicitor or her designate and City staff, as appropriate, to attend any LPAT hearing in support of Council's decision; and

That after notice of Council's decision has been given, the Clerk be authorized to endorse the plan as "Draft Approved" on the day after the appeal period has expired, in accordance with the Planning Act, provided that no appeals have been lodged; and

Further, that the Clerk be directed to make all necessary notifications. FORTHWITH

Summary

This application seeks approval of a Draft Plan of Vacant Land Condominium for condominium tenure for a development proposing ten townhouse dwelling units within a private road development and common element area for the private road, parking,

landscaping and servicing, as illustrated in Appendix 1 of this report. Condominium approval will allow each of the 10 dwelling units to be sold individually.

The private road development is currently being reviewed through the Site Plan Approval process and will be subject to a Site Plan Agreement. This application for Draft Plan of Vacant Land Condominium approval relates to tenure (ownership) of the units only. Should Council approve the subject application for Draft Plan of Condominium, the Condominium Corporation will be subject to the applicable Site Plan Agreement.

Staff is recommending approval of this Draft Plan of Vacant Land Condominium, subject to the Conditions of Draft Plan Approval outlined in Appendix 2 of this report. The proposal conforms to the relevant Provincial land use policies and Regional and local Official Plan policies.

Background

On January 14, 2019, Council received an [Information Report](#) (linked here and available upon request) on this Application for Draft Plan of Vacant Land Condominium Approval, which provided an overview of the proposed development, location and site description, planning context and policies, circulation comments and concerns that arose from public consultation.

The applicant previously submitted an application to the Committee of Adjustment for Consent (B-39 / 18SC) to adjust the boundary of the subject lands to facilitate a proposed private road development consisting of ten townhouse dwelling units. The Consent resulted in a boundary adjustment with the adjacent lands to the east, known as 121 Rykert Street. A portion of the lands from 121 Rykert Street was added to 125 Rykert Street. Each property previously contained a detached dwelling unit, which has since been demolished.

The Committee of Adjustment considered an associated application for Minor Variance (A-73 / 18) which was made to request a reduction in lot area per unit and various reduced setbacks within the proposed private road development. Both the Consent and Minor Variance applications were approved and no appeals were received. No members of the public provided any comments or objections at that time.

A site plan application has been submitted and is currently in the final stages of review and approval.

This Recommendation Report provides Council with a planning analysis and staff recommendation on the application.

Report

Proposed Development

The Draft Plan of Vacant Land Condominium application proposes to create ownership tenure for a development proposal of ten residential units within a private road

development, as well as a common element area for the private road, visitor parking, landscaping and servicing, as outlined in Appendix 1.

The development has one access from Rykert Street, and fourteen parking spaces internal to the site. The applicant has requested Draft Plan of Vacant Land Condominium Approval so that each of the proposed ten units can be sold individually. The development is currently under review through the Site Plan Approval process and a Site Plan Agreement is being drafted. Once approved, the Agreement will be registered on title of the lands.

Draft Plan of Condominium

Section 51(24) of the Planning Act guides staff in assessing plans of condominium, as described in the Information Report, which was considered at a Public Meeting under the Planning Act on January 14, 2019. To that end, the following analysis is provided.

- The development is consistent with and does not conflict with matters of provincial interest, it is not premature and is in the public interest. The parcel is zoned and permits residential uses, including private road developments and contributes to the City's growth which is supported and encouraged by the Growth Plan and Provincial Policy Statement.
- The proposal complies with the Official Plan, and the lands are suitable for development as proposed. The proposed density is generally in keeping with low density thresholds intended for this site, and the site contains adequate parking, access and landscaping.
- The adequacy of existing roads in the vicinity of the development is sufficient. The development is located on Rykert Street which is designated a Collector Road in the Official Plan. The development also proposes a private road, which has adequate width to support a fire route and two-way traffic. These roads are considered adequate to support traffic generated by the subject development.
- The proposed development is compatible with the surrounding land uses. The surrounding uses are residential, institutional, commercial and natural areas. The buildings proposed are to be two storey townhouse dwellings. The visual impact of the building massing is considered to be compatible with surrounding existing uses, while introducing an additional dwelling type of townhouse units.
- The size of the units is appropriate for the residential use and contributes to a mix of housing in the City.
- The existing utilities and services are adequate to support the development.
- A road widening along Rykert Street is to be dedicated to the City through the Site Plan Approval process.

Site Plan Control

Staff recommend approval of draft plan of condominium, subject to number of conditions as outlined in Appendix 2. Firstly, the approval and registration of a site plan is required prior to final condominium approval and, secondly, additional securities may be required prior to final condominium approval should certain site works not be completed in full.

These conditions are standard conditions for draft plan of condominium approvals.

The City's Site Plan Control By-law requires that residential developments of four units or more be subject to site plan control. Site plan control is the City's principal tool through which staff can implement design considerations to maximize compatibility with the surrounding neighbourhood and ensure the long-term maintenance and functionality of the site. As part of the process, a site plan agreement is registered against the title of the lands and security deposits are collected to ensure the terms of that agreement are adhered to.

A site plan application is under review, and an Agreement is being drafted which shall be registered on title of the subject lands. All development on site shall be in compliance with the Registered Site Plan Agreement. All agreements shall be complied with prior to final condominium registration.

Affordable Housing

While Council considers affordable housing a priority concern for the City, and the City's Official Plan recognizes the need to provide for affordable housing, actual mechanisms to require and sustain an affordable housing supply in the City, as per the definition in the Provincial Policy Statement 2014, are not currently in place. As such, at this time, applicants are *encouraged* to provide affordable housing, however it is not required.

The applicant for this project has advised that none of the proposed units are intended to meet the definition of affordable housing as defined by the Provincial Policy Statement 2014. Further, the applicant has confirmed there is no partnerships with community housing organizations, such as Niagara Regional Housing, Bethlehem Place, Habitat for Humanity or similar housing agency.

Council should be aware that the application is for condominium tenure, but approval does not necessarily suggest that the units, once constructed, are owner occupied. It is not unusual for condominium units to become investment properties for rental purposes. Additional availability of residential ownership units and/or rental units in the ownership / rental market serves to increase the inventory of residential accommodation and thereby increase the availability of dwelling units. The units may not be affordable pursuant to the PPS definition, but by increasing the inventory of available dwelling units, pressure is exerted in the market place for pricing to be more competitive.

Public Meeting

In accordance with the Planning Act, a Public Meeting was held by Council on January 14, 2019. Staff from Planning and Building Services presented an Information Report. The applicant's agent was in attendance. No members of the public spoke to the application and there were no questions raised at the Public Meeting. The Public Meeting was adjourned.

Financial Implications

All costs associated with this development will be borne by the developer. No direct financial impacts to the City are triggered should Council approve the subject application.

Relationship to Strategic Plan

Economic Sustainability

Draft approval of this vacant land condominium will support the goals of economic sustainability by:

- Facilitating private investment through development in the City (Goal 1).

Potential Appeals

Council should be aware that, effective April 3, 2018, certain land use appeals in Ontario are subject to new legislation. For draft plan of condominium applications, the appeal process has not been significantly revised and largely reflects the process in effect prior to April 3, 2018. Any appeals to draft plans of condominium are submitted to the Local Planning Appeal Tribunal (LPAT) which replaces the former Ontario Municipal Board (OMB).

Second Planning Opinion Advisory

Should Council consider not supporting the staff Recommendation provided in this report, Council is advised to defer its decision until such time as a second planning opinion from an outside consultant can be obtained. In the event the second planning opinion is supported by Council, and Council makes a decision based on that second planning opinion, and if and when the matter should be heard before the Local Planning Appeal Tribunal, then the planner who has provided the second opinion shall be retained for the purpose of a hearing before the Local Planning Appeal Tribunal.

Conclusion

In summary, staff recommends that approval be granted to this Draft Plan of Vacant Land Condominium, as illustrated in Appendix 1, subject to the Conditions of Draft Plan Approval outlined in this report and included as Appendix 2. A plan of condominium will allow each of the 10 townhouse dwelling units to be sold individually. The proposal is consistent with Provincial, Regional and local Official Plan policies, and is in accordance with all planning approvals already granted.

Notification

It is in order to advise Dan Romanko of Better Neighbourhoods Inc., 190A Ontario Street, St. Catharines ON, L2R 5K9, the owner's agent.

Prepared by:

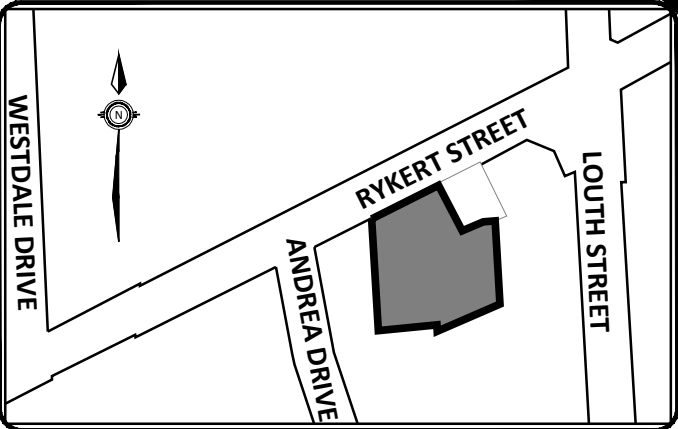
Taya Devlin
Planner I

Submitted by:

Judy Pihach, MCIP, RPP
Manager of Planning Services

Approved by:

Tami Kitay, MPA, MCIP, RPP
Director of Planning and Building Services



KEY MAP - NOT TO SCALE

LAND USE SCHEDULE		
		AREA
UNIT 1 - 10	PROPOSED TOWNHOUSES	0.16 ha
BLOCK 11	COMMON ELEMENT	0.11 ha
TOTAL AREA		0.27 ha

METRIC NOTE

DISTANCE SHOWN ON THIS PLAN ARE IN METERS AND CAN BE CONVERTED TO FEET BY DIVIDING 0.3048

REQUIREMENTS OF SECTION 51(17) OF THE PLANNING ACT

- (a) SEE PLAN

(c) SEE PLAN

(e) SEE PLAN

(g) SEE PLAN

(i) SILTY CLAY

(k) FULL SERVICED
- (b) SEE PLAN

(d) SEE PLAN

(f) SEE PLAN

(h) MUNICIPAL

(j) SEE PLAN

(l) SEE PLAN

LEGAL DESCRIPTION

PARTS 3 AND 4 OF PLAN 30R-15273 BEING PART OF LOT 21, CONCESSION 8, GEOGRAPHIC TOWNSHIP OF GRANTHAM, CITY OF ST. CATHARINES, REGIONAL MUNICIPALITY OF NIAGARA

OWNER'S CERTIFICATE

BEING THE REGISTERED OWNER, I HEREBY AUTHORIZE BETTER NEIGHBOURHOODS INC. TO PREPARE AND SUBMIT THIS DRAFT PLAN OF VACANT LAND CONDOMINIUM TO THE CITY OF ST. CATHARINES.

SURVEYOR'S CERTIFICATE

I HEREBY CERTIFY THAT THE BOUNDARIES OF THE LANDS TO BE SUBDIVIDED AND THEIR RELATIONSHIP TO THE ADJACENT LANDS ARE ACCURATELY AND CORRECTLY SHOWN ON THIS PLAN.

ROY S. KIRKUP, O.L.S. DATE

#	DATE	REVISIONS	
0	2018 10 26	FOR DP SUBMISSION	KH/MK

b

better neighbourhoods

DEVELOPMENT CONSULTANTS

190A Ontario Street, Ontario L2R 5K3
Office: 905.684.8585 betterneighbourhoods.ca

PROJECT TITLE:

ASPEN COMMON
St. Catharines, ON

DRAWING TITLE:

DRAFT PLAN OF
VACANT LAND
CONDOMINIUM

DATE OF ISSUE:

2018 10 26

DRAWING No:

0257 - DP1

REV. NO.

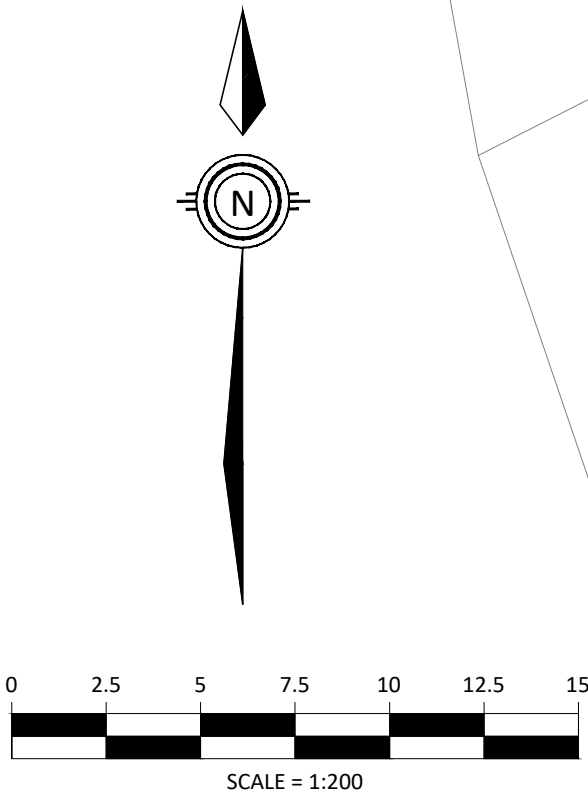
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LOUTH STREET

RYKERT STREET

ANDREA DRIVE



**Conditions of Draft Approval
125 Rykert Street
File No. 60.46.447**

General Approval

1. That this approval applies to lands described as Part Lot 21 CON 8 Grantham designated as Part 1 on 30R6366; City of St. Catharines, municipally known as 125 Rykert Street, for a draft plan of vacant land condominium prepared by Better Neighbourhoods Inc., dated October 26, 2018, and showing a 10-unit private road development.

Site Plan Agreement

1. That prior to final approval of the plan of condominium, the owner shall enter into a Site Plan Agreement with the City of St. Catharines, which shall be registered against the title of the lands. The Site Plan Agreement shall address certain matters including, but not limited to landscaping, fencing, parking, lighting, waste collection, road widening and servicing.

(City of St. Catharines)

2. The site plan agreement shall require, prior to final approval of the plan of condominium, that the Owner shall deposit securities in the amount of 100% of the estimated cost of all works that are incomplete and which comprise part of a common element.

(City of St. Catharines)

Administration

3. That if final approval is not given within three years of the approval date and no extension has been granted, draft approval shall lapse. If the owner wishes to request an extension to the draft approval period, a written explanation with reasons why the extension is required must be received by the City prior to the lapsing date. An updated review and revisions to the conditions of approval may be necessary at that time.

Prior to granting approval to the final plan of condominium, the City of St. Catharines shall be satisfied that all conditions herein have been satisfactorily met.



Corporate Report

Report from Planning and Building Services, Planning Services

Date of Report: January 4, 2019

Date of Meeting: January 28, 2019

Report Number: PBS-017-2019

File: 60.35.1045

Subject: *Planning Act Continuation of Public Meeting – Recommendation Report*
 Application to Amend Zoning By-law 2013-283
 Subject Lands: 10 Benfield Drive
 Owner: Mountainview Belmont Ridley Heights Inc.
 Additional Subject Lands: located opposite the intersection of Louth Street
 and Crestcombe Road (no assigned municipal address)
 Owner: Ministry of Transportation of Ontario
 Agent: Zelinka Priamo Ltd.

Recommendation

That Council approve an amendment to the City of St. Catharines Zoning By-law 2013-283, as outlined in Appendix 5 of this report, to permit a residential private road development comprising approximately 28 townhouse dwelling units and 192 apartment dwelling units within four 4-storey buildings on the subject lands illustrated in Appendix 1 of this report, municipally known as 10 Benfield Drive and additional lands with no assigned municipal address located opposite the intersection of Louth Street and Crestcombe Road; and

That the Mayor and City Clerk be authorized to execute the necessary By-law to give effect to Council's decision; and

That the Notice of Decision required by the Planning Act, R.S.O. 1990, c.P. 13, as amended, be processed by staff; and

That the Notice of Decision include a statement that public input has been received, considered and has informed the decision of Council; and

That upon expiration of the appeal period, staff be directed to forward any appeals to the Local Planning Appeal Tribunal (LPAT) for consideration and final approval; and

That, in the event of an appeal to the Local Planning Appeal Tribunal (LPAT), Council authorize the City Solicitor or her designate and City staff, as appropriate, to attend any LPAT hearing in support of Council's decision; and

Further, that the Clerk be directed to make all necessary notifications. FORTHWITH

Summary

This Recommendation Report considers an Application for Zoning By-law Amendment related to the privately owned lands located at 10 Benfield Drive as well as a parcel of land owned by the Ministry of Transportation located opposite the intersection of Louth Street and Crestcombe Road and having no assigned municipal address (Appendix 1). The Applicant is requesting that the zoning of the subject lands be changed from Major Commercial (C4) to Medium / High Density Mixed Use (M2) in order to permit a 220-unit residential private road development on the lands. The Applicant also requests that a Special Provision be applied to address certain site specific matters, including, but not limited to building height, setbacks, and driveway widths. The proposed 220 dwelling units represents a density of 87 units per hectare or 202 people per hectare.

This Recommendation Report follows the [Information Report](#) (PBS-236-2018 linked here and copy available upon request) that was received by Council at the December 10, 2018, Public Meeting. At that time, Council did not close, rather they adjourned the Public Meeting.

Staff is recommending approval of the Application, on the basis that it has regard for matters of provincial interest, is consistent with the Provincial Policy Statement, conforms to and does not conflict with the Growth Plan for the Greater Golden Horseshoe, and conforms to and does not conflict with both the Region of Niagara's Official Plan and the City of St. Catharines Official Plan, subject to the staff recommendations herein.

Background

On May 7, 2018, Council adopted the GO Transit Station Secondary Plan (GTSSP) and implementing Official Plan Amendment No. 19 (OPA 19). Subsequently, the Region of Niagara approved the GTSSP on July 5, 2018. OPA 19 is now in force and effect, however Planning staff have not yet initiated the zoning by-law amendment process that is necessary to implement the new official plan policies. OPA 19 amended the land use designation of the subject lands from Commercial to Mixed Use.

On December 10, 2018, Council received an Information Report respecting this Application for Zoning By-law Amendment, which provided an overview of the Application, location and site description, planning policy context, comments received on the Application from staff, outside agencies, and the public, and a detailed description of the Application process to-date.

Report

The subject lands are located within the GO Transit Station Secondary Plan Area in the City's West Planning District. Specifically, the lands are situated at the southeast corner of Benfield Drive (a local road) and Louth Street (a Regional arterial road) and are served by public transit. A location map is attached as Appendix 1. Additional details of the site and surrounding area can be found in the previous Information Report.

Proposed Development

The Applicant has submitted a concept site plan (Appendix 2), which illustrates how the subject lands could be developed for a mix of residential uses. Four blocks of townhouse dwellings (28 units) are situated along the south side of a private road that bisects the lands from Benfield Drive to the west through to Louth Street to the east. An 8.0 metre wide noise attenuation barrier, including a berm and fencing, is proposed between the private rear yards of the townhouse dwellings and adjacent existing industrial uses to the south. Four 4-storey apartment buildings frame the street edge along Benfield Drive and Louth Street. Three of the buildings will include 50 apartment dwelling units, while the fourth will include 42 dwelling units. In total, 220 dwelling units are proposed at a density of 87 units per hectare or 202 people per hectare.

The concept elevations and massing model submitted in support of this Application are appended to the previous Information Report as Appendices 3, 4, and 5.

Circulation of Application

This Application was circulated to all relevant departments and agencies in accordance with the requirements of the Planning Act; there were no objections received. The previous Information Report summarized comments received on the Applicant's original submission. In response to comments received the Applicant has submitted additional information, including an odour study and an addendum to the noise feasibility study. This information was circulated to Niagara Regional planning staff for review, who confirm they are satisfied with the consultant's conclusions and recommendations.

Planning Analysis

Recent changes to the Planning Act require Council to fully consider planning applications in the context of consistency with the Provincial Policy Statement, and conformity with Provincial Land Use Plans (in this case the Growth Plan for the Greater Golden Horseshoe, also known as Places to Grow), the Regional Official Plan, and the City's Official Plan, the Garden City Plan. Staff recommendations have been formulated accordingly. The Ontario Land Use Planning Framework applicable to this Application is outlined in Appendix 3 of this report.

Provincial Policy Statement

The Provincial Policy Statement, 2014 (the PPS), provides direction on matters of Provincial interest related to land use planning and development. Section 3(5) of the Planning Act stipulates that decisions of Council "shall be consistent" with the PPS.

The subject lands are located within a settlement area under the PPS. Sections of the PPS, which are particularly relevant to the subject proposal, are summarized below.

Settlement Areas

The PPS requires settlement areas to be the focus of growth and development, with land use patterns based on densities and a mix of land uses that efficiently use land, resources, and infrastructure, and which are transit supportive and support active

transportation, among other matters. A range of uses and opportunities for intensification and redevelopment are to be provided, with planning authorities having the ability to identify suitable locations for growth, subject to appropriate development standards. Minimum intensification targets and redevelopment within settlement areas are to be established.

Land Use Compatibility

The PPS requires that when planning for major facilities (e.g. industries) and sensitive land uses (e.g. residential dwellings), appropriate design, including buffering and/or separation, should be implemented to prevent or mitigate adverse effects from odour and noise, minimize risks to public health and safety, and to ensure the long-term viability of major facilities.

Housing

The Housing policies of the PPS require that planning authorities plan for an appropriate range and mix of housing types and densities to meet the needs of current and future residents. Lands in the municipality shall be suitably zoned with sufficient servicing capacity available to meet a three-year demand of growth through intensification. Planning authorities must permit and facilitate all forms of housing and intensification, subject to appropriate development standards, with densities that efficiently use land and resources, are transit supportive, and support active transportation.

Implementation and Interpretation

The PPS stresses the importance of official plans and zoning by-laws in implementing the policies of the Province. Land use designations and policies of official plans, along with development standards of zoning by-laws, are important tools to protect provincial interests and direct development to suitable areas.

PPS Summary Opinion

Staff are of the opinion that the proposed Zoning By-law Amendment is consistent with the policies of the Provincial Policy Statement in that policies related to intensification, growth, land use compatibility, and housing are upheld.

At a density of approximately 87 units (or 202 people) per hectare, this transit-supportive proposal will efficiently use existing infrastructure (subject to an upgrade to the Benfield Drive sanitary sewer at the Applicant's cost), and public service facilities, and will contribute to the range of housing options in the city. The subject lands are located along both an existing transit route and the Greater Niagara Cycle Route, and in close proximity to the West Major Transit Station (GO Station) at 6 Great Western Street. The Applicant has submitted various land use compatibility studies in support of this Application, the recommendations of which will mitigate noise, odour, and air quality impacts associated with adjacent industries, thereby ensuring the long term viability of those existing industries and minimizing risks to public health and safety.

Growth Plan for the Greater Golden Horseshoe

The Growth Plan for the Greater Golden Horseshoe, 2017 (the Growth Plan), came into effect on July 1, 2017. The Growth Plan stresses compact and well-designed development and prioritizes intensification in built-up areas. The Growth Plan supports the achievement of complete communities that “ensure the development of high quality compact built form, an attractive and vibrant public realm, including public open spaces, through site design and urban design standards.” Section 3(5) of the Planning Act stipulates that decisions of Council “shall conform with” the Growth Plan, or “shall not conflict with” the Growth Plan, as the case may be.

The subject lands are located within a built-up area and within a settlement area, as delineated by the Growth Plan. The Growth Plan reiterates many of the policies of the PPS. Several sections of the Growth Plan are particularly relevant to the subject proposal.

Managing Growth

The Growth Plan directs the vast majority of growth to settlement areas that have a delineated built boundary, have existing or planned water and wastewater systems, and can support the achievement of complete communities. Municipalities are required to establish a hierarchy of growth within settlement areas, intended to achieve complete communities that provide a range and mix of land uses and housing, optimize infrastructure, and have a high quality and attractive private and public realm.

Delineated Built-Up Areas

The Growth Plan specifically sets out that by the year 2031, a minimum of 60% of all residential development occurring annually within upper-tier municipalities will be within delineated built-up areas. Further, the Growth Plan requires that each municipality will have an intensification strategy, which identifies the appropriate type and scale of development and transition of built form to adjacent areas, and which ensures lands are zoned and development is designed in such a manner as to support complete communities. Intensification strategies are to be implemented through official plan policies and designations, zoning, and other documents.

Summary Growth Plan Opinion

The Growth Plan provides a policy framework that is intended to be achieved within the context of each individual community, sensitive to adjacent areas. The City’s Official Plan and Zoning By-law set out the policies and standards to achieve the level of intensification and growth intended to occur within the City’s built-up area. These policies support the development of complete communities and help guide what is considered appropriate development given the context of the area. Staff are of the opinion that the proposed Zoning By-law Amendment conforms with and does not conflict with the policies of the Growth Plan for the Greater Golden Horseshoe in that policies related to managing growth, directing growth to built-up areas, and housing are upheld.

These vacant lands are designated Mixed Use by the City's Official Plan and are therefore targeted for general intensification. Notwithstanding the 60% intensification target set out in the Growth Plan, the Regional Official Plan (ROP) provides that 95% of all residential development occurring annually in St. Catharines must be within the built-up area. The proposed density of approximately 87 units per hectare will count toward this intensification target, while also contributing to the range of housing opportunities in the neighbourhood. Through the Site Plan Approval process, the design of this site will be refined to ensure that development is compatible and context sensitive and in keeping with the City's urban design guidelines.

Regional Official Plan

As with the Growth Plan, the Regional Official Plan (ROP) directs that intensification in the form of compact, mixed-use, transit supportive development is to take place in built-up urban areas where appropriate servicing and infrastructure exists. The ROP establishes a framework for developing complete communities that include a diverse mix of land uses, a range of housing types, high quality public open spaces, and access to commercial uses by bike, car, and transit.

The subject lands are located within the St. Catharines Urban Area, as defined by the ROP. Policies of the ROP that are particularly relevant to this application are summarized below.

Intensification and Greenfield Growth

The ROP states that intensification includes all forms of development that occurs within the built-up area. The ROP requires that municipalities develop intensification strategies, including the identification of specific Intensification Areas and general intensification to support the achievement of targets. The ROP establishes a residential intensification target of 95% for the City of St. Catharines, together with policies to establish Intensification Areas and to support general intensification throughout the built-up area, including within Mixed Use designations.

Summary Regional Official Plan Opinion

Regional planning staff have confirmed their support for the Application and are satisfied that the proposal aligns with its Official Plan policies. The proposed density of approximately 87 units per hectare will count toward the city's 95% residential intensification target and contribute to the range of housing opportunities in the neighbourhood. Staff agree with Regional staff that the proposal is consistent with the Regional Official Plan.

Local Official Plan (Garden City Plan)

The City's Official Plan, The Garden City Plan (GCP) sets out land use designations, policies, and principles to direct and manage the future orderly and efficient growth of the City. The Plan reflects the City's individual community interests within the context of Provincial and Region planning regimes. Section 1.1 b) i) of the Official Plan states that "no By-law may be passed... that does not conform to this Plan".

The City's Official Plan was approved by the Region of Niagara in 2012. Since then, the Province has updated the PPS in 2014, and the Greenbelt Plan, Niagara Escarpment Plan and the Growth Plan in 2017. The City's Official Plan will need to be updated once the Region of Niagara Official Plan is updated to reflect Provincial land use policy. In any event, the City's current Official Plan is consistent with, conforms with, and does not conflict with the PPS, Provincial Plans and the Regional Official Plan.

Land Use

The subject lands are located within the boundaries of the West Transit Station Area, as identified by the GO Transit Station Secondary Plan (GTSSP) adopted by Council on May 7, 2018. As a result of the GTSSP and implementing Official Plan Amendment (OPA 19) the land use designation of the subject lands was changed from Commercial to Mixed Use, which designation is intended to provide a broad array and mix of medium and higher density housing, live / work accommodation, commercial, local office, institutional, indoor recreation, and cultural uses primarily intended to serve the immediate neighbourhood and community population. Mixed Use areas are so designated to recognize opportunities for redevelopment and intensification, which may support compact, transit supportive development.

The range and scale of land uses permitted in the City's various mixed use areas are set out in District Plans contained within the GCP. Previously designated Major Commercial, OPA 19 amended the refined land use designation of the subject lands to Mixed Use 2, permitting a broad range and mix of uses intended to serve not only the neighbourhood and community populations, but also an intra-and inter-regional population and market. The Mixed Use 2 policies support both stand-alone residential and mixed use development at a minimum density equivalent to 150 people and jobs per hectare for the optimum use of the West Transit Station Area.

Permitted uses in the Mixed Use 2 designation include triplex, fourplex, townhouse and apartment dwellings, as well as retail, service commercial, office, institutional, indoor recreation and community and cultural facilities. Lower-density residential uses, such as detached, semi-detached, and duplex dwellings are not permitted. Live / work accommodation is also permitted, as well as transit station uses. Maximum building height shall generally not exceed 35 metres (10 storeys) on the subject lands.

The Official Plan emphasizes more compact, innovative and alternative design to manage growth and development. Section 7.1 stipulates that any proposal for development or redevelopment within the Urban Area must be evaluated having regard for the following matters.

- Support safe, accessible and connected active transportation linkages.
- Provide opportunities to create or enhance public spaces, active transportation linkages, facilities and gateways.
- Require building, site, streetscape, and neighbourhood context sensitive design to ensure:

- the integration of compatible built form, scale, massing, height, setbacks, spacing, siting, orientation, facades, and architectural materials with adjacent uses and the surrounding neighbourhood;
- adverse impacts on adjacent properties are minimized as they relate to grading, drainage, access, circulation, and parking, etc.;
- possible negative impacts, such as noise, odour, and emissions, are not excessive;
- adequate and appropriate lot size, shape and configuration, access, on-site facilities, and outdoor amenity areas;
- that parking areas do not dominate the site physically or visually.
- Provide opportunities to support and improve connectivity and accessibility to public transit and active transportation.
- Consideration for the treatment of existing topographic features, and greening and landscaping opportunities.
- Promote the best use of existing resources, infrastructure and service capacity.
- Minimize surface parking areas and promote shared parking areas, standards, and access, where appropriate and compatible.
- Ensure land assembly and configuration will not detract from the potential development or redevelopment of adjacent properties.

Summary Opinion of Official Plan Land Use Policies

Staff are satisfied that the Applicant's development proposal complies with the land use policies of the City's Official Plan, the Garden City Plan. The proposed design of the private road development, comprising four-storey apartment buildings and two-storey townhouses (both permitted uses), is compatible with lower-rise residential uses to the north in terms of built form, scale, massing, height, siting, orientation, among other considerations. A substantial southerly interior side yard and noise attenuation barrier ensure that potential negative impacts associated with the adjacent industrial uses will be mitigated. All parking associated with the proposed use is located to the interior of the site, framed by buildings that will line Louth Street and Benfield Drive. The provision of one main access to the site eliminates the requirement for multiple private driveway accesses to the public street network. A new 1.5 metre sidewalk will be provided along both the Louth Street and Benfield Drive frontages, thereby improving connectivity and accessibility throughout the neighbourhood. At a density of 87 units per hectare or 202 people per hectare, the proposal complies with the density policies of the Mixed Use 2 designation and efficiently uses existing resources, infrastructure and service capacity.

Growth Management

A framework for managing growth in the City is set out in Section 2.3 of the Official Plan. The Plan provides a balanced approach that reflects and respects the character, function, scale and context of each area of the City, and which is consistent with and supports the growth and intensification strategies as set out in the Growth Plan.

The City's Urban Area boundary is finite and cannot be expanded and there is only a small amount of Designated Greenfield Area remaining. Section 2.3.3.2 of the Official Plan provides that growth and development in the City will occur by using land and

infrastructure efficiently, building compact forms, having a mix of uses and housing forms, achieved through compatible design. The primary areas for intensification within the built boundary are the Urban Growth Centre, Intensification Areas and other vacant lands dispersed throughout the City. The Plan also recognizes and supports general intensification on all designated Residential and Mixed Use properties, subject to the density parameters set out in the Plan.

Section 2.3.3.5 of the Garden City Plan sets out policies that support opportunities for medium and high density housing and a mix of housing choices, primarily directed towards the Urban Growth Centre, Intensification Areas, and in the form of general intensification on lands designated Residential and Mixed Use. New housing will be provided in a manner not to compromise, but to accentuate and enhance the character, structure, function, and accessibility of established residential neighbourhoods

Summary Opinion of Official Plan Growth Management Policies

The proposed Zoning By-law Amendment aligns with the growth management policies of the Official Plan in that the proposal provides for apartment dwelling units and townhouse dwelling units on a vacant site that is designated Mixed Use. At a density of 87 units (202 people) per hectare, the development will efficiently use existing municipal services, is transit supportive, and provides for a mix of dwelling unit types and sizes. The proposed zoning will also permit a range of commercial and office uses so as not to preclude non-residential uses on the lands. Approval of the Zoning By-law Amendment will contribute towards intensification targets and a diversity of housing opportunities in the City.

Zoning By-law

Zoning By-law 2013-283 zones the subject lands as Major Commercial (C4), permitting a broad range of commercial uses, including retail, service commercial, and automobile-related uses, as well as non-commercial uses, such as office and places of worship. Residential uses are also permitted in the C4 zone, but only in conjunction with a commercial use and to a maximum lot coverage of 15%. Building height is not restricted in the C4 zone.

The proposal is to change the zoning of the subject lands to Medium/High Density Mixed Use (M2) so that stand alone residential uses may be permitted (that is to say that residential uses may be permitted without the requirement for an accompanying commercial use). The Applicant also requests certain site specific provisions, as outlined in Appendix 4 attached to this report. A planning analysis of each of these provisions is provided below. The technical staff recommendation is attached to this report as Appendix 5.

Phasing and Creation of Future Lot Lines

The Zoning By-law Amendment will permit the subject lands to develop in phases, specifically as it relates to each individual phase complying with minimum density requirements. For example, should the first phase of the development proceed and include only one apartment building, the proposal would be deemed to comply with the

minimum density requirements of the Zoning By-law, provided the entire site, upon full build-out of all phases, complies with the minimum density of 85 units per hectare.

Additionally, the By-law will avoid potential non-compliance with zoning provisions, should there be creation of additional lot lines within the development in the future. This provision is not intended to exempt the subject lands from complying with zoning in the future (e.g. should there be severances proposed to facilitate multiple development proposals, different than that which is proposed by the Applicant today), rather the provision would facilitate the future plan of condominium for this development, which could include the creation of new lot lines within the overall development. The entirety of the subject lands will be bound by the terms of one site plan agreement, which ensures the lands will function as one parcel, notwithstanding it may be made up of several different lots or parcels.

Yards and Lot Lines

Due to the irregular shape of the subject lands, it is difficult to differentiate between front, rear, and side lots lines. Accordingly, the proposed site specific provisions will provide clarity by designating the front, interior, and exterior lot lines, by establishing that there is no technical rear lot line, and by redefining interior side yard and exterior side yard.

Platform Structures

Zoning By-law 2013-283 sets out certain setback requirements from lot lines for platform structures, such as decks and balconies. The Applicant has requested that these requirements not apply to the subject lands so that the structures can encroach into any yard with a zero setback from lot lines. The subject lands do not directly abut any sensitive uses, such as lower-rise residential uses. Residential uses exist on the opposite side of Louth Street, significantly separated from the subject lands by virtue of the road allowance (a Regional arterial road with a right-of-way width of approximately 31 metres). Staff are satisfied that the proposed exemption will not cause any adverse impacts to those existing residential uses. Balconies, porches and decks will provide desired amenity space for future residents.

Prohibited Uses

Zoning By-law 2013-283 permits all forms of housing in the Medium / High Density Mixed Use (M2) zone. Detached, semi-detached, and duplex dwellings would not contribute to an efficient use of these lands, nor are they appropriate forms of housing in an area where transit-supportive densities are envisioned. These specific uses are not permitted in the Mixed Use 2 Official Plan designation that was recently approved for these lands. The recommended prohibited uses are, therefore, in keeping with the Official Plan policies.

Density

The proposed Zoning By-law Amendment will establish a minimum density of 85 units per hectare (approximately equivalent to 202 people or jobs per hectare) for this site, which is an appropriate transit-supportive density in this location, in close proximity to

the West Major Transit Station. The proposed minimum density will allow for additional density beyond 85 units per hectare and the 220 units proposed by the Applicant; this is reflective of the policies of the Mixed Use 2 designation of the Official Plan, which does not cap density. The proposed minimum density will replace the existing minimum and maximum lot area requirements (another mechanism to control density) that would otherwise apply to the site. The M2 provision that caps residential density to 198 units per hectare would also be deleted.

Yards Requirements

The Applicant's concept site plan attached to this report as Appendix 2 demonstrates how all yard requirements of the M2 zone can be met, save and except the required yard adjacent to the southerly lot line of the smaller parcel owned by the MTO. The Applicant has requested a minor change to these yards to provide for a level of flexibility during the detailed design stages and to accommodate pinch points.

For apartment buildings and a private road development, the Applicant is seeking changes to the required front and exterior side yards from a minimum of 3 metres to 2.5 metres at pinch points, and a maximum of 7.5 metres from 5 metres. The yards in question are located along Benfield Drive and Louth Street where no private driveway accesses are proposed. The minimum 3.0 metre front and exterior side yards of the M2 zone will continue to apply, however reductions to 2.5 metres will be permitted along 20 per cent of the front and exterior lot lines to accommodate pinch points between the building and lot lines, primarily caused by the irregular shape of the lot. Staff are satisfied that the requested changes are minor and will result in an appropriate interface between the public and private realms, including the provision of sufficient space for landscaping on private property. Staff recommends that these yard requirements also apply to mixed use buildings and non-residential buildings.

The Applicant also requests a reduction in the minimum interior side yard for apartment buildings from 1.0 metre to 0.5 metres to accommodate a pinch point between the northeast corner of apartment building 'D' and the southerly lot line of the MTO parcel. It should be noted that the Applicant is actively seeking acquisition of these lands and the MTO has authorized the Applicant to include them in this Application for Zoning By-law Amendment. Once the lands are transferred to the Applicant and merge in title with 10 Benfield Drive, the requested 0.5 metre reduction will be redundant. Staff also recommends this yard requirement apply to mixed use buildings and non-residential buildings.

No specific change has been requested by the Applicant for the southerly interior side yard adjacent to existing industrial uses. However, staff believe it prudent to impose a larger yard here, given the site's proximity to existing industrial uses and the recommendations of the noise feasibility study for a noise attenuation barrier along the southerly lot line. Staff are recommending a minimum 12.9 metre southerly interior side yard (consistent with the yard provided in the Applicant's concept site plan and noise feasibility study) be imposed for all stand-alone residential uses and mixed use buildings.

Building Height and Step-backs

The existing C4 zoning that applies to these lands does not cap building height. No maximum building height is prescribed. As part of the GO Transit Station Secondary Plan, Council approved a maximum 35 metre (10 storey) building height for these lands; staff is recommending that this height be implemented through this Zoning By-law Amendment.

To ensure tall buildings do not impose upon or overwhelm the public realm, and in keeping with the principles of the GTSSP Urban Design Guidelines and M2 zoning provisions, a 2.5 metre step-back from both Louth Street and Benfield Drive will be required for any portion of a building 14 metres (4 storeys) in height or taller.

Parking Coverage, Landscape Open Space, and Driveway Widths

Zoning By-law 2013-283 regulates maximum parking coverage in residential zones for properties with four or fewer dwelling units. No such requirement exists in the existing C4 zoning that applies to this site, nor the proposed M2 zoning. Restricting the amount of paved parking area on this site to a maximum of 35% helps to augment the amount of landscaped open space provided. To that same end, staff recommend a minimum landscaped open space requirement of 25%.

With respect to driveway widths for townhouse dwellings, the Applicant requests an increase in the maximum 50% of the unit width to 55% of the unit width. The intent of the maximum 50% provision is to ensure sufficient space in front of the dwellings for street trees and to minimize the amount of asphalt visible from the street. Staff are satisfied that a 5% increase is minor and will leave sufficient space for trees and other plantings.

Affordable Housing

While Council considers affordable housing a priority concern for the City, and the City's Official Plan recognizes the need to provide for affordable housing, actual mechanisms to require and sustain an affordable housing supply in the City, as per the definition in the Provincial Policy Statement 2014, are not currently in place. As such, at this time, applicants are *encouraged* to provide affordable housing, however it is not required.

The Applicant for this project has advised that while none of the proposed units are intended to meet the definition of affordable housing as defined by the Provincial Policy Statement (2014), apartment dwelling units and townhouse dwellings are often considered a more affordable housing option than detached dwellings.

Council should be aware that although condominium tenure is envisioned for this project, this does not suggest that the units, once constructed, will be owner occupied; it is not unusual for condominium units to become investment properties for rental purposes. Additional availability of residential ownership units and/or rental units in the ownership / rental market serves to increase the inventory of residential accommodation, thereby increasing the availability of dwelling units. The 220 units

proposed by the Applicant may not meet the definition of affordable under the PPS, but by increasing the inventory of available dwelling units, pressure is exerted in the market place for pricing to be more competitive.

Public Consultation

In accordance with the requirements of the Planning Act, and subsequent to the Open House held by Planning Services staff on September 19, 2018, a statutory Public Meeting was held by Council on December 10, 2018. Staff presented the previous Information Report. The Applicant was in attendance, together with their consulting planner; there were no members of the public in attendance who spoke to the Application. Afterward the Public Meeting was adjourned.

Written comments received prior to the Public Meeting are attached to the previous Information Report. The following is a summary of, and response to, public comments not specifically addressed by staff at the time of the Public Meeting.

Comment: The proposed four-storey apartment buildings are too high and will impact privacy and cause shadowing on low-rise residential uses located north of Louth Street. The location of the townhouses and apartment buildings should be flipped so that the lower rise dwellings line the Louth Street frontage.

Response: The proposed four-storey apartment buildings are located approximately 45 metres away from the nearest residential building north of Louth Street. Shadow and privacy impacts, if any, would be minimal. It is noted that the existing C4 zoning applicable to these lands does not restrict building height. The proposed zoning by-law amendment will decrease permitted height to a maximum of 35 metres.

Notwithstanding, the City regularly requires shadow impact studies for any building six storeys in height or taller.

Comment: Both the Noise Feasibility Study and D-6 Compatibility Assessment underestimate the operations of the existing industrial facilities to the south. Further, the height of the existing industrial buildings appears to be inaccurate. These inaccuracies should be addressed so that future residents of the development are fully aware of the potential impacts of living in proximity to industrial uses.

Response: As noted in the previous Information Report, Planning Services staff reached out to the owners / operators of the existing industrial uses to the south to better understand these concerns. To-date, no response has been received. In any event, the Applicant's consultant has submitted a letter of comfort that provides further clarification on these matters (see Appendix 6). Regional staff and Planning Services staff are satisfied that the additional information provided meets the requirements set out by the Ministry of the Environment and Climate Change. Further, the future site plan agreement will include warning clauses advising future owners of the operations of the neighbouring industrial facilities.

Site Plan Approval

As referenced above, an Application for Site Plan Approval will be required should the requested amendment to the Zoning By-law be approved. Site Plan Control is the City's primary tool for implementing City standards and urban design guidelines; it regulates the placement of buildings, architectural elevations, access, parking, site servicing, grading, drainage, lighting, landscaping, fencing, storage of waste and other details of site design. Appendix 2 provides the proposed conceptual site design.

An additional public Open House relating to the detailed design of the site will be hosted by City staff as part of the site plan process (and draft plan of condominium approval process). Through that forum, Planning Services staff and the Applicant will present the refined designs to the public for information purposes. It is not a forum for the public to oppose the development altogether, rather it is an opportunity to provide input on the design of the site and buildings.

Site plan approval includes the registration of a site plan agreement against the title of the lands that will be legally binding upon the existing and future property owners. The Applicant will be required to post securities to ensure the terms of the agreement are carried out.

Public Notice

In accordance with established procedures, notice for the public meeting has been circulated.

Second Planning Opinion Advisory

Should Council consider not supporting the Staff Recommendation, Council is advised to defer its decision until such time as a second planning opinion from an outside consultant can be obtained. In the event the second planning opinion is supported by Council, and Council makes a decision based on that second planning opinion, and if and when the matter should be heard before the Local Planning Appeal Tribunal, then the planner who has provided the second opinion shall be retained for the purpose of a hearing.

Potential Appeals

Council should be aware that, effective April 3, 2018, certain land use appeals in Ontario are subject to new legislation. For zoning by-law amendment applications, any appeals are submitted to the Local Planning Appeal Tribunal (LPAT) which replaces the former Ontario Municipal Board (OMB).

Financial Implications

All costs associated with this development will be borne by the developer. If approved and constructed, additional tax assessment would be generated by the development.

Relationship to Strategic Plan

Approval of this Application for Zoning By-law Amendment would support the following pillars of the City's Strategic Plan.

Economic Sustainability: The proposed development supports private investment through new construction on vacant lands. Future residents of the proposed development will support transit and local business in the GO Transit Station Secondary Plan Area.

Social Sustainability: The proposed development contributes to the connectivity of people, places and neighbourhoods by providing a mix of residential uses and new pedestrian connections along Louth Street and Benfield Drive. Future residents will benefit from the local parks, trail systems, and public recreation facilities.

Conclusion

In summary, staff are recommending approval of the proposed Zoning By-law Amendment, subject to the recommendations set out in this report. The Amendment would facilitate the efficient and appropriate use of vacant lands within the urban area with access to existing infrastructure, services, transit, and community commercial facilities. The mix of dwelling types and sizes will contribute to the City's intensification targets and the range of available housing in the community.

This Application for Zoning By-law Amendment has regard for matters of provincial interest, is consistent with the Provincial Policy Statement, conforms to and does not conflict with the Growth Plan for the Greater Golden Horseshoe, and conforms to and does not conflict with both the Region of Niagara's Official Plan and the City of St. Catharines Official Plan, subject to the staff recommendations herein.

Notification

It is in order to advise Jonathan Rodger, Zelinka Priamo, 20 Maud Street, Suite 305, M5V 2M5, the owner's agent.

Prepared by:

Amanda Knutson
Senior Planner

Submitted by:

Judy Pihach, MCIP, RPP
Manager, Planning Services

Approved by:

Tami Kitay, MPA, MCIP, RPP
Director, Planning and Building Services

Aerial Photo Subject Site



Subject Lands

10 Benfield Drive & the adjacent parcel of land with no assigned municipal address located opposite the intersection of Louth Street and Crestcombe Road
File: 60.35.1045

ZONING BYLAW REQUIREMENTS

M2 Zone Regulations

Parking Regulations	Required	Provided
Parking Spaces - Min		
Apartment	240	240
Townhouse	35	61
Total	282	301
Accessible	7	7
Parking Space Dimensions - Min (Standard) (m)		
Apartment	2.6m by 5.2m	2.6m by 5.2m
Townhouse	3m by 5.2m	3m by 5.2m
Accessible	5.2m by 5.2	5.2m by 5.2
Parking Area - Landscape Buffer - Min (m)		
Lot line Abutting a Public Road	6	>6
Lot Line Not Abutting a Public Road	3	>3
Parking Area - Landscaped Open Space - Min (%)	10	12.9
Parking Area Lot Coverage - Max	20%	30%
Bicycle Parking		
Min spaces - Apartment Building (10+ Units)	40	40
Min space dimensions (m)	1.8 by 0.3	1.8 by 0.3
Apartment Building		
Lot Area - Min (sq. m)	45	>45
Lot Area - Max (sq. m)	165	<165
Lot Frontage - Min (m)	20	>20
Front Yard - Min (m)		
Portion of Building up to 14 m in height	3	3
For portion of Building 14 m in height or greater	5	N/A
Front Yard - Max (m) to dwelling	5	3
Rear Yard - Min (m)		
Portion of Building up to 14 m in height	4.5	3
Additional for each additional 1 m in building height	0.5	0
Interior Side Yard - Min (m)	1.2	0.558
Exterior Side Yard - Min (m)		
Portion of Building up to 14 m in height	3	2.9
For portion of Building 14 m in height or greater	5	2.9
Private Road Development (Townhouses)		
Lot Area - Min (sq. m)	100	>100
Lot Area - Max (sq. m)	165	<165
Lot Frontage - Min (m)	12	>12
Front Yard - Min (m)		
To dwelling	3	3
To garage	6	3
Front Yard - Max (m)	5	6.05
Rear Yard - Min (m)		
From End Wall	3	3
From Rear Wall	6	N/A
Interior Side Yard - Min (m)		
Between End Walls	3	N/A
Between Rear Walls	6	>6M
Yard Between Buildings - Min (m)		
Between End Walls	3	3
Between Rear Walls	7.5	N/A
Between End and Rear Walls	6	N/A
Distance from Private Road - Min (m)		
To Attached Garage	6	6.05
To Dwelling	6	6.05
Unit Driveway Width - Maximum (% of Unit Width)	50	49
Number of Attached Dwelling Units - Max	8	7
Max Building Height (m)	11	8.2



UNIT COUNT		
UNITS PER HECTARE: 87		
UNITS PER ACRE: 35		
BUILDING A	= 50 UNITS	800 sq.ft. per unit average
BUILDING B	= 50 UNITS	
BUILDING C	= 42 UNITS	
BUILDING D	= 50 UNITS	
TOTAL	= 192 UNITS	
TOWNHOUSES	= 28 UNITS	1,550 sq.ft. per unit average
TOTAL	= 220 UNITS	

PARKING REQUIREMENTS	
APARTMENT	= 1.25 PER UNIT = 192 UNITS X 1.25 = 240
TOWNHOUSE	= 1.25 PER UNIT = 28 X 1.25 = 35
ACCESSIBLE PARKING REQUIREMENTS (INCLUDED IN ABOVE)	
APARTMENT ACCESSIBLE	= 2% OF REQUIRED + 2 = 240 x 0.02 = 4.8(5) + 2 = 7
TOTAL PARKING REQUIRED	= 282
PARKING PROVIDED	
CONDO PARKING	= 240
TOWNHOUSE PARKING	= 61 (INCLUDING GARAGE + VISITOR PARKING)
TOTAL	= 301

SITE STATS			
DESCRIPTION	AREA (SM)	AREA (SF)	%
BUILDING FOOTPRINT	6390 m²	68800 ft²	25.1%
	6390 m²	68800 ft²	25.1%
HARD LANDSCAPE			
ASPHALT	9070 m²	97640 ft²	35.6%
SIDEWALK	1340 m²	14380 ft²	5.3%
	10410 m²	112020 ft²	40.9%
SOFT LANDSCAPE			
LANDSCAPE	8650 m²	93080 ft²	34.0%
	8650 m²	93080 ft²	34.0%
	25450 m²	273900 ft²	100.0%
OVERALL SITE	25450 m²	273920 ft²	100.0%

PARKING STATS

TYPE	DESCRIPTION	COUNT
ACCESSIBLE PARKING	5.2m x 5.2m	7
TYPICAL PARKING	2.6m x 5.2m	233
		240

TYPE	DESCRIPTION	COUNT
TOWNHOUSE TYPICAL PARKING	3m x 5.2m	28
TOWNHOUSE VISITOR PARKING	3m x 5.2m	5
		33

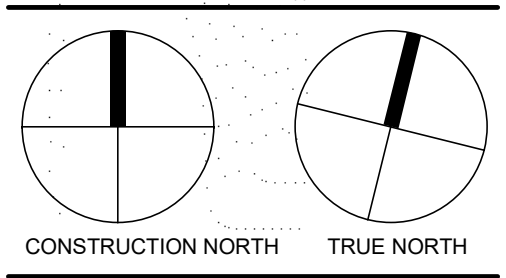
TYPE	DESCRIPTION	COUNT
BIKE SPACE	1.8m x 0.3m	40
		40

NO.	ISSUED	DATE
1.	PRECONSULTATION CITY MEETING	2017-10-20
2	SITE PLAN REVIEW	2018-06-01
3	SITE PLAN REVIEW	2018-06-29



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SEAL



10 BENFIELD DRIVE
St. CATHARINES

10 BENFIELD DRIVE. St. CATHARINES

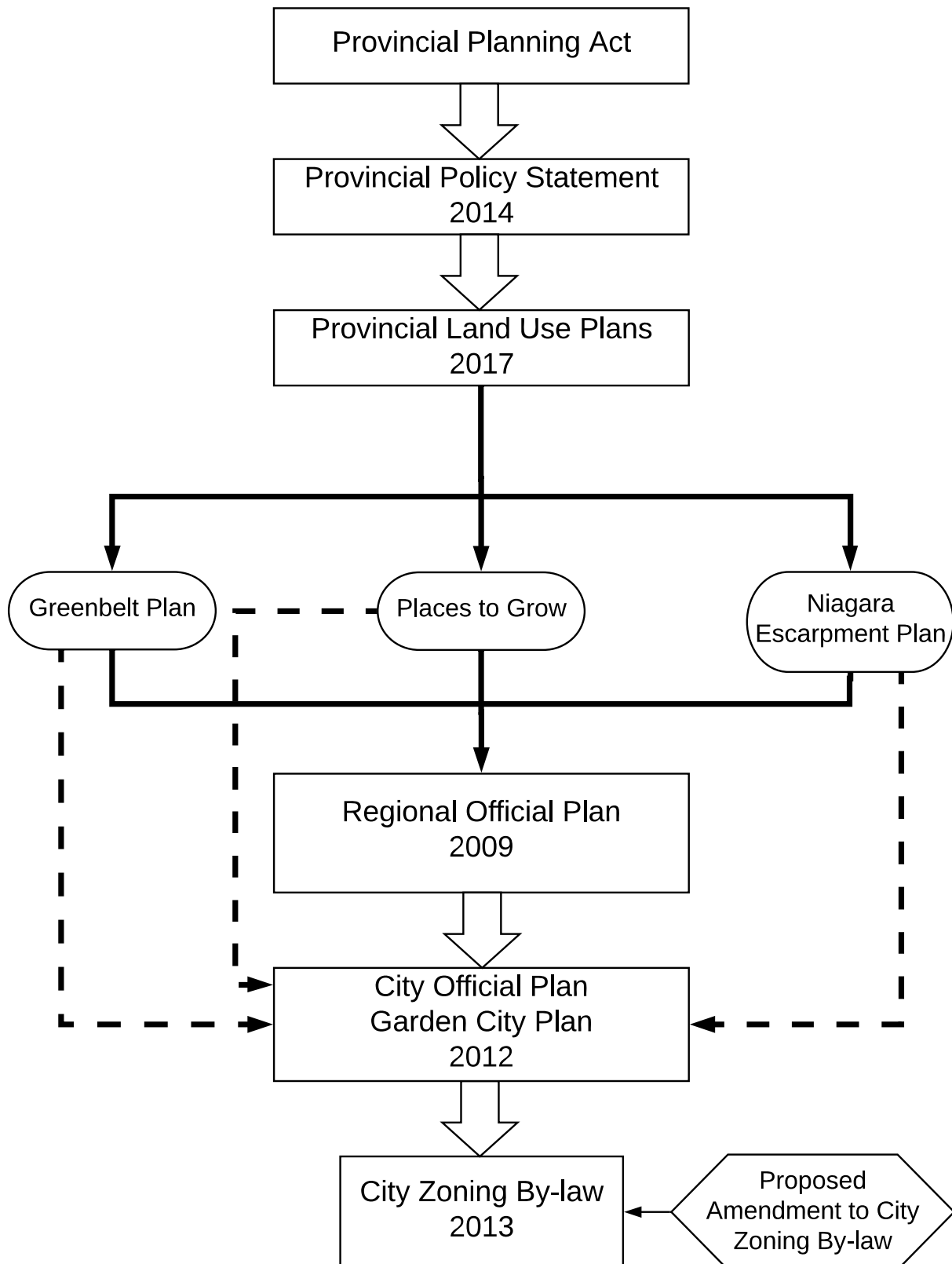
SHEET NAME

SITE PLAN
4 STOREY
4 BUILDINGS &
TOWNS

START DATE	OCTOBER 2017
DRAWN BY	CMC
CHECKED BY	JM
SCALE	1 : 400
PROJECT NO.	117093
DRAWING	

A001

Ontario Land Use Planning Framework



Additional Proposed Zoning Changes

Application for Zoning By-law Amendment

10 Benfield Drive (together with a parcel of land with no assigned municipal address opposite the intersection of Louth Street and Crestcombe Road)

Zoning Provision	Current Requirement	Requested Change	Comment
General Provisions			
Maximum Parking Area Coverage	Does not apply in mixed use zones	35%	Requested for overall site Note: 20% maximum applies in residential zones
Maximum Driveway Width	7.5 m or 50% of the front or exterior lot line distance, whichever is less	55% of the width of the dwelling unit	Requested as it relates to proposed townhouse dwelling units
Platform structures – height above grade between 0.15 m and 0.6 m	Minimum 3 m setback from the exterior lot line; Minimum 1.8 m setback from the rear lot line; Required interior side yard setback for the principal building for an interior lot line	May encroach to the lot line of any yard	
Platform structures – height above grade more than 0.6 m and less than 1.2 m	Minimum 3 m setback from the exterior and rear lot lines; Required interior yard setback for the principal building for an interior lot line	May encroach to the lot line of any yard	
Platform structures – height above grade 1.2 m or greater	Minimum 4.5 m setback from the rear lot line; Required interior	May encroach to the lot line of any yard	

	and exterior side yard setbacks for the principal building for the respective lot lines		
Provisions for Stand Alone Residential Buildings in Mixed Use 2 (M2) Zone – Apartment Buildings			
Minimum Lot Area	45 m ² per dwelling unit	n/a	
Maximum Lot Area	165 m ² per dwelling unit	n/a	
Minimum Front Yard	3 m for portion of building less than 14 m in height / 5 m for portion of building 14 m in height or greater	2.5 m	
Maximum Front Yard	5 m to dwelling	7.5 m	
Minimum Rear Yard	4.5 m for portion of building up to 14 m in height / additional 0.5 m for each additional 1 m in building height	n/a	By definition, the subject lands do not have a rear yard
Minimum Interior Side Yard	1.2 m	0 m	Requested to address yards adjacent to the additional lands owned by the MTO (triangular parcel with no address)
Minimum Exterior Side Yard	3 m for portion of building less than 14 m in height / 5 m for portion of building 14 m in height or greater	1.5 m	Requested to address pinch points along the curve of the Louth Street
Maximum Building Height	Does not exist	35 m	Requested to align with height

			permissions of the Garden City Plan
Provisions for Stand Alone Residential Buildings in Mixed Use 2 (M2) Zone – Private Road Development			
Minimum Lot Area	100 m ² per dwelling unit	n/a	
Maximum Lot Area	165 m ² per dwelling unit	n/a	
Minimum Front Yard	3 m to dwelling / 6 m to garage	2.5 m	
Maximum Front Yard	5 m to dwelling	7.5 m	
Minimum Rear Yard from End Wall	3 m	n/a	By definition, the subject lands do not have a rear yard
Minimum Rear Yard from Rear Wall	6 m	n/a	By definition, the subject lands do not have a rear yard
Minimum Exterior Side Yard	3 m	2.5 m	(adjacent to Louth Street)

Recommendation – Zoning By-law Amendment for 10 Benfield Drive and adjacent parcel with no assigned municipal address located opposite the intersection of Louth Street and Crestcombe Road

1. THAT Section 15.1, Schedule A (19), Zoning Maps, be amended by changing the zoning of 10 Benfield Drive and the adjacent parcel of land with no assigned municipal address located opposite the intersection of Louth Street and Crestcombe Road, as illustrated on the schedule attached, from Major Commercial (C4) to Medium / High Density Mixed Use with Special Provision No. 155 (C4-155);
2. THAT Section 13.1, List of Special Provisions, be amended by adding a new Special Provision No. 155, as follows:

Special Provision	Zone	Schedule A	Schedule B	Location	By-law
155	M2	19		10 Benfield Drive, and adjacent parcel located opposite the intersection of Louth Street and Crestcombe Road	2019-
1.	Notwithstanding any other provision of this By-law, should any Zoning non-compliance result from the future establishment of a new lot line within the boundaries of the approved site plan for the lands which are subject to this Special Provision, the lands shall be deemed to conform to the Zoning By-law.				
2.	Notwithstanding any other provision of this By-law, the lands that are subject to this Special Provision shall be permitted to develop in phases. Building phases that do not individually comply with the provisions of the				

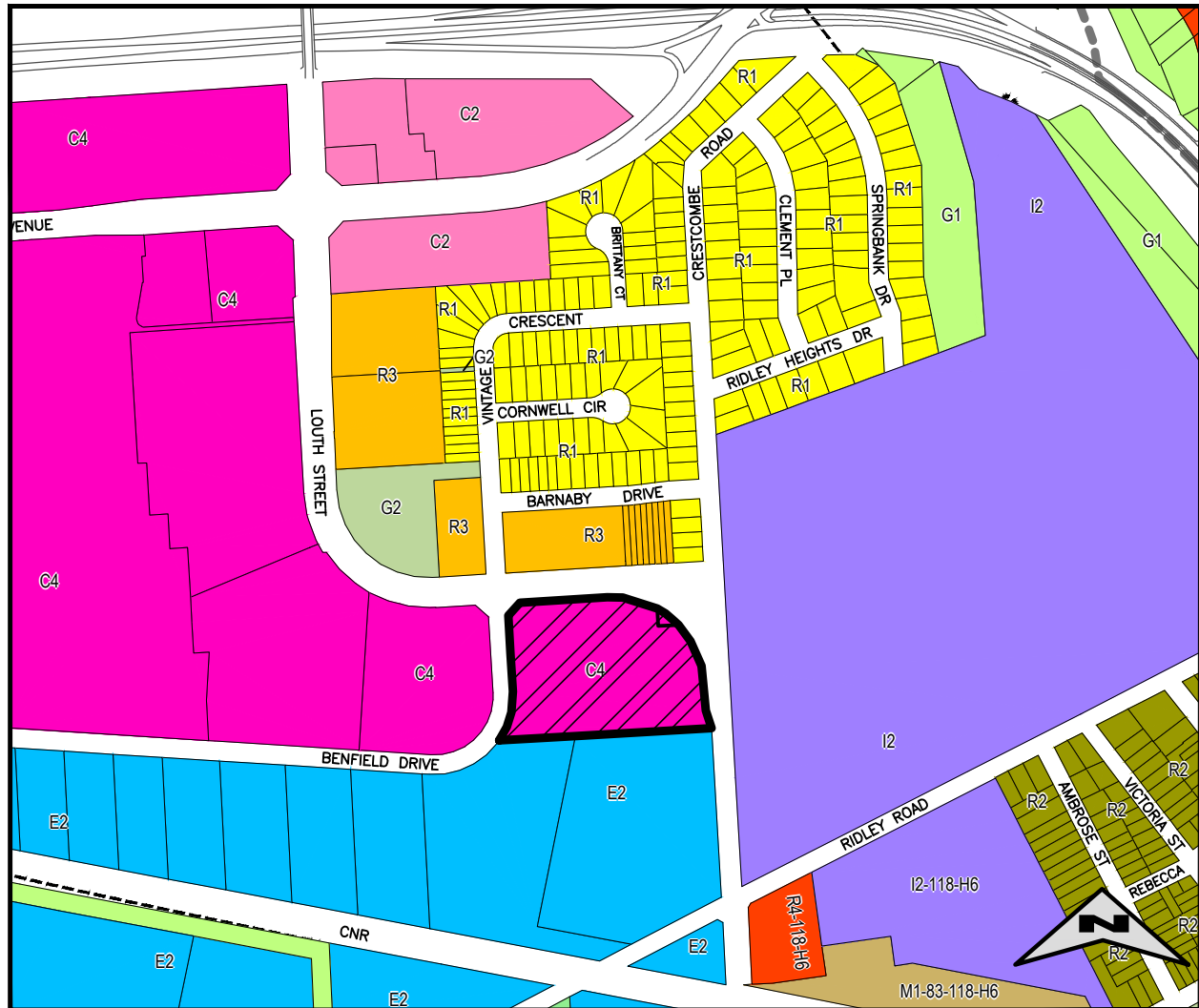
	Zoning By-law shall be permitted without amendment to the Zoning By-law, if upon full build out the site would otherwise comply.
3.	The lot line adjacent to Benfield Drive shall be deemed to be the front lot line.
4.	Any lot line adjacent to Louth Street shall be deemed to be an exterior lot line.
5.	The furthest southerly lot line shall be deemed to be the southerly interior lot line.
6.	All other lot lines shall be deemed to be interior lot lines.
7.	The lands shall be deemed to have no rear lot line.
8.	Despite the definition of Yard, Exterior Side set out in Section 12, Definitions, the said definition shall be “that portion of a lot between the exterior side lot line and the specified distance to where buildings or structures may be located”.
9.	Despite the definition of Yard, Interior Side set out in Section 12, Definitions, the said definition shall be “that portion of a lot between the interior side lot line and the specified distance to where buildings or structures may be located”.
10.	In addition to the structures referenced in Section 2.10.1, Permitted Encroachments, General Structures, canopies shall also be permitted to encroach to within 0.15 metres of any lot line.
11.	Despite the provisions of Section 2.10.2, Permitted Encroachments, Platform Structures, there shall be no minimum setbacks for platform structures, regardless of height above grade.
12.	Despite the provisions of Section 8.2, Permitted Uses, detached, semi-detached and duplex dwellings shall not be permitted.
13.	Minimum density shall be 85 units per hectare.
14.	Despite the provisions of Section 8.4, Provisions for Stand Alone Residential Buildings in Mixed Use (M2) Zone, the following shall apply:

	<p>a) There shall be no Minimum Lot Area or Maximum Lot Area requirements;</p> <p>b) Minimum Southerly Interior Side Yard for all stand alone residential uses shall be 12.9 metres.</p>
15.	<p>In addition to the provisions of Section 8.4, Provisions for Stand Alone Residential Buildings in Mixed Use (M2) Zone, Maximum Exterior Side Yard shall be 7.5 metres (measured from a rear wall or an end wall for Private Road Developments).</p>
16.	<p>Despite the provisions of Section 8.4, Provisions for Stand Alone Residential Buildings in Mixed Use (M2) Zone, the following shall apply for Private Road Developments:</p> <p>a) Minimum Front Yard shall be 3.0 metres to the dwelling, 6 metres to the garage (* **), with reductions in the Minimum Front Yard to 2.5 metres to the dwelling permitted along 20 per cent of the length of the Front Lot Line;</p> <p style="padding-left: 40px;">(* garages shall be flush with or recessed beyond the corresponding wall of the dwelling unit)</p> <p style="padding-left: 40px;">(** the yard requirement for garage applies to an attached garage, and to the yard from which the attached garage is accessed by a driveway)</p> <p>b) Maximum Front Yard shall be 7.5 metres;</p> <p>c) Minimum Exterior Side Yard from a rear wall or an end wall shall be 3.0 metres, with reductions in the Minimum Exterior Side Yard to 2.5 metres from a rear wall or an end wall permitted along 20 per cent of the length of the Exterior Lot Line.</p>
17.	<p>In addition to the provisions of Section 8.4, Provisions for Stand Alone Residential Buildings in Mixed Use (M2) Zone, the following shall apply for Private Road Developments:</p> <p>a) Minimum distance from a private road to an attached garage shall be 6.0 metres;</p> <p>b) Minimum distance from a private road to a dwelling shall be 3.0 metres.</p>

18.	<p>Despite the provisions of Section 8.4, Provisions for Stand Alone Residential Buildings in Mixed Use (M2) Zone, the following shall apply for Apartment Buildings and Long Term Care Facilities:</p> <ul style="list-style-type: none"> a) Minimum Front Yard shall be 3.0 metres for any portion of a building less than 14 metres in height, with reductions in the Minimum Front Yard to 2.5 metres permitted along 20 per cent of the length of the Front Lot Line, and 5 metres for any portion of a building 14 metres in height or greater; b) Maximum Front Yard shall be 7.5 metres; c) Minimum setback for all other Interior Side Yards shall be 0.5 metres; d) Minimum Exterior Side Yard shall be 3.0 metres for any portion of a building less than 14 metres in height, with reductions in the Minimum Exterior Side Yard to 2.5 metres permitted along 20 per cent of the length of the Exterior Lot Line, and 5 metres for any portion of a building 14 metres in height or greater; e) Maximum Building Height shall be 35 metres.
19.	<p>In addition to the provisions of Section 8.4, Provisions for Stand Alone Residential Buildings in Mixed Use (M2) Zone, Maximum Exterior Side Yard for Apartment Buildings and Long Term Care Facilities shall be 7.5 metres.</p>
20.	<p>Despite the provisions of Section 8.7, Provisions for Non-Residential Buildings and Mixed Use (Residential and Non-Residential) Buildings in the Mixed Use (M1), (M2), (M3) Zones, the following shall apply:</p> <ul style="list-style-type: none"> a) There shall be no Maximum Lot Area or Maximum Lot Frontage requirements; b) Minimum Front Yard shall be 3.0 metres for any portion of a building less than 14 metres in height, with reductions in the Minimum Front Yard permitted to 2.5 metres along 20 per cent of the length of the Front Lot Line, and 5 metres for any portion of a building 14 metres in height or greater; c) Maximum Front Yard shall be 7.5 metres;

	<p>d) Minimum Southerly Interior Side Yard for mixed use buildings shall be 12.9 metres;</p> <p>e) Minimum Southerly Interior Side Yard for non-residential buildings shall be 4.5 metres for any portion of a building less than 14 metres in height, plus an additional 0.5 metres for each additional 1 metre in building height;</p> <p>f) Minimum setback for all other Interior Side Yards shall be 0.5 metres;</p> <p>g) Minimum Exterior Side Yard shall be 3.0 metres for any portion of a building less than 14 metres in height, with reductions in the Minimum Exterior Side Yard to 2.5 metres permitted along 20 per cent of the length of the Exterior Lot Line, and 5 metres for any portion of a building 14 metres in height or greater;</p> <p>h) Maximum Exterior Side Yard shall be 7.5 metres;</p> <p>i) Maximum Building Height shall be 35 metres;</p> <p>j) There shall be no Maximum Residential Density requirement.</p>
21.	Maximum Parking Coverage shall be 35%.
22.	Minimum Landscaped Open Space shall be 25%.
23.	Maximum townhouse dwelling unit driveway width shall be 55% of the width of the corresponding townhouse unit.

Proposed Amendment to Zoning By-Law 2013-283 (Schedule A - Zoning By-law 2013-283)



Subject Lands

10 Benfield Drive & the adjacent parcel of land with no assigned municipal address
located opposite the intersection of Louth Street and Crestcombe Road
Subject Lands to be re-zoned from Major Commercial
(C4) to Medium/High Density Mixed Use (M2) with
Special Provision 155 (M2-155)

Zones

R1 Low Density Residential
R2 Low Density Residential
R3 Medium Density Residential
R4 High Density Residential
M1 Medium Density Mixed Use
C2 Community Commercial

C4 Major Commercial
E2 General Employment
I2 Community Institutional
G1 Conservation / Natural Area
G2 Minor Green Space

File:60.35.1045

October 23, 2018

Mountainview Belmont Ridley Heights Inc.

c/o Ken Gonyou

Mountainview Homes

3350 Merrittville Highway, Unit 9

Thorold, ON L2V 4Y6

Re: Response to Verbal Comments made at the Public Open House for 10 Benfield Drive, St. Catharines, Ontario

Dear Ken,

As requested, we have prepared a response to verbal comments made during the public open house that took place on September 19, 2018. The comments are in reference to our noise report for the subject site, entitled, "Environmental Noise Feasibility Study, Proposed Residential Development, 10 Benfield Drive, St. Catharines, Ontario" dated July 17, 2018.

We understand that the comments were communicated by a representative associated with one of or both of Flagro Industries and/or Decora Powder Coatings, businesses located on the properties immediately to the south of the subject site. Below, in *italics*, we have included our understanding of these comments, following which we have included our responses:

1. *Assumptions with regards to truck and loading activity on site were false and underrepresent the actual amount of truck and loading activity on site.*

This comment seems to indicate that the numbers used in our report may have been misunderstood. To be clear, the number of trucks assumed in our acoustic model was based on a busy hour and not the entire day, as required by MECP guidelines. For instance, for Flagro Industries, and Decora Powder Coatings, we assumed 6 medium trucks and 2 tractor trailers entering/departing each facility per hour (not per day), respectfully. This distinction may have not been clear in the noise report.

2. *References to building heights on the properties to the south were incorrect and lower than what is constructed.*

The acoustic model assumed that the industrial buildings to the south are approximately 5 meters in height. As noted in our report, we also assumed industry-standard worst-case operating conditions for the rooftop HVAC equipment located on these buildings, the location of which is available from aerial imaging.

We would expect a taller building to shield noise generated from rooftop equipment at the townhouse units. The actual building heights, if they are provided, can be revised in our acoustic model when building plans are proposed through the future site plan application.



ACOUSTICS



NOISE



VIBRATION

3. *No one contacted the businesses operating on the properties to the south to question and better understand their business operations.*

HGC Engineering was provided with general information regarding operations from reception on March 16, 2018. HGC Engineering also collected information on these industries during a number of separate site visits. If we had suspected that the general information provided by reception was materially different from our onsite observations we would have made further inquiry.

In any case, we are able to revise the description of the industry if further information can be provided from the respective industry representatives and this information can be included with the submission of a future site plan application.

4. *Plans to expand their business and once expanded the truck and loading activity will pick-up further, and they are budgeting to turn this into a 24/7 operation.*

We understand that the subject site was recently re-designated as part of the St. Catharines Go-Station Secondary Plan for mixed-use, which permits residential uses. Further, we understand that apartment buildings were already permitted on the subject site in the City's existing official plan. As such, the existing industries are required to operate in such a manner that the MECP guidelines are met at the subject site at all times for residential uses. This will apply to any change in operation and/or expansion of the business activities on the industrial lands.

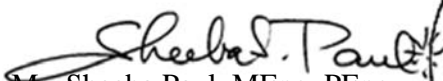
Notwithstanding the foregoing, we look forward to discussions with the industrial operators to further reflect their processes (both current and future), and to determine appropriate mitigation for the mutual benefit of the industry and the designated residential property. We are able to incorporate this information into our noise report, acoustical analysis and acoustic model for the purposes of a future site plan application in order to confirm the appropriate noise mitigation measures related to the definitive building plans that will be proposed when the site plan application is made.

In consideration of the above information, it is our opinion that the recommended mitigation measures outlined in our noise report dated July 17, 2018 do not change with the verbal comments provided at the public open house. Mitigation for noise at the subject site will be in the form of acoustic barriers along the mutual property line of sufficient height or a combination of physical mitigation and architectural solutions for the residential buildings themselves. As noted in our noise report, a more detailed noise analysis will be completed for the southern portion of the site where the townhouses are proposed when detailed building façade elevations, building sections and grading and landscape plans are available. This analysis will be completed for the future site plan application.

Trusting this information is sufficient for your present purposes. Please call if you have any further questions or require clarification.

Yours truly,

Howe Gastmeier Chapnik Limited


Ms. Sheeba Paul, MEng, PEng



ACOUSTICS



NOISE



VIBRATION



CITY OF
ST. CATHARINES

Corporate Report

Report from Financial Management Services, Director / City Treasurer; and
Transportation and Environmental Services, Director / City Engineer

Date of Report: January 10, 2019

Date of Meeting: January 14, 2019

Report Number: FMS/TES-019-2019

File: 10.57.10

Subject: 2019 Capital Budget, Program Report and Four Year Forecast

Recommendation

That the report from Financial Management Services and Transportation and Environmental Services Departments dated December 14, 2018, regarding the 2019 Capital Budget, Program Report and Four Year Forecast (Appendix A) be referred to City Council for consideration of the Committee Recommendation after the Public Meeting scheduled for January 28, 2019 for which notice will be duly given; and

That beginning with the 2020 Capital Budget, staff present the annual Capital Budget to the Budget Standing Committee in the Fall prior to the budget year it relates, and the annual Operating Budget be presented to the Budget Standing Committee subsequent to the Capital Budget approval. FORTHWITH

Committee Recommendation

That Council approve the 2019 Capital Budget, related Program Report and Four Year Forecast as detailed in Appendix 1, Tab 2, Schedules A to P, S to W, as recommended by the Budget Standing Committee at its meeting of January 7, 2019; and

That the City Solicitor be directed to prepare the necessary borrowing by-laws. FORTHWITH.

Background

At its meeting of January 7, 2019, the Budget Standing Committee (BSC) received the 2019 Capital Budget, Program Report and Four Year Forecast report (Report [FMS-B001-2019](#), attached as Appendix A). The BSC approved the following motion:

That the report from Financial Management Services and Transportation and Environmental Services Departments dated December 14, 2018, regarding the 2019 Capital Budget, Program Report and Four Year Forecast be approved by the Budget Standing Committee (BSC) and referred to Council for consideration after the Public Meeting scheduled for January 28, 2019 for which notice will be duly given; and

That beginning with the 2020 Capital budget, staff present the annual Capital Budget to the BSC in the Fall prior to the budget year it relates, and the annual Operating Budget be presented to the BSC subsequent to the Capital Budget approval. FORTHWITH

Financial Implications

Not applicable

Prepared by:

Evan McGinty, Council and Committee Coordinator, Legal and Clerks Services

Submitted / Approved by:

Kristine Douglas, Director, Financial Management Services / City Treasurer

Report from Financial Management Services, Director/Treasurer and Transportation and Environmental Services, Director/City Engineer

Date of Report: December 14, 2018 **Date of Meeting:** January 7, 2019

Report Number: FMS-B001-2019 **File:** 10.57.10

Subject: 2019 Capital Budget, Program Report and Four Year Forecast

Recommendation

That the report from Financial Management Services and Transportation and Environmental Services Departments dated December 14, 2018, regarding the 2019 Capital Budget, Program Report and Four Year Forecast be approved by the Budget Standing Committee (BSC) and referred to Council for consideration after the Public Meeting scheduled for January 28, 2019 for which notice will be duly given; and

That beginning with the 2020 Capital budget, staff present the annual Capital Budget to the BSC in the Fall prior to the budget year it relates, and the annual Operating Budget be presented to the BSC subsequent to the Capital Budget approval. FORTHWITH

Summary

The 2019 Capital Budget, including the related funding, and the Four Year Forecast, in conjunction with the 2019 Update on the Status of Corporate Asset Management is provided for consideration in Appendix 1.

Background

The City of St. Catharines' infrastructure is essential for the delivery of services to our citizens, and its condition impacts the levels of service the City can provide. The City owns, operates and maintains infrastructure with an estimated replacement value of \$4.6 billion as outlined in Table 1 below. The nature of this infrastructure is varied and complex, ranging from sporting arenas to sewer pipes.

Table 1 — Asset Categories and Replacement Values

Asset Category	Inventory	Asset Replacement	
		Value (\$M)	% of Total
Roads	564 km	\$900.0	19.7%
Bridges & Major Culverts	70 each	47.6	1.1%
Water Distribution System	606 km	1,317.0	28.9%
Sanitary Sewer Collection System	570 km	900.0	19.7%
Storm Sewer Collection System	404 km	716.0	15.7%
Sidewalks	585.2 km	139.2	3.0%
Facilities & Improved Lands	128 sites	526.5	11.5%
Playgrounds / Courts	109 each	19.6	0.4%
Total		\$4,565.9	100.0%

The new Provincial Regulation 588/17 reflects the need for municipalities' ongoing focus on asset management. The Regulation explicitly directs each municipal Council to approve its own Strategic Asset Management Policy (SAMP) prior to June 30, 2019. The regulation stipulates that the SAMP address the roles and responsibilities of staff and Council, related to the stewardship of its infrastructure and assets. Council is responsible for approving levels of service, in conjunction with related costs and risk, which Council does when it approves strategy, plans and budgets. Staff is responsible for developing and complying with corporate policy and legislation, delivering on plans and identifying and communicating risks.

Other subsequent regulatory requirements with deadlines over the next five years include strengthened links between the state of infrastructure, levels of service, related costs, and risks. During 2018, staff formed a multi-disciplinary Asset Management Working Group (AMWG). Comprised of key staff, the AMWG will drive compliance with the regulatory requirements and work on furthering the City's comprehensive and coordinated approach to asset management (see Appendix 2 for AMWG membership and mandate). The AMWG will bring forward the SAMP, for Council's approval within the next five months, prior to the June 30, 2019 deadline.

As indicated in Table 2 below, the City's total infrastructure deficit is estimated at \$364.5 million, representing approximately 8% of the replacement value of those asset categories. The reinvestment rates are based on each asset category's life expectancy as published in the 2016 Canadian Infrastructure Report Card. The City's infrastructure deficits include the replacement value of assets that through a condition assessment have been rated as in poor or very poor condition or as a high priority building deficiency.

TABLE 2

Asset Category	Asset Replacement		Infrastructure Deficit			Target Reinvestment Rate (1)	
	Value (\$M)	% of Total	Value (\$M)	% of Replacement Value		Lower %	Upper %
Roads	\$900.0	19.7%	\$59.0	6.6%		2.0%	3.0%
Bridges & Major Culverts	47.6	1.1%	8.6	18.1%		1.0%	1.5%
Water Distribution System	1,317.0	28.9%	62.0	4.7%		1.0%	1.5%
Sanitary Sewer Collection System	900.0	19.7%	119.7	13.3%		1.0%	1.3%
Storm Sewer Collection System	716.0	15.7%	55.8	7.8%		1.0%	1.3%
Sidewalks and Walkways	139.2	3.0%	7.0	5.0%		2.0%	3.0%
Facilities & Improved Lands	526.5	11.5%	46.2	8.8%		1.7%	2.5%
Playgrounds / Courts	19.6	0.4%	6.2	31.6%		1.7%	2.5%
Total	\$4,565.9	100.0%	\$364.5	8.0%		1.3%	1.9%

While the City's infrastructure deficit remains below the national average of 12%¹ the deficit needs to be considered in relation to sustainable funding. Comparing annual target reinvestment rates to the infrastructure deficit, suggests that it would take several years of funding above the target upper limit to significantly reduce the current infrastructure deficit.

Comparing the City's infrastructure deficit 5-year trend reflects significant growth over the period, as shown in graph 1 below. The annual growth in the infrastructure deficit is a product of including more asset classes in the AMP, improved information related to both asset condition, and replacement costs, and ongoing underfunding of annual requirements. Current work that will improve the accuracy of the estimated infrastructure deficit include, the pavement condition assessment project and the storm sewer zoom camera program. Results of this work will be incorporated into the estimated 2020 infrastructure deficit.

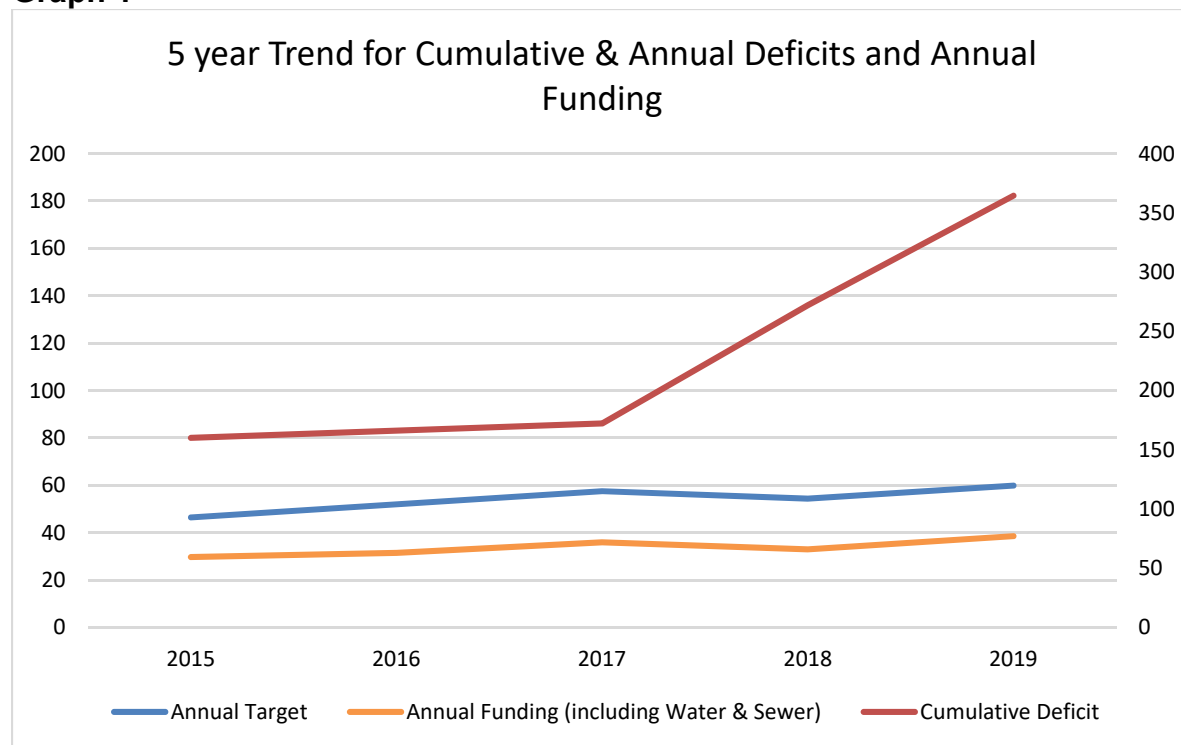
¹ The Canadian Infrastructure Report Card (CIRC) – 2016 report indicated that the replacement value of municipal assets rated as poor or very poor across the country was approximately \$141 billion or 12% of the total replacement value of those assets.

Graph 1 also shows the targeted funding levels² compared to the City's capital budget, which provides an indicative annual funding gap. As the accuracy of these estimates improve the reliability of the trend data will also improve.

Funding programs like the Federal Gas Tax have provided upper level of government grants to support sustainable funding, however there still remains a widening infrastructure gap caused by the shortage of funding for capital. Increasing cumulative infrastructure deficits are a predictor of future increases in risks related to infrastructure and also potentially lower levels of service capacity.

While Council's approval of a separate infrastructure tax levy has increased funding for capital, the City's 2019 annual reinvestment in infrastructure remains below the recommended reinvestment targets (shown in Graph 1 below). Annual reinvestment below the targets reduces current funding requirements but will increase the funding needs in future years. This is contrary to the principle of intergenerational equity, where each generation of citizens would pay their share for the assets that provide their services and benefits.

Graph 1



² Based on the average city rate using 2016 CIRC target rates and the related estimated 2019 replacement values

Reporting Timelines

The Budget Standing Committee (BSC) at its meeting of June 20, 2016, approved a motion that after 2017, capital budget presentations will be provided in the first quarter of the specified budget year. This report complies with this direction for the 2019 capital budget.

Beginning with the 2020 Capital Budget, staff is recommending that the Capital Budget be presented for BSC's consideration in advance of the Operating Budget. This timing is recognized best practice, as decisions resulting from the Capital Budget impact the Operating Budget. Understanding and considering the City's capital needs in advance of how they are funded shifts attention to asset requirements rather than prioritizing within a specified funding envelope. This timing also promotes better recognition of the lifecycle costs of infrastructure. Adoption of the recommended timing would result in the 2020 Capital Budget being considered in the fall of 2019, prior to consideration of the 2020 Operating Budget. Part of the recommendation of this report would allow for staff to proceed with implementing this change in timing for Budget approvals for 2020 and future years.

Report

The 2019 Capital Budget totals \$31,338,887. The Capital budget project details and related funding as well as summary information on the four-year forecast is included in Appendix 1.

The 2019 Capital budget also contains:

- Narrative related to each of the Capital programs
- Capital expenditure amounts
- Narrative related to funding sources of capital assets

The contents of the 2019 Capital Budget, Program Report and Four Year Forecast in Appendix 1 are as follows:

Item	Description	Appendix Page #
Tab 1 Asset Management Background Information		1
Tab 2 2019 Capital Budget Program Details		
Schedule A	Capital Budget Summary (breakdown of 2019 capital expenditures with funding sources)	8
Schedules B – P	2019 Capital Budget detailed expenditures proposed by function, including narrative for each function and the related projects listed.	9 to 48

Item	Description	Appendix Page #
Tab 3 2019 Water and Wastewater Budget Program Details		
Schedules Q – R	2019 Detailed capital expenditures for Water and Wastewater Systems that are included in that rate budget	49 to 53
Tab 4 2019 Operating Budget Improvement Program Details		
Schedules S – U	2019 Operating Budget Improvement Programs (capital expenditures included in the approved 2019 operating budget)	54 to 57
Tab 5 Future Capital Budget and Forecast Information		
Schedules V – X	2020 to 2023 Capital Forecast and other potential projects 2024 and beyond	58 to 60
Tab 6 Funding for Capital Budget		
Schedules Y – AF	Details of grant, reserve, pay –as –you-go, and debt funding. Debt strategy, restrictions and municipal comparators Reprioritized prior four year forecast	61 to 75
Tab 7 Projects by Ward (excluding facilities)		
Schedules AG-AH	Projects by Ward (facilities) Projects by Ward (excluding facilities)	76 to 78 79 to 82

Prioritizing Capital Projects

Consistent with Corporate Strategy, sustainability is the goal for City owned infrastructure assets, which requires that the first priority in the capital budget is providing funds for maintaining existing infrastructure, while the second priority is to expand or replace existing assets aimed at improving services and programs for residents.

The introductory information in each capital budget section provides detail of how capital projects are prioritized within each group of assets. The overarching prioritization process considers deficiencies, target condition ratings, as well as co-ordination with other construction activities in order to minimize impact on the public.

In order to balance cost, levels of service and risk there are a series of asset class specific criteria to assist in determining project prioritization. However, across the asset

classes the general priority criteria are similar; namely that the highest priorities are given to:

- Regulatory requirements
- Health and safety concerns
- Maintenance of the basic asset / preventative maintenance
- Service level requirements or system reliability
- Programming quality of the asset

Capital budget projects which are a result of a current regulation or guideline are as follows:

- Accessibility upgrades (roads and buildings)
- Pollution control plan implementation
- New storm sewers for combined sewer separation; and
- Water/Wastewater Financial Plan

**Table 2 – City of St Catharines – Asset Categories - Replacement Values,
Reinvestment Rates & Target Reinvestment Rates
(in \$millions or %)**

Asset Category	Asset Replacement	2019 Budget				Total Reinvest value		Target Annual reinvestment. value \$(3)	
		Operating(1)	Utilities (2)	Capital	Excl. New/Up - grade	\$	%		
Roads	\$900.0	\$2.331	-	\$6.196	-	\$8.527	28%	\$18.0	\$27.0
Bridges & Major Culverts	47.6	0.145	-	3.865	-	4.010	13%	0.5	0.7
Water Distribution System	1,317.0	-	\$5.7	1.128	(1.128)	5.700	191%	13.2	19.8
Sanitary Sewer Collect. System	900.0	-	2.5		-	2.500	9%	9.0	11.7
Storm Sewer Collection System	716.0	-	-	3.241	(2.823)	.418	2%	7.2	9.3
Sidewalks	139.2	0.350	-	0.350	(0.300)	0.400	1%	2.8	4.2
Facilities & Improved Lands	526.5	.275	-	11.268	(3.30)	8.243	28%	9.0	13.2
Playgrounds / Courts	19.6	-	-	.325	-	.325	1%	0.3	0.5
Total	\$4,565.9	\$3.101*	\$8.2*	\$26.373	(\$7.551)	\$30.123	100%	\$60.0	\$86.4
Other capital expenditures				4.966					
Adjustment to target limit notes (4)								14.9	6.9
TOTAL				\$31.339*				74.9	93.3
Total Capital including new & upgrade *				\$42.64				40%	

Table 2 above shows that the 2019 funding levels for infrastructure reinvestment in all budgets represents 40% of the estimated annual lower reinvestment target. With annual allocation for capital asset renewal and improvement below the sustainable

(1) As per Appendix 1 Tab 4, schedules S,T,U & Tab 2 schedule J (narrative)

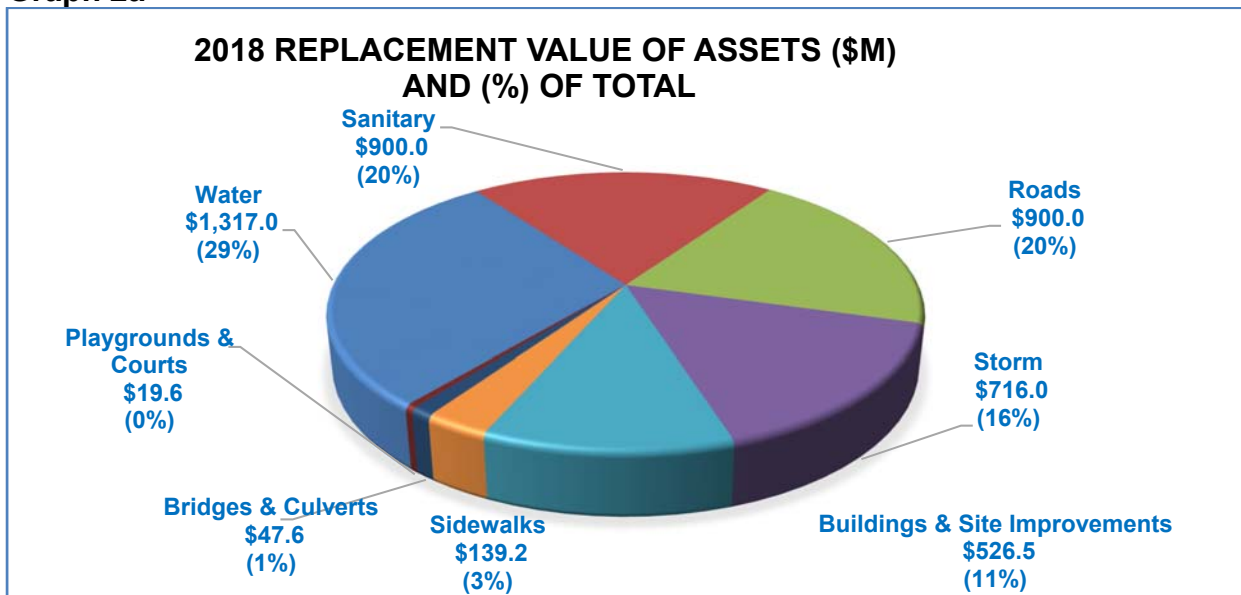
(2) As per Appendix 1 Tab 3, schedules Q & R

(3) Based on Target Average Annual Reinvestment - Source: Canadian Infrastructure Report Card 2016- see table 1 for the percentages

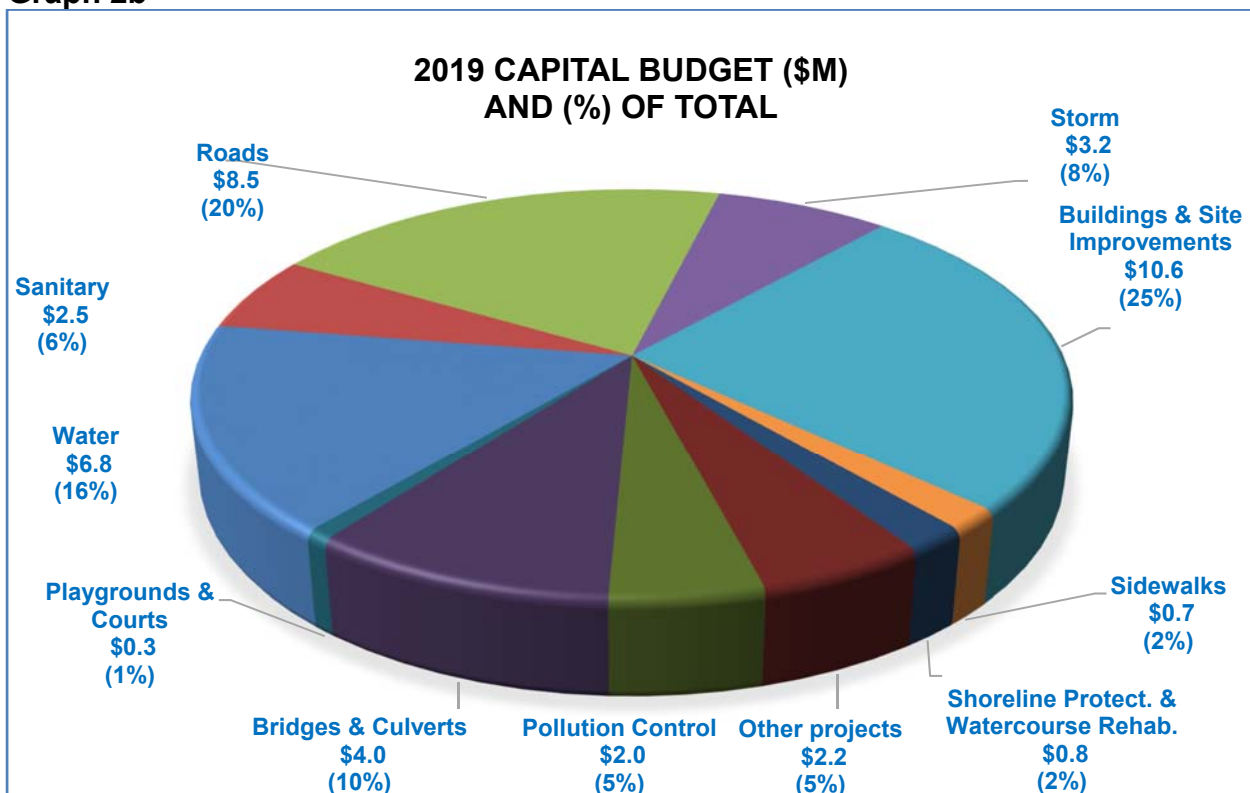
(4) Adjustment for the City's estimated sustainable reinvestment rates being higher than CIRC (see Appendix 1 Tab 1 table 2 notes 3 & 4)

levels of funding required, available funding is shared among all assets categories to address highest priorities and manage risk. As expected the annual budget support of the various asset categories approximates the percentages for the replacement value of each asset category. (see Graph 2a & 2b below).

Graph 2a



Graph 2b



The receipt of grants for specific asset categories, and specific asset priorities, create differences between the reinvestment percentages and the asset replacement value percentages.

Of the total \$42.64 million being spent on capital (as shown in Table 2 above), the majority of the budget relates to renewal of assets. The budget for new assets includes the Fire training centre (\$2.2 million), Corporate Accommodations Project (\$1.1 million), additional storm sewers (legislatively required and \$3.241 million) and Canada games (\$320,000 for 4 years totaling \$1.28 million). When comparing the annual target reinvestment rates of \$60 to \$86.4 million to the City's 2019 reinvestment rate of \$42.64 million, the \$7.551 million investment in new assets should be excluded, which further increases the annual reinvestment gap. As previously discussed in the background section of this report, the main limitation on asset renewal and improvement is available funding.

Work continues on the Asset Management Plan to support the City in achieving a sustainable level of funding for all levels of infrastructure.

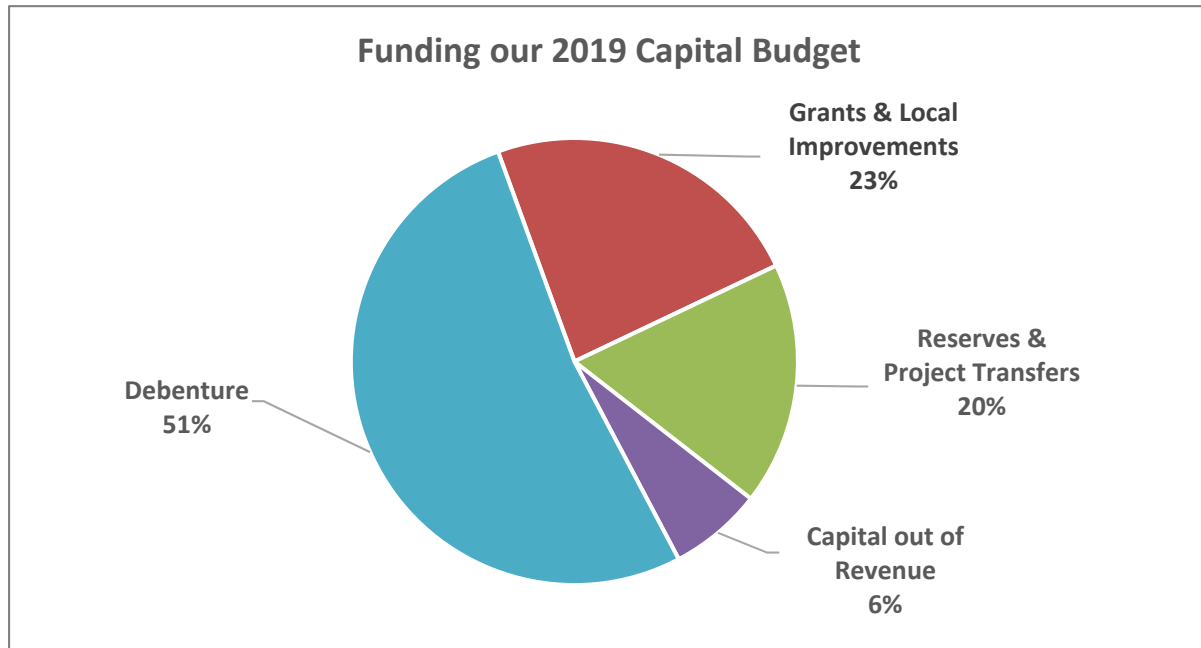
Items not included in 2019 Capital Budget and 2020 to 2023 Forecast

There have been discussions regarding an Arena Complex for the 2021 Canada Summer Games. However, this item has **not been** included in the 2019 Capital Budget or the Four Year Forecast. This item will be the subject of future Council Reports as details are further refined.

Also the Facer Street environmental assessment is excluded. As there is still at least five years of residual life left in the road, it would be premature to complete an Environmental Assessment (EA) at this time. (see appendix 3 for details on EA costs and preliminary reviews.)

Funding Capital Assets

The Capital Budget is funded first by available grants, reserves, operating fund contributions and the remainder is funded by debt. Figure 3 below depicts the split between funding sources.

Figure 3 - Funding sources of 2019 Capital Budget

After applying external grants, the City has the option to use pay-as-you-go financing for infrastructure, however with the high cost of capital projects, using this funding source alone would result in significant underfunding of infrastructure and a significant impact on the operating budget. Increasing the annual operating budget support of the capital out revenue (tax levy) from 10% to a higher percentage is a method to increase the amount of sustainable funding provided for the City's capital assets. Increasing the capital out of revenue in 2019 operating budget could not be accommodated. This will continue to be reviewed by staff in future budget years. As discussed, moving the approval of the capital budget prior to the operating budget will allow for more fulsome consideration of these options to address the funding of the City's infrastructure deficit. In addition, staff apply for various grants as they become available from upper levels of government to support renewal and aid in additional investment in our capital assets.

A separate annual infrastructure tax levy annually has been implemented to help address the existing infrastructure deficit. The projects funded from the 2019 Infrastructure Levy are shown in Appendix 1, Tab 6, Schedule AA. Increasing the Infrastructure Levy percentage in future years is another method to raise the amount of sustainable funding provided for the City's capital assets.

As shown in the graph in Figure 3 above, debt continues to be used to finance the major portion of the City's Capital Budget as it allows for the cost of large long-life assets to be spread over several years. Annual debt repayments are spread over a longer term, however, in compliance with current Council policy debentures are taken for shorter

timeframes, typically between five or ten years. The amortization periods are shorter than the timeframe that the public benefits from the use of asset.

Restrictions on Debt Issuance

Ontario Regulation 403/02 Debt Repayment Limit, restricts annual debt charges to no more than 25% of the City's net revenue fund revenues. The City Treasurer has updated the Limit for the City and the debenture amount proposed in the 2019 Capital Budget will not cause the City to exceed its limit. This updated document is available in Appendix 1, Tab 6, Schedule AD. Using this Limit calculation there is significant remaining debt capacity of at least \$135.2 million.

The Ministry of Municipal Affairs (MAH) has also issued guidance stating that debt costs between 5% and 10% are considered moderate risk, which is where the City of St. Catharines currently resides based on the most recent report from MAH. The City has adopted a strategy aimed at containing debt charges at approximately 10% of the City's total own expenditures. Given the current debt charges as a percentage of operating budget are almost 9% and the 2023 forecast is slightly over 10%, very little debt capacity remains within the City's self-imposed guideline; however, within the Provincial Regulation there is ample debt capacity. (See Table 3 below)

Table 3 Debt Outstanding and Debt Ratios

	2018	2019	2020	2021	2022	2023
Outstanding Debt	\$119.1m	\$122.6m	\$124.4m	\$123.2m	\$122.9m	\$126.0m
Debt Per Household	\$2,030	\$2,089	\$2,121	\$2,100	\$2,095	\$2,149
Total Annual Debt Charges	\$15.8m	\$16.7m	\$17.7m	\$19.0m	\$ 20.6m	\$20.6m
As % of Operating Budget	8.95%	9.21%	9.52%	9.93%	10.50%	10.24%

Conservative assumptions are employed to generate the estimates above. Any changes made to any of the assumptions – whether that is timing, term or interest rate for debt issuances, or the level of capital spending supported by debt, will have an impact on the debt charges as a percentage of expenditures, projected debt balance and future tax levy impacts. As a result, actual results will vary from the estimates provided above.

In addition to the quantitative debt limits, the current Debt Management Strategy also outlines other restrictions on the types of asset eligible for debt financing, and requires a 10% down payment through tax levy for each capital project funded by debt. The debt financing proposed for the 2019 capital budget complies with these requirements.

Prior Year Capital Budget Approvals

Council, at the December 19, 2016 meeting, authorized the recommendation for early budget approval in the amount of \$320,000 per year for 2018, 2019, 2020 and 2021 for the 2021 Canada Summer Games. The bid for the games was awarded to Niagara in 2017, and the funds will be targeted towards capital facility investments as detailed in report PRCS/CAO-320-2016. The \$320,000 annual budget has been included in the four year forecast (See Appendix 1, Schedule B for portion included in the 2019 Capital Budget).

Reprioritization of Capital Budgets

Reprioritization of projects is ongoing to meet the Council adopted policy of containing the debt charges at approximately 10% of the City's total expenditures and to meet the changing needs of the City. Similar to previous years and again in 2019, funding is also transferred from previously approved projects to fund the current priorities. Proposed for 2019 is the Fire Training Tower, Fire Training Centre and the Water Filling Stations in various City Parks to be funded with unutilized amounts from previously approved capital projects. Additionally, funds from the previous Lake Street Service Centre Project and Fire Station 4 project are being repurposed for Carlisle Street Garage fit out for use by City Departments that are being displaced. Council's approval of the 2019 Capital Budget, also provides approval for these budget transfers. Details of reprioritized capital projects are attached in Appendix 1, Tab 6, Schedule AA.

Financial Implications

The 2019 Capital Budget, Program Report and Four Year Forecast allows the Corporation to address a portion of the capital needs of the Corporation. Longer term as well as current financial implications are outlined within the report.

Relationship to Strategic Plan

The 2019 Capital Budget and Program Report reflects and implements many of the Strategic Plan's goals and actions. Renewal of existing infrastructure and newly constructed infrastructure is critical to a municipality. The recommendations of this report support the following:

Economic Stability:

Goal 1: Attracting public investment

- By providing safe, reliable, attractive infrastructure and by providing employment opportunities to locally owned companies.

Goal 2: Strive for the highest quality of life for all citizens

- Action 2.4 Optimize capital infrastructure through effective asset management and sustainable investment.

Social Sustainability:

Goal 4. Strive for the highest quality of life for all citizens. The construction program addresses all areas of the City's infrastructure.

- Action 4.1. Implementing a recommendation of the Parks and Recreation Master Plan.
- Action 4.4. Implement the St. Catharines accessibility plan, specifically at the George Taylor Field Washroom and Changeroom use Ontario's built environment standard as a minimum standard during sidewalk replacements and the construction of new sidewalks.

Goal 5: Connect people, places and neighbourhoods

- Action 5.1. To be a city that embraces connectivity between people, places and neighbourhoods.
- Action 5.2. Establish a complete neighbourhoods matrix to ensure that redevelopment of properties enhances the livability of neighbourhoods. All road reconstructions will be designed in consideration of the "Complete Streets" philosophy.
- Action 5.3. Accelerate the implementation of active transportation networks that link people with parks, trails and waterfront and support the development of complete streets.

Environmental Sustainability

Goal 7: Lead in the protection of our environment for future generations.

- Action 7.1. Establish a plan to integrate green energy and sustainable technologies in municipal infrastructure and program delivery.
- Action 7.2. Continue to achieve the tree canopy targets in the city as identified in the Urban Forestry Management Plan. All road reconstruction and new building projects include provisions for greening. All other infrastructure includes funding for replacement of any trees removed by the proposed construction.
- Action 7.3. Increase protection of our water resources by working with partners to reduce pollution and ensure long-term sustainability. CSO reductions proposed by the Burleigh Hill CSO Project and storm sewer separation projects will directly improve water quality in our watercourses.
- Action 7.4. Maintain and enhance the health and accessibility of our waterfront through the waterfront trail system, municipal beaches, rivers and creeks, is supported by the Shoreline Protection Program (Appendix 1 Tab 2, schedule L)

Goal 8: Be prepared for the impacts of climate change.

- Action: 8.1. Review and update all municipal operations to minimize the impacts of and ensure preparation for climate change including winter control procedures and flooding. All sanitary sewer and storm sewer designs are undertaken using updated design criteria.

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Appendices:

Appendix 1 – 2019 Capital Budget, Program Report and Four Year Forecast
Appendix 2 – Asset Management Working Group Mandate and Membership
Appendix 3 – TES – Facer Street EA

2019 Capital Budget, Four Year Forecast and Program Report**Table of Contents****1. Asset Management Background Information****2. 2019 Capital Budget Program Details - \$31,338,887**

Schedule A - Capital Budget Summary

Schedule B - Parks, Recreation and Culture Services
- Adjusted for non-building related asset improvements only

Schedule C - Road & Construction Improvement

Schedule D- Storm Sewer and Sewer Separation Projects

Schedule E - Regional Projects - City Share

Schedule F- Water Improvements

Schedule G - Niagara District Airport

Schedule H - Sidewalk Construction Program

Schedule I - Pollution Control Plan

Schedule J - City Building and Accessibility Improvements

Schedule K - Watercourse Rehabilitation Program

Schedule L - Shoreline Protection Program

Schedule M - Bridge and Culvert Rehabilitation

Schedule N - Corporate Infrastructure

Schedule O - Parking Services

Schedule P - Transit Commission Services

3. 2019 Water / Wastewater Budget Program Details - \$8,200,000

Schedule Q - Sanitary Sewer Improvement Program

Schedule R - Water Systems Improvement Program

2019 Capital Budget, Four Year Forecast and Program Report

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4. 2019 Operating Budget Improvement Program Details - \$2,826,000

Schedule S- Road and Drainage Improvements

Schedule T - Bridge and Culvert Rehabilitation Program

Schedule U - Sidewalk Construction Program

5. Future Capital Budget & Forecast Information

Schedule V - 2020 Capital Forecast

Schedule W - 2019 Capital Budget and Four Year Forecast

Schedule X - Other Potential Capital Projects - 2024 and Beyond

6. Funding our Capital Budget

Schedule Y - Funding our Capital Assets

Schedule Z - Grant Funding Details

Schedule AA - Funding From Reserves and Project Transfers

Schedule AB - Capital Budget and Debt

Schedule AC - Debt Management Strategy

Schedule AD - Debt and the Municipal Act

Schedule AE - Capital Budget and Debt - Municipal Comparators

Schedule AF - Reprioritized Prior Four Year Forecasts

7. 2019 Construction Program - Ward Listings

Schedule AG - 2019 Construction Program by Ward Listing - Non-Facilities

Schedule AH - 2019 Construction Program by Ward Listing - Facilities

Asset Management

The City's Asset Management Plan (AMP) was presented to Council in 2013. At that time, the AMP included the major asset categories of roads, bridges & major culverts, water mains, sanitary sewers and facilities. Subsequently, staff have, with limited resourcing, continued to refine and update the City's AMP, however several asset classes and subclasses are not represented in this report. These assets include:

Fleet	Watercourse improvements
Trees (Street and Park)	Niagara District Airport assets
Street lights	Traffic lights & signage
Shoreline protection	Recreational trails
Other Corporate Assets (Furniture, IT equipment, etc.)	Street and Park Furniture (Waste Receptacles, Benches)

While not incorporated into this report, information regarding these assets is maintained to varying degrees by staff to inform decisions regarding the rehabilitation or replacement of those assets. For example, information is maintained regarding the operational and replacement costs and ultimate replacement timing for our various fleet equipment. Funding for fleet replacement is incorporated as part of various equipment reserves in the Operating Budget.

As staff resourcing allows, these additional asset classes and subclasses will be incorporated into a more comprehensive AMP. In addition, with the new legislation from the Province related to Asset Management, TES and FMS staff have been working together in 2018 and will work together onward to ensure that the City of St. Catharines meets all of the various milestones required by the new Provincial legislation.

Infrastructure Deficit

In 2016, the Federation of Canadian Municipalities (FCM), in cooperation with the Canadian Construction Association (CCA), the Canadian Public Works Association (CPWA) and the Canadian Society for Civil Engineering (CSCE) published the *Canadian Infrastructure Report Card – 2016 (CIRC – 2016)*.

The CIRC – 2016 assessed the state of municipal roads and bridges, public transit, buildings, sport and recreation facilities, stormwater, wastewater and potable water infrastructure, providing a snapshot in time of the replacement value, condition (good, fair, poor, etc.) and reinvestment monies being spent on those asset categories by municipalities across Canada. The City of St. Catharines was one of a total of 120 survey respondents from municipalities across Canada that helped to inform the report.

The City's Infrastructure Deficit for a particular asset is being defined as the replacement value of known deficiencies for that asset. Depending upon the asset, this may include assets rated as poor or very poor or as a high priority building deficiency identified through condition assessments. A summary of the infrastructure deficit by asset category for the City is provided in Table 1 below.

Table 1 – City of St Catharines – Asset Categories - Replacement Values & Infrastructure Deficit

Asset Category	Asset Replacement		Infrastructure Deficit	
	Value (\$M)	% of Total	Value (\$M)	% of Replacement Value
Roads	\$900.0	19.7%	\$59.0	6.6%
Bridges & Major Culverts	47.6	1.0%	8.6	18.1%
Water Distribution System	1,317.0	28.8%	62.0	4.7%
Sanitary Sewer Collection System	900.0	19.7%	119.7	13.3%
Storm Sewer Collection System	716.0	15.7%	55.8	7.8%
Sidewalks and Walkways	139.2	3.0%	7.0	5.0%
Facilities & Improved Lands	526.5	11.5%	46.2	8.8%
Playgrounds / Courts	19.6	0.4%	6.2	31.6%
Total	\$4,565.9	100.0%	\$364.5	8.0%

The City's current infrastructure deficit of approximately \$364 million represents approximately 8% of the replacement value of those asset categories. This value has increased by \$92 million from the 2018 values, largely due to the refinement of estimates in most asset classes. However, the largest increase is attributable to the sewer assets (\$37 million). This increase reflects the preliminary findings of an ongoing system wide zoom camera technology inspection. The \$27.5 million increase in the Water Distribution system's total replacement value, is largely due to construction cost increases at a rate greater than the inflation rate.

The CIRC – 2016 report indicated that the replacement value of assets rated as poor to very poor across the entire country was estimated to be approximately \$141 billion or 12% of the total replacement value of those assets. With the City's infrastructure deficit currently below the national average, the infrastructure deficit needs to be considered in relation to sustainable funding and annual reinvestment targets for the City's assets, as the City's current contributions are below the recommended reinvestment targets.

Sustainable or Reinvestment Funding

Each asset category has a life expectancy, and a need to renew these assets on a regular basis. Generally speaking, the replacement value divided by the expected life provides an approximation of the anticipated level of annual sustainable funding. The CIRC – 2016 report provides a lower and upper range of suggested annual reinvestment rates (based as a percentage of replacement value) for various asset

classes. A summary of the sustainable, or target reinvestment rates for the City's assets are presented in Table 2 below.

Table 2 – City of St Catharines – Asset Categories - Replacement Values, Infrastructure Deficit, Reinvestment Rate & Renewal Budget

Asset Category	Asset Replace-ment	Infra-struct-ure Deficit	Target Reinvestment Rate (1)				2019 Total Renewal all Budget (\$M)(2)
	Value (\$M)		Lower %	Upper %	Lower \$M	Upper \$M	
Roads	\$900.0	\$59.0	2.0%	3.0%	\$18.0	\$27.0	\$8.5
Bridges & Major Culverts	47.6	8.6	1.0%	1.5%	0.5	0.7	4.0
Water Distribution System(3)	1,317.0	62.0	1.0%	1.5%	13.2	19.8	5.7
Sanitary Sewer Collect. System	900.0	119.7	1.0%	1.3%	9.0	11.7	2.5
Storm Sewer Collection System	716.0	55.8	1.0%	1.3%	7.2	9.3	0.42
Sidewalks(4)	139.2	7.0	2.0%	3.0%	2.8	4.2	0.4
Facilities & Improved Lands	526.5	46.2	1.7%	2.5%	9.0	13.2	8.2
Playgrounds / Courts	19.6	6.2	1.7%	2.5%	0.3	0.5	0.3
Total	\$4,565.9	\$364.5	1.3%	1.9%	\$60.0	\$86.4	\$30.1
Adjustment for notes (3) &(4)					14.9	6.9	-
					\$74.9	\$93.3	\$30.1
% of lower limit							40%

Notes:

(1) - Lower and Upper Target Average Annual Reinvestment - Source: Canadian Infrastructure Report Card 2016.

(2) - Includes amounts from operating, capital, and water and wastewater budgets, excluding 2019 Funding for new infrastructure (ie Fire training center, consolidation of City staff project, new storm sewers new water distribution and new sidewalks).

(3) - City's estimated sustainable reinvestment rate is higher at 2% for the target limit because City has a lower estimated life expectancy for the water distribution system, increasing the target to \$26.3 million.

(4) - City's sustainable reinvestment rate is higher at 3.3% for the target limit because the City has a lower estimated life expectancy for sidewalks increasing the target to \$6.6 million.

As highlighted above and discussed further in the 2019 Capital Budget and Program Report, funding levels still fall well short of the sustainable levels of funding required with 2019 funding for capital from all budgets represents 40% of the estimated lower funding target. These annual shortfalls impact the City's ability to sustain its current levels of service and increase risk of asset failure contribute to the City's growing Infrastructure Deficit.

Figure 1, 2 & 3 - Comparison of Replacement Value, Infrastructure Deficit, Sustainable Funding and 2019 Budget Allocation by Asset Category

Figure 1.

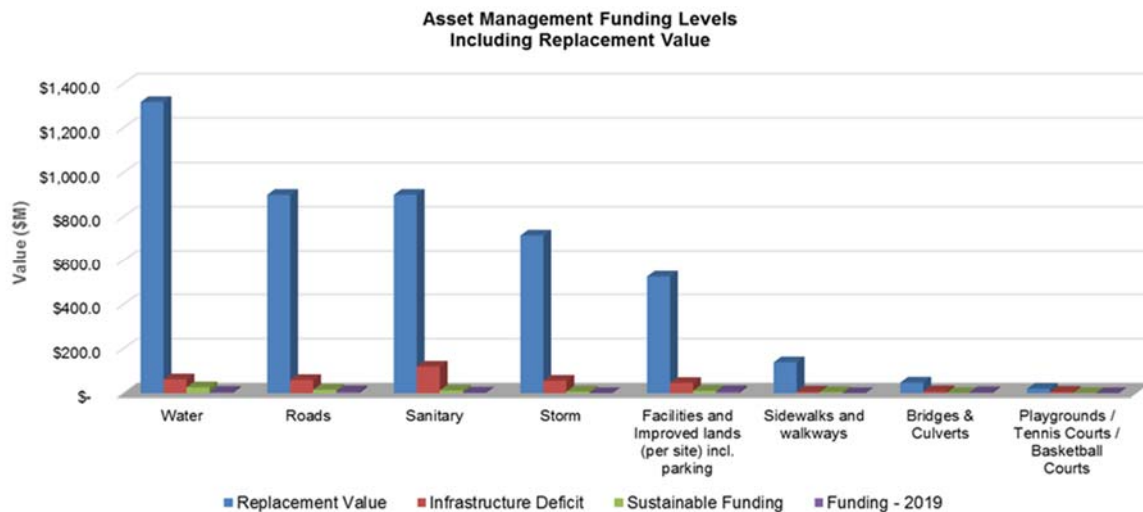


Figure 2.

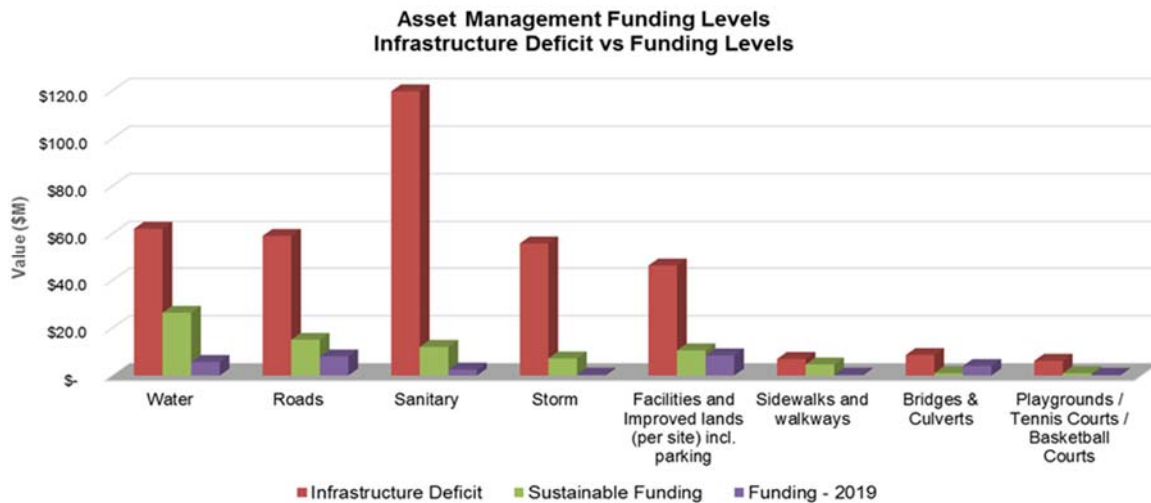
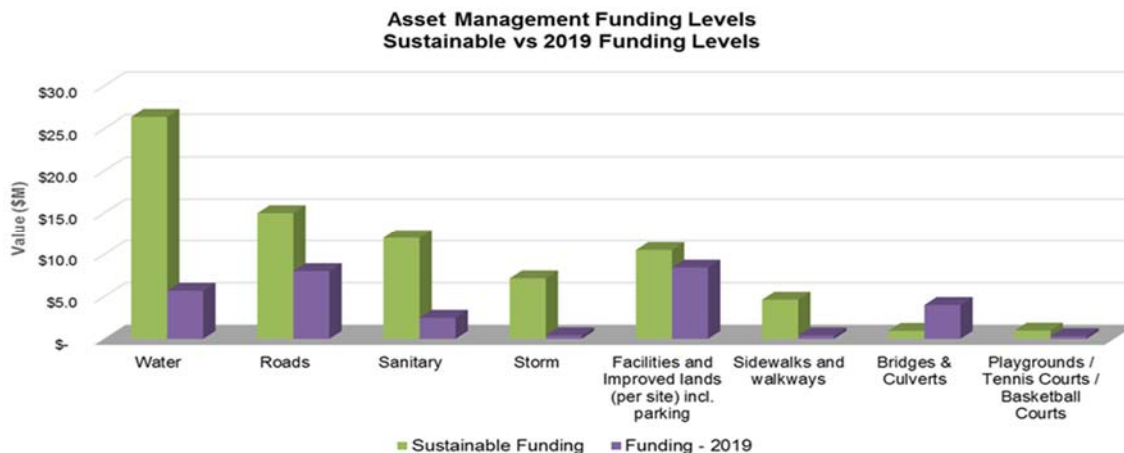


Figure 3.



Prioritization of Construction Programs

Prioritization of construction for each asset class addresses reducing corporate risk. There are a series of criteria specific for each asset class that assist in determining which projects to proceed with. As an example, pavement condition ratings provide input into the pavement management system that provides suggested roads to be rehabilitated; water main break rates prioritize the replacement of water mains, etc. However in all classes, the general priority criteria are similar; namely that the highest priorities are given to:

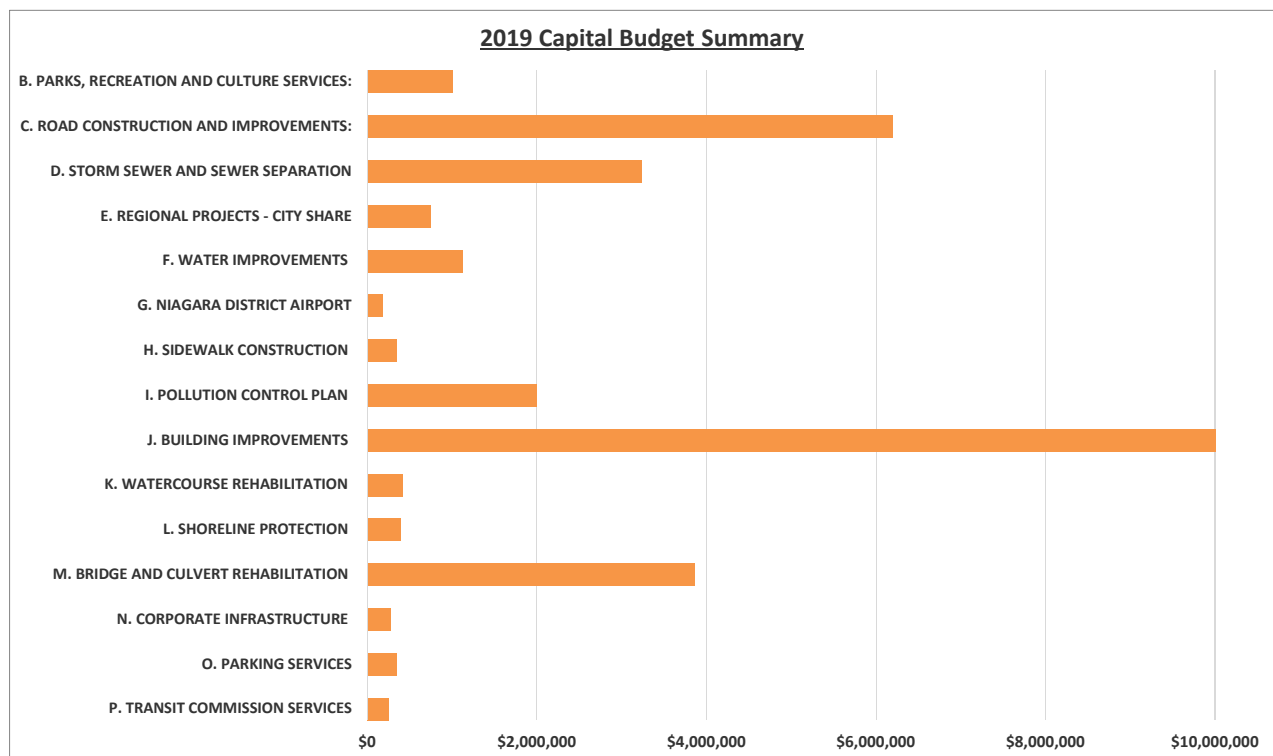
- Regulatory requirements
- Health and safety concerns
- Maintenance of the basic asset / preventative maintenance
- Service level requirements or system reliability
- Programming quality of the asset

The introductory paragraphs of each capital budget section provide additional details of how these general priority criteria are applied to each asset class.

For asset categories within the road allowance (roads, sidewalks, sewers, water mains), coordination with other construction activities is also always taken into consideration to take advantage of economies of scale and reduce the impact of construction on abutting property owners and the travelling public. This coordination may impact the timing of asset renewal for some asset categories. As an example, the need to rehabilitate a certain section of roadway may accelerate the timing of sewer or water main replacement under that section of roadway. Determination of the final construction program for all of the assets for a particular year is the result of an iterative effort by staff in reviewing all of the asset categories.

**CITY OF ST. CATHARINES
SCHEDULE A - 2019 CAPITAL BUDGET SUMMARY**

DESCRIPTION	SCHEDULE	ESTIMATED GROSS COSTS	LOCAL IMPT'S/ GRANTS	PROJECT TRFRS/ RESERVES	CAPITAL OUT OF REVENUE	NET DEBENTURE AMOUNT
<u>PARKS, RECREATION AND CULTURE SERVICES:</u>	SCHEDULE B	1,009,000	-	689,000	32,000	288,000
<u>ROAD CONSTRUCTION AND IMPROVEMENTS:</u>	SCHEDULE C	6,196,420	250,000	250,000	570,000	5,126,420
<u>STORM SEWER AND SEWER SEPARATION</u>	SCHEDULE D	3,241,000	1,291,200	-	196,000	1,753,800
<u>REGIONAL PROJECTS - CITY SHARE</u>	SCHEDULE E	750,000	-	-	75,000	675,000
<u>WATER IMPROVEMENTS</u>	SCHEDULE F	1,128,000	1,020,500	-	11,000	96,500
<u>NIAGARA DISTRICT AIRPORT</u>	SCHEDULE G	181,800	-	-	18,000	163,800
<u>SIDEWALK CONSTRUCTION</u>	SCHEDULE H	350,000	-	50,000	30,000	270,000
<u>POLLUTION CONTROL PLAN</u>	SCHEDULE I	2,000,000	800,000	-	120,000	1,080,000
<u>BUILDING IMPROVEMENTS</u>	SCHEDULE J	10,918,000	1,300,000	5,263,000	673,000	3,682,000
<u>WATERCOURSE REHABILITATION</u>	SCHEDULE K	420,000	-	-	42,000	378,000
<u>SHORELINE PROTECTION</u>	SCHEDULE L	400,000	-	-	40,000	360,000
<u>BRIDGE AND CULVERT REHABILITATION</u>	SCHEDULE M	3,864,667	2,488,147	-	137,000	1,239,520
<u>CORPORATE INFRASTRUCTURE</u>	SCHEDULE N	280,000	-	-	28,000	252,000
<u>PARKING SERVICES</u>	SCHEDULE O	350,000	-	-	35,000	315,000
<u>TRANSIT COMMISSION SERVICES</u>	SCHEDULE P	250,000	-	-	50,000	200,000
		<u>\$31,338,887</u>	<u>\$7,149,847</u>	<u>\$6,252,000</u>	<u>\$2,057,000</u>	<u>\$15,880,040</u>



Parks, Recreation and Culture Services – Non Building Assets Schedule B

Parks, Recreation and Cultural services will be involved in a number of capital projects in 2019.

Multi year commitments:

Canada Summer Games

In the spring of 2017, Niagara was named the host of the 2021 Canada Summer games. The capital budget dollars set aside for this event is the second of a four year commitment the City has made for this unique occasion that will take place in our City.

Funds will be targeted towards capital facility investments as detailed in report PRCS/CAO-320-2016. The \$320,000 annual commitment set aside for this event has been included in the four year forecast for Parks, Recreation and Culture Services.

Annual program

The other projects included in the 2019 capital budget relate to park improvements through replacement of playground equipment and basketball courts. In 2019, the playground equipment at Douglas Park is scheduled for replacement, and the Barley Drive Park basketball court will be improved.

Tree Planting

The City of St Catharines plants trees to help improve air quality and add economic value, beauty and shade to its neighbourhoods. Trees are planted on public boulevards, road allowances fronting residences and in circles where trees are currently not planted.

In 2018 the City of St Catharines held two tree giveaways and with the current tree canopy at 21%, these capital dollars assist towards increasing that percentage on both public and private property.

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET

SCHEDULE B: PARKS, RECREATION AND CULTURE SERVICES - NON-BUILDING ASSETS

<u>Project / Description</u>	<u>Gross Cost</u>	<u>Remarks</u>
Additional Budget Dollars for Previously Approved Projects		
Canada Summer Games commitment - Year 2 of 4	\$320,000	Funding towards the Canada Summer Games
Budget Dollars for New Projects		
* Parks Renewal Program, 2019 Douglas Park Playground Equipment Barley Drive Park Basketball Court	325,000	Park and court improvements
* Tree Planting Program	364,000	Tree Planting efforts across the City to increase our canopy percentage.
	<u>\$1,009,000</u>	
 <u>Funding Source Breakdown:</u>		
Capital Out of Revenue (10%)	\$32,000	
* Reserves - Infrastructure levy	689,000	
Debentures	288,000	
	<u>\$1,009,000</u>	

Road Construction and Improvement Program

Schedule C and Schedule S

The City owns and maintains 564 km of roadway or 1,150 km of traffic lanes. The current replacement cost is estimated at \$900 million, in 2019 dollars. Over 98 per cent of the City's roadways are made up of asphalt, asphalt on concrete or concrete surfaces.

The City uses a pavement management software to guide long-term planning for the road system. The software predicts the condition of roads into the future, based on historical data collected on how similar roads have performed in the past, and the risks of delaying treatments. Generally, this means that priority is given to roads with higher volumes and which in turn means higher risk to the City and the residents. The software also generally recommends resurfacing rather than more costly reconstruction.

Of the City's roadways, approximately 27% are rated in excellent or good condition, 31% are rated in fair condition, and 41% rated in poor condition. Approximately 1% of the roads have received a fail rating. The existing pavement conditions for various streets are given a pavement condition rating (PCR) of between 1 and 100 (with 100 being a newly reconstructed road). Council approved a "target" average PCR of 75 for the entire road network. Depending upon the class of road, a PCR of 75 is typically a pavement approximately 10-20 years old with few cracks. The average PCR for the City's entire road network is currently 68.

Given the current PCR, further declines in the rating is expected within 10 years if funding remains at 2018 levels. To address this scenario, the City's Asset Management Plan recommended the implementation of a 4% annual funding increase. This increase would help address the aging road network and work towards attaining desired levels of service. The current roadway infrastructure deficit is estimated to be \$59 million.

Based on the \$8.527 million of 2019 funding for this program and comparison to the minimum annual target reinvestment rate of \$18 million, the City is supporting reinvestment at approximately 47% of the recommended target.

The funding for the Road Construction and Drainage Improvement Program comes from two budgets:

- Capital budget (Schedule C) - \$6.196 million - funds major road works, such as complete reconstruction and road resurfacing. Many projects are coordinated with other major asset improvement projects occurring in the same geographical area. The Road and Drainage Program includes:
 - \$.250 million for a traffic signal repair program supported by the

Infrastructure Levy.

- \$.650 million to initiate engineering studies to investigate a potential secondary access to Moffat Street neighbourhood as directed by City Council at its meeting of May 23,2018.
- Operating budget (Schedule S) - \$2.331 million - supports city wide concrete and asphalt resurfacing or spot repairs, concrete road repairs, curb repairs, milling, crack sealing and guardrails.
 - this funding was approved by Council in December, 2018 as part of the 2019 operating budget. Specific project details are provided within this document in Schedule S.

Consistent with the City's approach to completing more resurfacing than reconstruction, the 2019 budget includes \$2.372 million for resurfacing 6.2 km of roadway ((\$1.501 million funded by the operating budget and \$.871 million funded by the capital budget)

In 2018 Stantec Engineering was retained to conduct a detailed, objective, condition survey of all our roads and update the City's pavement management system. As part of this project a new City's pavement management software and an update of strategies used to best maintain the City's roads will be provided. The results of this project are anticipated early 2019, and will provide more reliable data, which may result in noticeable changes to the future years' overall road deficiency and long term targets.

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET

SCHEDULE C: ROAD CONSTRUCTION AND IMPROVEMENTS

<u>Project / Location</u>	<u>From</u>	<u>To</u>	<u>Gross Cost</u>	<u>Remarks</u>
Additional Budget Dollars for Previously Approved Projects				
P13-067 Moffatt Street Reconstruction	North Limit	South Limit	\$2,660,000	Additional funds for construction. Design and utility relocations previously budgeted
P15-067 Third Street Louth Road Improvements	South Service Road	Hutton Road	200,000	Additional funds for construction. External funding from Ontario Municipal Commuter Cycling Program previously approved
P17-067 St Davids Road	Burleigh Hill Drive	Townline Road	400,000	Additional funds for construction in partnership with the City of Thorold and in conjunction with underground improvements
P18-045 St. Paul Street At William Street - Intersection And Plaza Improvements	William Street		45,000	Intersection improvements in conjunction with externally funded 40 St Paul Street plaza and pedestrian crossing. Top asphalt to be budgeted in 2020 with road resurfacing
P18-061 Gale Crescent Road Improvements	Geneva Street	Oakdale Avenue	100,000	Rebudget of engineering services for environmental assessment and design for future road reconstruction
P18-062 Lake Street Reconstruction	Welland Avenue	Queen Street	100,000	Rebudget of engineering services for environmental assessment and design for future road reconstruction
P18-063 Merritt Street Road Improvements	Almond Lane	Walnut Street	100,000	Rebudget of engineering services for environmental assessment and design for future road reconstruction
Subtotal:			<u>\$3,605,000</u>	
Budget Dollars for New Projects				
Environmental Assessment and Design				
P19-065 Ridley Road Realignment And Road Improvements	Louth Street	Henrietta Street	\$400,000	Engineering services and utility relocations for future road reconstruction estimate cost of reconstruction
P19-066-1 Moffatt Street Secondary Access	Location to be determined		650,000	Engineering services for an environmental assessment and preliminary designs for a possible 2nd access into neighbourhood. See Council May 23, 2018, Item 5.3
P19-067 Arglye Crescent Road Improvements	Collier Street	Westchester Cres	50,000	Engineering services for future road works

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET

SCHEDULE C: ROAD CONSTRUCTION AND IMPROVEMENTS

<u>Project / Location</u>	<u>From</u>	<u>To</u>	<u>Gross Cost</u>	<u>Remarks</u>
Road Improvements				
P19-002				
Hampstead Place Road Reconstruction	Carlton Street	Manchester Avenue	120,000	Road reconstruction in conjunction with underground improvements
P19-080				
2019 Resurfacing Program			871,420	
Brackencrest Road	Woodrow Street	St Augustine Drive		Curb repairs and drainage improvements, milling and road resurfacing
Dorchester Blvd	Carlton Street	Meredith Drive		Spot curb repairs, milling and road resurfacing
Glen Morris Drive	Jacobson Avenue	Briarsdale Drive		Top course asphalt following 2018 underground improvements.
Heritage Court	Huntington Lane	End		Spot curb repair, milling and road resurfacing and drainage improvements
Meredith Drive	Rendale Avenue	Ancaster Blvd		Curb repairs, drainage improvements, milling and road resurfacing
Merigold Street	St Paul Street W	Chetwood Street		Spot curb repairs, milling and road resurfacing
Welland Avenue	Dieppe Road	Grantham Avenue		Spot curb repairs, milling and road resurfacing
Various				
2019 Traffic Signal Program				
* Locations to be Determined			250,000	Priority determined by the Region
Active Transportation Infrastructure				
** P19-069				
Locations to be Determined			250,000	Multi year initiative to implement recommendations of the Active Transportation Committee
Subtotal:			<u>\$2,591,420</u>	
Grand Total:			<u>\$6,196,420</u>	
<u>Funding Source Breakdown:</u>				
Capital Out of Revenue (10%)			\$570,000	
** Grant - Federal Gas Tax			250,000	
* Reserves - Infrastructure Levy			250,000	
Debenture			<u>5,126,420</u>	
			<u>\$6,196,420</u>	

Legend:

P - Project

ST - Study

- Year of Commencement

Ex. P13-067 = Project, 2013 - 67 (Identified project)

Storm Sewer Program

Schedule D

The total length of storm sewers in the City is approximately 404 km with current estimated replacement value of \$716 million, in 2019 dollars.

As previously reported to Council, storm sewer separation forms a significant component of a multifaceted approach to alleviate basement flooding problems.

The construction of storm sewers is concentrated in combined sewer areas which experienced recent basement flooding or have combined sewage overflow. The new storm sewers will reduce the amount of surface water entering the sanitary or combined sewer system thereby providing relief to the immediately adjacent areas as well as the properties upstream and downstream of the new sewers. New storm sewers are also proposed in areas with surface flooding when open channel solutions cannot be effective.

Staff recommend very few newly identified projects in 2019. The majority of the funds recommended in the 2019 budget are additional funds for projects previously approved (and not yet tendered).

The project requiring the largest amount of additional funding is a storm sewer on Else Street. Initial design efforts have identified some limitations due to existing infrastructure and nature of the area in relation to storm water.

Unlike sanitary and combined sewers, storm sewers are not inspected and as a result it is more difficult to assess their condition. A zoom camera inspection program is currently underway to prioritize further investigations, maintenance and replacements. Approximately 32% of the total length of the storm sewer system has been inspected with a zoom camera at this time. Preliminary data for the inspected sewers (mostly north of the QEW) has indicated that approximately 7.8% of the existing storm sewer system is in a poor condition.

Based on an estimated life cycle of a hundred years, the annual sustainable reinvestment rate for the storm sewer system should be approximately \$7.2 million.

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET
SCHEDULE D: STORM SEWER AND SEWER SEPARATION PROJECTS

<u>Project / Location</u>	<u>From</u>	<u>To</u>	<u>Gross Cost</u>	<u>Remarks</u>
Additional Budget Dollars for Previously Approved Projects				
P16-003				
* Else Street	Else Street	Linwell Road	\$2,080,000	Additional funds for combined sewer separation (including 60% Regional CSO Management Action Program funding)
P17-009				
* Yale Crescent	Yale Cres	Berryman Avenue	248,000	Additional funds for combined sewer separation in conjunction with sewer and watermain replacements (including 60% Regional CSO MAP funding)
P17-022				
Page Street Sewer Reconnection	Cross Street	#10 Page Street	310,000	Additional funds to install new storm sewer to bypass existing deficient trunk sewer connection
P17-068				
Terry Lane Reconstruction	Shelley Avenue	Graham Avenue	110,000	Install storm drainage system in conjunction with sanitary sewer, watermain and road works
Subtotal:			<u>\$2,748,000</u>	
Budget Dollars for New Projects				
P19-002				
Hampstead Place Road Reconstruction	Carlton Street	Manchester Avenue	\$208,000	Funds for Engineering Services to initiate combined sewer separation in conjunction with future watermain and roadworks
P19-006				
Catch Basin And Storm Service Installations	Various		100,000	Installations of catch basins, catch basin leads and storm laterals to alleviate isolated surface flooding
P19-015				
Design For 2020 Sewer Projects	City Wide		50,000	Engineering services for future storm sewer or storm drainage projects
P19-016				
Storm Sewer Spot Repairs	Various		100,000	Storm sewer spot repairs or rehabilitation as required
P19-020				
Underground Improvements			35,000	Engineering services to install storm sewers in conjunction with watermain replacement and roadworks
Avalon Place Bernhardt Street Embassy Avenue	Lakeport road Avalon Place Lakeport road	South end Embassy Avenue South end		
Subtotal:			<u>\$493,000</u>	
Grand Total:			<u><u>\$3,241,000</u></u>	
Funding Source Breakdown:				
* Regional CSO Management Action Program Funding			\$1,291,200	
Capital out of Revenue (10%)			196,000	
Net Debenture Amount			1,753,800	
			<u><u>\$3,241,000</u></u>	

Regional Cost Sharing Projects

Schedule E

This category includes the City's share of municipal works (such as sewer repairs, watermain replacements, sidewalk replacements and new storm sewer construction) on projects undertaken in cooperation with the Regional Municipality of Niagara. Coordinating the City's efforts in this way helps to minimize cost and disruption to the affected residents and businesses.

For 2019 Niagara Region has advised they will be proceeding with a construction tender for Martindale Road from Bridge Street to Fourth Avenue including a second bridge over Highway 406. The City's share includes improvements to the existing sidewalks, new sidewalk, and the costs to widen a sidewalk on the proposed bridge from 1.5m to 2.4m wide.

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET

SCHEDULE E: REGIONAL PROJECTS - CITY SHARE

<u>Project / Location</u>	<u>From</u>	<u>To</u>	<u>Gross Cost</u>	<u>Remarks</u>
Budget Dollars for New Projects				
RN19-xxa				
Martindale Road Reconstruction	Bridge Street	Fourth Avenue	\$750,000	City share of costs for sidewalk replacement, watermain upgrades and 2.4m wide sidewalk on new bridge over Highway 406 in conjunction with Regional Road
Grand Total:			<u>\$750,000</u>	
 Funding Sources Breakdown:				
Capital out of Revenue (10%)			\$75,000	
Net Debenture Amount			<u>675,000</u>	
			<u>\$750,000</u>	

Water System Improvement Program

Schedule F and Schedule R

The total length of watermain under the City's jurisdiction is approximately 606 km with an estimated replacement value of \$1,317 million (including water service replacements) in 2019 dollars. If the City were to replace all of the watermain sections made of older materials, the cost in 2019 dollars will be approximately \$624 million. Assuming an average service life of 50 years for watermain, the annual cost for watermain replacement should be 2% of the total replacement cost of the whole system or \$26 million, in order to maintain a sustainable watermain distribution system. It is estimated that approximately 4.7% of all of the watermain sections are currently in a critical condition. The cost to replace these sections is approximately \$62 million.

As shown in Schedule F, the 2019 capital budget of \$1.128 million, funded from sources other than the water budget, will add new infrastructure namely;

- Completion of the water meter replacement program, and
- A 140m long watermain section installation on Fourth Avenue to service 1507 Fourth Avenue. The proposed watermain will address groundwater deficiency as identified by the Region. After partial cost recovery of an estimated \$20,500 through Local Improvement Regulations, the net cost to the City is estimated at \$107,500.

The funding for the Water System Improvement Program comes from the Water Budget (Schedule R) - \$5.7 million – which has not yet been approved by Council.

- This budget will be tabled with Council in 2019 with public meeting and deliberations before the end of March.
- Council has directed that each year's water budget should include an allocation of at least \$750,000 for the replacement of old and deteriorated cast iron watermain in areas experiencing coloured water problems
 - \$3,490,000 is proposed to be spent on replacement of old and deteriorated cast iron watermain which are usually the cause of coloured water in the system
 - This will generally help in alleviating coloured water problems in the areas where the watermain are being replaced.

Based on the 2019 funding for this program and in comparison to the minimum annual target reinvestment rate of \$26 million, the City is supporting approximately 22% of the recommended reinvestment level (using City reinvestment rate of 2%).

The table below represents the total length of watermain included within the proposed budget. As shown in this table the available funding supports less than the 2% replacement target.

	Additional funds for previously approved projects (km)	Newly identified replacement watermain (km)	Engineering Services (km)
Water Budget	1.1	2.3	0.37
Capital - Gas Tax	0.00	0.00	0.00
Capital - City share of Region	0.00	0.00	0.00
Total (km)	1.1	2.3	0.37
% of total existing watermain system (606km)	0.18%	0.38%	0.06%

Staff continues to monitor the number and location of existing watermain breaks. A total of 86 breaks had occurred in 2017 which is approximately 23% below the 10-year average of 112 breaks per year. In 2018 a total of 82 breaks occurred from January to the end of August. For the same period in 2017 a total of 44 breaks had occurred.

Figures 1 and 2 present records for the past 15 years and reflect the high variability in the number of annual watermain breaks.

Figure 1

Watermain Breaks

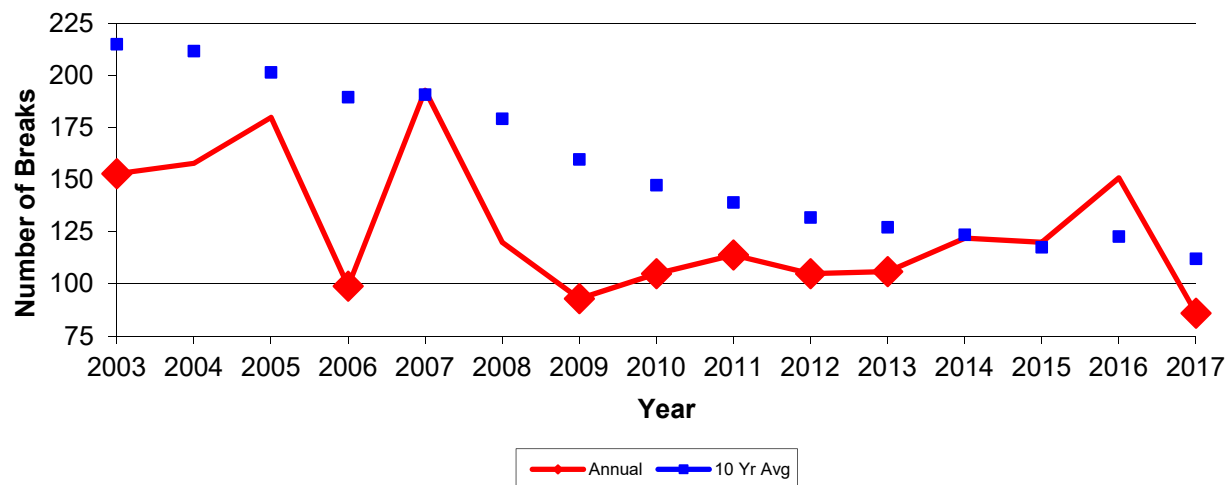
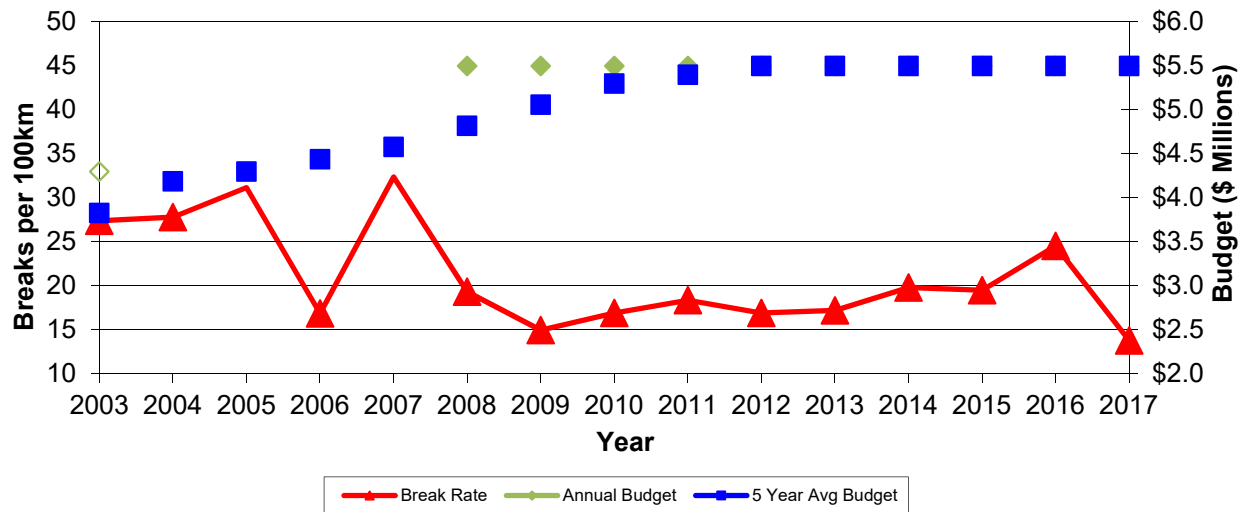


Figure 2

Watermain Breaks per 100km



ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET

SCHEDULE F: WATER SYSTEM IMPROVEMENTS

<u>Project / Location</u>	<u>From</u>	<u>To</u>	<u>Gross Cost</u>	<u>Remarks</u>
Additional Budget Dollars for Previously Approved Projects				
** Water Meter Replacement Program	Year 6 of a multi year program to replace existing water meter to AMR's		\$1,000,000	Additional funds (year 6) of multi-year project.
Subtotal:			<u>\$1,000,000</u>	
Budget Dollars for New Projects				
P19-101				
Fourth Avenue Water Servicing	Old Fourth Avenue	Fourth Avenue	\$128,000	New watermain to address groundwater deficiency identified by the Regional Health department. Partial cost recovery using Local Improvement Regulations.
Subtotal:			<u>\$128,000</u>	
Grand Total:			<u>\$1,128,000</u>	
<u>Funding Source Breakdown:</u>				
** Grants - Federal Gas Tax			\$1,000,000	
Local Improvement Recovery			20,500	
Capital out of Revenue			11,000	
Net Debenture Amount			96,500	
			<u>\$1,128,000</u>	

Legend:

C.I - Cast Iron
D.I - Ductile Iron
A.C. - Asbestos Cement

Niagara District Airport Schedule G

The City supports the capital needs of the Niagara District Airport based on population percentage portion along with the City of Niagara Falls and the Town of Niagara on the Lake. The City of St Catharines has included \$181,800 in the capital budget for its share of the cost of the various works at the airport.

The details of the upgrades include Risk/Compliance and safety items along with sustainability works such as crack sealing and line painting.

The funding sources for these costs are shown in Schedule G.

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET**SCHEDULE G: NIAGARA DISTRICT AIRPORT**

<u>Project / Details</u>	<u>Gross Cost</u>	<u>Remarks</u>
2019 Municipal Working Capital Grant	\$181,800	As per request submitted by the Niagara District Airport
<u>Risk/Compliance/Safety</u>		
Fuel Pad		
Maintenance Shed - FOAM, Jet sand		
Wingbar Lighting		
<u>Sustainability/Repair</u>		
Cracksealing		
Line Painting		
	<u>\$181,800</u>	
<u>Funding Source Breakdown:</u>		
Capital Out of Revenue (10%)	\$18,000	
Debentures	163,800	
	<u>\$181,800</u>	

New Sidewalk Construction & Sidewalk Rehabilitation Programs Schedule H and Schedule U

The City's sidewalks are those which are located within road allowances. Walkways include those sidewalks which run mid block between two streets or are located between houses to connect a park or school to an adjacent street. An emergency access is like a walkway but is wide enough to allow vehicles to drive along it. Walkways and emergency accesses are located on City owned lands. The sidewalk construction program excludes recreational trails or sidewalks within a City facility's site such as within a park, arena or parking lot.

The total length of existing City sidewalks is approximately 559 km. The estimated replacement value of the sidewalks is \$134 million. The total length of the City's walkways and emergency accesses is 26.2 km. The estimated replacement value of the walkways and emergency accesses is \$5.2 million.

Based on an estimated life cycle of 30 years, the annual sustainable reinvestment rate of the sidewalks, walkways and emergency accesses is \$4.6 million. Existing sidewalks are replaced to maintain safe walking routes for pedestrians.

Staff estimates approximately 5% of the existing sidewalks are in poor condition representing a \$7 million deficiency. Locations for sidewalk replacement are determined from a list of sidewalk deficiencies compiled by Operations staff based on field observations and resident complaints. The lists are then analyzed and in cases where more than 50% of the existing sidewalks are deficient, then the entire block is recommended for replacement. Locations not meeting the 50% threshold are repaired as "spot" repairs. Spot repairs are carried out by both City crews and contractors. The spot repairs are included in the operating budget.

The construction of new sidewalks based on requests from residents and businesses and to complete the gaps of existing sidewalk network. Priorities for new sidewalks are based on safety considerations (traffic volume, traffic speed), pedestrian demand (bus routes, generators, destinations) and coordination with other City works.

The 2019 \$700,000 funding for the New Sidewalk Construction & Sidewalk Rehabilitation Program comes from two budgets:

- Capital budget (Schedule H) - \$350,000
 - \$300,000 funds the installation of new sidewalks located within municipal road allowances. \$28,000 is included for the west side of Martindale Road south of Elderwood is recommended in 2019 in conjunction with a Regional Road reconstruction.
 - \$50,000 is supported by the Infrastructure Levy for repair of existing walkways
- Operating Budget (Schedule U) - \$350,000 - funds the repair of existing sidewalks located within municipal road allowances
 - Funding approved by Council in December 2018 as part of the 2019 Operating Budget

Based on the 2019 \$400,000 funding for the renewal component of this program and comparison to the minimum annual target reinvestment rate of \$4.6 million, the City's is supporting approximately 9% of the recommended minimum reinvestment level.

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET

SCHEDULE H: SIDEWALK CONSTRUCTION PROGRAM

<u>Location</u>	<u>From</u>	<u>To</u>	<u>Cost</u>	<u>Remarks</u>
Budget Dollars for New Projects				
P19-040-1				
* 2019 Walkway Repairs	City Wide		\$50,000	Annual Program - priority
	Locations to be determined			walkway location(s)
P19-041				
2019 New Sidewalk Program	City Wide		272,000	Priority location(s)
	Locations to be determined			
RN19-xxa				
Region - Martindale Road	Elderwood Drive	75m north of	28,000	New sidewalk from Elderwood
Reconstruction		Fourth Avenue		Drive to 75m north of Fourth
		Subtotal:	\$350,000	
		Grand Total:	\$350,000	
<u>Funding Source Breakdown:</u>				
			\$30,000	
			50,000	
* Reserves - Infrastructure Levy			270,000	
Net Debenture Amount			\$350,000	

Pollution Control Plan Schedule I

Projects identified by the City's Pollution Control Plan are the City's effort to meet or exceed the requirements of the Ministry of the Environment and Climate Change's Procedure F-5-5 Determination of Treatment Requirements for Municipal and Private Combined and Partially Separated Sewer Systems. The Pollution Control Plan (PCP) identified a number of initiatives to upgrade the existing sewage infrastructure to address combined sewer overflow discharges to the natural environment.

An update of the Pollution Control Plan is underway.

The first phase of construction for the Burleigh Hill / Glendale Combined Sewer Overflow (CSO) is nearing completion. A tender for a second construction phase is anticipated soon for works north of Glendale Avenue.

Due to geotechnical factors confirmed in 2018, additional funding for the Burleigh Hill / Glendale Avenue CSO Control project is included in the 2019 Capital Budget at \$2.0 million. An application will be submitted for 40% funding from the Region's CSO MAP program which is equivalent to \$0.8 million.

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET**SCHEDULE I: POLLUTION CONTROL PLAN**

<u>Project / Location</u>	<u>Description</u>	<u>Gross Cost</u>	<u>Remarks</u>
Additional Budget Dollars for Previously Approved Projects			
P13-19			
Burleigh Hill/Glendale Avenue	Glengarry Park CSO storage facility and inlet sewer	\$2,000,000	Additional funds for installation of CSO storage facility and inlet sewer.
		<u>\$2,000,000</u>	
 <u>Funding Sources Breakdown:</u>			
Capital Out of Revenue (10%)		\$120,000	
Grant - Regional CSO Management Action Program Funding		800,000	
Net Debenture Amount		1,080,000	
		<u>\$2,000,000</u>	

City Building and Accessibility Improvements Schedule J

This schedule identifies the key initiatives and programs that will be carried out to improve City owned facility assets such as structures, buildings and related land improvements. The various programs below highlight costs associated with the asset management of individual building features such as roofing, washrooms or accessibility upgrades or the costs associated with various classes of building such as Heritage Buildings and Arenas.

The estimated total replacement value of City owned structures addressed in the program is \$526.5 million, and the related current infrastructure deficit is estimated at \$46.2 million.

The facility improvements proposed in 2019 total \$11.543 million, of which \$10.918 million is included in the capital budget as shown in Schedule J, \$350,000 is included in Schedule O and \$275,000 is included in the approved 2019 Operating Budget.

The \$11.543 million represents 87% of the upper end of the annual targeted reinvestment rate, as per the Canadian Infrastructure Report Card 2016. However, if the new investments in the Fire Tower Training Centre and the project for consolidation of City Staff are excluded, the City's reinvestment in its existing facility infrastructure is 91% of the lower end and 62% of the upper end of the annual targeted reinvestment rate.

Accessibility Implementation

In 2007 approximately 47 City buildings were audited by an external consultant for their compliance with Facility Accessibility Design Standards. The final report outlining all of the non-compliant items as a result of the audit was received in 2008. An estimated total of \$5.750 million in non-compliant items was identified. Historically, Council has approved an annual budget of \$250,000 for the Accessibility Implementation Program.

The 2019 \$250,000 budget includes accessibility upgrades and the addition of barrier free washrooms at Merritton Park (George Taylor Field). In 2019 there is also a budget of \$2.23 million for accessibility upgrades at the old Robertson School. This is contingent upon being awarded the Enabling Accessibility Program grant funding.

Heritage Building Rehabilitation

The Old Court House, Montebello Park Pavilion and Band Shell, City Hall, the Morningstar Mill, Dalhousie House, and the Old Robertson School at 85 Church Street are examples of City owned buildings which are classified as Heritage Buildings.

Building Condition Assessments have been carried out on the Heritage Buildings by various consultant teams. Due to the age of these buildings many of the architectural, structural, mechanical and electrical components of these structures have well exceeded their useful service life and will require regular maintenance and rehabilitation in order to preserve the integrity of these buildings. There are some components within these structures that do not meet current codes and safety regulations and require retrofits in order to remain in operation. The estimated total value of the identified deficiencies for the City's heritage buildings, based on the original and some updated reports, is approximately \$9.7 million.

The 2019 Heritage Building Rehabilitation Program will include an allocation of \$2.435 million for Accessibility upgrades and addition of an elevator to Robertson School, repairs to the stone wall along Yates Street, Window replacement at Robertson School Annex, Bandshell repairs at Montebello Park, along with select upgrades at the Old Courthouse and City Hall.

Roofing Rehabilitation

The total roof area of all of City buildings is approximately 923,000 square feet. Many of these roofs have either passed or are close to the end of their service lives. Condition assessments and the annual inspections are carried out on many of the roofs. Based on the most recent roof reports it was estimated that the total replacement value of the roofs is \$9.7 million.

The proposed 2019 program includes an allocation of \$565,000 for roof repairs / replacements at the St. Catharines Museum, Merritton Community Centre, Fire Hall No. 5, as well as partial replacements at Buchanan House & Robertson School, as well as a handful of storage facilities.

Washroom Repair Initiative

The City has various washroom facilities in parks and buildings throughout the City for use by both the public and staff. The condition of these facilities has deteriorated significantly over the years, requiring that many of them be either rehabilitated or replaced. Washrooms are also a primary component of a building that requires upgrade in order to meet accessibility needs of a facility.

The 2019 Budget includes funds to upgrade the existing washrooms at Merritton Park (George Taylor Field), and to carry-out the detailed design and site services of new washroom facilities at Sunset Beach.

Arena Improvement Initiative

There is an estimated deficiency of \$18.8 million within the City's arenas (excluding Meridian Centre). This number represents architectural, structural, mechanical and electrical deficiencies identified in the most recent condition assessments that were carried out on these facilities, along with needs that have been identified by staff. Items not included in the estimated deficiency are upgrades for accessibility, security systems and any other initiatives.

The replacement value for the four older arenas including demolition and replacement with new equivalent size facilities is approximately \$42.2 million. Priority repairs include those items which did not meet current code as well as deteriorated items which require replacement.

The 2019 Arena Improvement program will have an allocation of \$823,000 to replace the ice-making plant at Bill Burgoyne Arena, and to perform preventative maintenance on equipment at Seymour Hannah. It also includes security upgrades and elevator repairs at the Meridian Centre.

Fire and Emergency Management Services

Schedule J also includes capital works to be undertaken for Fire Services. The 2019 Fire Services program will have an allocation of \$4 million to construct a new training centre and replace the existing nonfunctional fire training tower.

Parking Services

The 2019 Parking Improvement program has an allocation of \$360,000 to carry out preventative maintenance on the Carlisle Street Parking Garage and to replace the light fixtures with LED in the Ontario Street Parking Garage. There is also an additional \$350,000 included in Schedule O for Race Street parking lot reconstruction.

General Building Improvement

Based on the result of recent condition assessments and requirements of regulatory bodies, repairs are required on some of the City Buildings. The 2019 General Building Improvement Program totaling \$665,000 will address some of these issues, including the structural, architectural, mechanical and electrical components of various City buildings that have reached the end of their service life.

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET

SCHEDULE J: CITY BUILDING AND ACCESSIBILITY IMPROVEMENTS

Project / Location	Details	Gross Cost	Remarks
Additional Budget Dollars for Previously Approved Projects			
P17-180			
Corporate Consolidation - Phase 3 and Carlisle Street Garage Fit-out	ongoing consolidation with City staff resulting in additional moves in other City facilities	\$1,100,000	Fit out of Carlisle St garage as continuation of Staff consolidation efforts
Parks, Recreation and Culture Services			
P13-131			
** Happy Rolph's Bird Sanctuary	House demolition - cut/cap services (additional funds)	20,000	Additional funds to cut and cap services in conjunction with the house demolition
**	Barn - Roof replacement	25,000	To address roofing deficiencies
P17-135			
Yates St/Oakhill Park	Replace/rebuild stone wall	150,000	Replace/rebuild stone wall
P17-161			
** Garden City Arena Complex	Low roof replacement and steel roof replacement - additional funds	30,000	Additional funds to address roof deficiencies
Subtotal:		\$1,325,000	
Budget Dollars for New Projects			
Fire and Emergency Management Services			
P19-152			
Fire Training Tower Replacement	Construction of replacement fire training tower	\$1,800,000	Construction of replacement fire training tower - location tbd
Fire Tower - Training Centre	Construction of new fire training centre	2,200,000	Construction of new fire training centre in conjunction with training tower
P19-158			
** Fire Station #5	Roof replacement	50,000	To address roofing deficiencies
Subtotal:		\$4,050,000	
Parks, Recreation and Culture Services - Arenas			
P19-165			
Arena Improvement Program, 2019 Bill Burgoyne Arena	Iceplant rebuild	\$675,000	Ongoing capital improvements to BBA to ensure longevity of asset
P19-166			
** Bill Burgoyne Arena	Fire alarm panel replacement	20,000	To address building deficiencies
P19-164			
** Seymour Hannah Sports and Entertainment Complex	Replacement actuators for compressors	30,000	To address building deficiency
P19-188/P19-189			
Meridian Centre	Security improvements	48,000	To address additional security upgrades
	Elevator improvements	50,000	To address elevator upgrades
Subtotal:		\$823,000	

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET

SCHEDULE J: CITY BUILDING AND ACCESSIBILITY IMPROVEMENTS

Project / Location	Details	Gross Cost	Remarks
Parks, Recreation and Culture Services			
P19-181			
St Catharines Museum Roof replacement	Replace the steel roof and improve insulating value	\$300,000	To address identified roofing and insulation deficiencies
P19-182			
Welland Canals Centre	Replace concrete at rear of building	100,000	to address building deficiencies
P19-135			
Merritton Community Centre	Roof replacement	60,000	To address roofing deficiency
P19-137-1			
Merritton Park - George Taylor Field Washroom/Changerooms	Renovate and add washrooms	500,000	To address building deficiency and add a washroom
	Accessibility upgrades	250,000	Implement accessibility measures
P19-137-2			
Merritton Park - Grandstand improve	Repair/Paint the structure	150,000	To address structural deficiencies
P19-177			
Parks, various - water filling stations	Install water filling stations within 5 City parks	50,000	Protection to the environment with the ban on plastic bottles
P19-179			
Sunset Beach washroom site servicing	Site servicing for the new washroom	70,000	to address site deficiencies
P19-175			
** Kiwanis Aquatic Centre	Basement - concrete repairs	15,000	To address bulding deficiencies
P19-138			
** Outdoor Pool Improvements	Lion Dunc Schooley small pool - sandblast and paint	25,000	To address pool deficiencies
**	Port Dalhousie Lions large pool - sandblast and paint	55,000	To address pool deficiencies
P19-178			
** Port Weller Community Centre	Replace deficient windows	40,000	to increase energy efficiencies by replacing windows
P19-167			
** Victoria Lawn Cemetery	Service centre - roof replacement	15,000	To address roofing deficiencies
	Subtotal:	\$1,630,000	
Parks Improvement Program			
P19-172			
** Catherine Street Park	Splashpad - upgrade water distribution centre	\$20,000	To address park improvements and better efficiencies
P19-133			
Jaycee Park	Garage - mould remediation/add ventilation	50,000	To address building deficiency
P19-174			
** Lester B Pearson park	Splashpad - upgrade water distribution centre	35,000	To address park improvements and better efficiencies
P19-139			
** Malcolmson Park	Office building - roof replacement	10,000	To address roofing deficiencies
P19-161			
** Merritton Park	Roof replacement	10,000	To address roofing deficiencies
P19-134			
** Montebello Park	Bandshell - replace roof/repairs to soffitt, decking, railings	35,000	To address bulding deficiencies
**	Washroom - roof replacement	20,000	To address roofing deficiencies
	Subtotal:	\$180,000	

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET

SCHEDULE J: CITY BUILDING AND ACCESSIBILITY IMPROVEMENTS

Project / Location	Details	Gross Cost	Remarks
Heritage Buildings			
P19-147/P19-148/P19-149			
Robertson School-85 Church Street	Window replacement in Annex	\$120,000	To address building deficiency
	Accessibility upgrades including elevator	2,230,000	To address accessibility items in conjunction with Provincial grant funding
	Main Building - flat roofs (EPDM) replacement	15,000	To address roofing deficiencies
P19-143			
** Old Courthouse - 101 King Street	Fire alarm panel replacement	20,000	To address building deficiency
P19-140/P19-141			
** City Hall - 50 Church Street	James Street door repairs - preliminary design	35,000	To address building deficiency
**	UPS capacitor upgrade	15,000	To address building deficiency
	Subtotal:	\$2,435,000	
Transportation and Environmental Services			
P19-142			
Lake Sreet Service Centre	Install additional swipecard security at LSSC following completion of building upgrades	\$50,000	Ensure integrity of staff and public safety
P19-144			
** Buchanan House - 361 Niagara St	Roof replacement	35,000	To address roofing deficiencies
P19-183			
** Port Dalhousie Library Branch	Replace/install FADS ramp	30,000	Implement accessibility measures
	Subtotal:	\$115,000	
Parking Garages			
P19-151			
** Carlisle Street Parking Garage	Waterproofing repairs	\$60,000	Ongoing building preventative maintenance
P19-150			
* Ontario Street Parking Garage	Overall lighting replacement with LED lights	300,000	To improve lighting efficiencies
	Subtotal:	\$360,000	
	Grand Total:	\$10,918,000	
Funding Sources Breakdown:			
* Grant - Federal Gas Tax		300,000	
Grant - Provincial - Enabling Accessibility		1,000,000	
Reserve- Meridian Centre Capital		48,000	
** Reserves - Building Improvement Program		650,000	
Capital Project transfer		4,550,000	
Stakeholder Contribution		15,000	
Capital out of Revenue (10%)		673,000	
Net Debenture Amount		3,682,000	
		\$10,918,000	
Accessibility Upgrades Summarized for Information Purposes			
Merritton Park George Taylor Field - Washroom Accessibility		\$250,000	

Watercourse Rehabilitation Program Schedule K

The City of St. Catharines has approximately 36.5 km of natural watercourses that generally drain water from within the City out to Lake Ontario. The deterioration of the protective works for these watercourses and the limited funds available to rehabilitate them, may lead to the erosion of public and private lands and structures.

A Watercourse Flooding and Erosion Control Priority Study Report was provided to the City in February 2015 for creeks within the urban areas of St. Catharines. Staff is using the report's findings and recommendations for prioritizing watercourse restoration works in conjunction with other required infrastructure works / projects. The study also has prioritized rehabilitation needs of culverts with diameters too small to be included in the mandatory biennial Bridge and Major Culvert Inspections.

That 2015 study identified 87 watercourse sites within the urban boundary that are eroded and need rehabilitation. Out of the worst 25 eroded sites, 6 are located on private property and the City does not have any responsibility.

Although it is difficult to provide a figure for the work required, inflation adjusted cost estimates for rehabilitating the remaining 19 eroded sites is approximately \$9.3 million. As each erosion site and the rehabilitation required at each site is different, for estimation purposes the above total cost translates to an average cost of approximately \$491,000 for each erosion site. If this average rehabilitation cost is applied to all the 87 erosion sites identified, the total citywide watercourse rehabilitation costs will be approximately \$42.6 million in 2019 dollars.

Old Welland Canal Lock Walls

In 2018, a detailed inspection program was undertaken to inspect the exposed lock walls constructed for first three (3) Welland Canals. The downstream end of these canals was in Port Dalhousie at the mouth of Twelve Mile Creek.

The inspection program summarized the General Overall Condition of each lock wall as follows: Very Good; Good; Fair; and Poor. Based on the 2018 inspections, none of the structures were determined to be in Very Good overall condition.

In addition to the General Overall Condition, a Priority Rating was used to assign a timeline for any recommended maintenance and capital (rehabilitation / replacement) needs. Based on the 2018 inspections the following table summarizes the maintenance and capital needs:

Priority Rating	Maintenance Cost	Capital Cost	TOTAL
Adequate	\$0	\$0	\$0
6-10 Years	\$15,000	\$58,250	\$73,250
1-5 Years	\$299,000	\$458,000	\$757,500
NOW	\$171,750	\$725,500	\$897,250
TOTAL	\$485,750	\$1,242,250	\$1,728,000

These needs / priority ratings translate to an average annual funding level of over \$170,000 over a 10-year period.

2019 funding of \$120,000 is included to undertake the necessary engineering services to complete the detailed design and contract documents in order to stabilize and prevent further erosion and settlement of several lock walls is. Funding to undertake the construction will be budgeted in future years.

Martindale Pond Weir & Heywood Generating Station

A weir which controls the water level in Martindale Pond was also constructed for the former Welland Canals. During another earlier inspection, deficiencies related to the Martindale Pond Weir were identified. Adjacent to the Martindale Pond Weir is the Heywood Generating Station which is owned by St. Catharines Hydro Generation Inc. In order to comply with Federal and provincial legislation, a facility safety review is underway for both structures by the same engineering consultant. This review will also identify and prioritize any required capital improvements to address deficiencies. Additional funds are not recommended for 2019 as detailed investigations for the safety review continue. However, substantial investments are anticipated to be required in the foreseeable future.

Clifford Creek Park

The 2019 Budget includes \$300,000 to reduce the infiltration rates of Clifford Creek into a former landfill found under Clifford Creek Park. This construction is in response to a November 2016 MOECC request of the City to develop a work plan to address sources of low level polychlorinated biphenyls (PCB's) that were identified to be entering Old Welland Canal. The contaminant levels are below drinking water standards but cumulatively impact the overall Great Lakes watershed. A geotechnical engineering consultant's recommendation is that the City reduce the amount of infiltration at the park and specifically along the stream bed of Clifford Creek.

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET

SCHEDULE K: WATERCOURSE REHABILITATION

<u>Project / Location</u>	<u>Details</u>	<u>Gross Cost</u>	<u>Remarks</u>
Budget Dollars for New Projects			
P19-130			
Clifford Creek Park - Watercourse protection	Clifford Creek	\$300,000	to address identified deficiencies in clay cap of a closed landfill
P19-131			
Old Welland Canal Lockwall Stabilization	Second Welland Canal @ Lock 6 7 and 9	120,000	Undertake engineering studies to address identified deficiencies
		<u>\$420,000</u>	
<u>Funding Source Breakdown:</u>			
Capital Out of Revenue (10%)		\$42,000	
Net Debenture Amount		378,000	
		<u>\$420,000</u>	

Shoreline Protection Program Schedule L

The City has approximately 4,700 metres of shoreline. The City has installed approximately 2,900 metres of shoreline protection structures. The 2019 replacement value of the existing shoreline protection is approximately \$17 million.

Approximately 2,600 metres of shoreline is in good condition requiring no immediate foreseeable remediation / actions.

The City is currently undertaking a City Wide Review and this report will be finalized by the end of 2018 or early 2019. Based on the preliminary findings of this review there are:

- 4 sites (approx. 650m) of HIGH priority at a total estimated costs of \$3.3 Million
- 8 sites (approx. 1,400m) of MEDIUM / LOW priority at a total estimated costs of \$4.1 Million

The HIGH priority sites should be addressed between the 1 to 5-year timeframe, while the MEDIUM / LOW priority sites include ongoing maintenance and repairs between the 1 to 5-year timeframe and repairs within the 6 to 10-year period.

Staff recommends \$400,000 be included in the 2019 Capital Budget for initial engineering studies to initiate detail design at the highest priority location.

ASSET IMPROVEMENTS 2019 CAPITAL BUDGET**SCHEDULE L: SHORELINE PROTECTION**

<u>Project / Location</u>	<u>From</u>	<u>To</u>	<u>Gross Cost</u>	<u>Remarks</u>
P19-132				
Shoreline Protection Program 2019	Christie Street	Colton Avenue	\$400,000	To initiate engineering studies at priority locations identified for enhanced shoreline protection
			<u>\$400,000</u>	
<u>Funding Source Breakdown:</u>				
Capital Out of Revenue (10%)			\$40,000	
Net Debenture Amount			360,000	
			<u>\$400,000</u>	

Bridge and Culvert Rehabilitation & Replacement Programs

Schedule M and T

The 2018 replacement value for bridges and major culverts (greater than 3m span) is approximately \$47.6 million. The estimated priority infrastructure deficit is \$8.6 million, which includes identified needs for the “NOW” and “1 to 5 Year” categories.

Currently \$5.9 million of \$8.6 million infrastructure deficit is under preliminary engineering studies or detailed design. Approximately \$0.73 million of the \$5.9 million has been previously approved.

The total 2019 funding for this asset category is \$4.010 million. This amount is above our annual targeted reinvestment range, as it includes funding for two major rehabilitations and the replacement of three existing culverts which are in poor shape.

Rehabilitation Programs

In 2019 there are three annual programs totally \$145,000 being funded from the Operating Budget, the Bridge Rehabilitation Program, the Large Culvert Rehabilitation Program and the 2019 Bridge and Large Culvert Inspection and Maintenance Program. Each annual program budget received Council approval as part of the 2019 Operating Budget. The programs will fund high priority miscellaneous minor repairs identified by the 2018 inspection.

Provincial regulations require that biennial inspections (once every two years) be carried out on bridge and culverts with spans greater than three metres. These inspections were undertaken during the 2018 calendar year but a summary report has not yet been received at the time of writing this budget.

Consequently, the following table which summarizes the Bridge Management System maintenance, rehabilitation and replacement priority needs and efforts since 2016 is based on data in the 2016 Inspection Report. The total inventory has reduced as several pedestrian bridges were removed and not replaced. One vehicular bridge was replaced by a smaller diameter culvert; thus it is longer considered part of the bridge inventory. One pedestrian structure was previously included but MTO has acknowledged jurisdiction of it.

Priority Rating in 2016	As of 2016 (in 2016\$)			After 2018 construction season (in 2018\$)		
	Total Structures	% of Total	Total Deficiency \$	Total Structures	% of Total	Total Deficiency \$
Adequate	18	23%	\$0	34	39%	\$0
6-10 Year	10	13%	\$3,532,000	13	25%	\$3,675,000
1-5 Year	22	28%	\$6,181,500	14	31%	\$5,628,000
NOW	28	36%	\$3,394,250	7	5%	\$3,007,000
Total	78	100%	\$13,107,750	70	100%	\$12,310,000

The 2016 needs translated to an average annual funding level of over \$1.3 million over a 10-year period. The consequences of not implementing a proactive plan and sustainable levels of funding for the City's bridge assets include:

- Further aging of the City's structural infrastructure;
- An increase in the amount of backlog work to be completed;
- Increased capital expenditures at the time of rehabilitation or replacement;
- Unexpected emergency structure closures or maintenance activity affecting the travelling public, businesses, and emergency services; and
- The possibility of catastrophic failures.

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET

SCHEDULE M: BRIDGE AND CULVERT REHABILITATION PROGRAM

<u>Project / Location</u>	<u>Details</u>	<u>Gross Cost</u>	<u>Remarks</u>
Additional Budget Dollars for Previously Approved Projects			
P15-122 Third Street Louth Culvert Replacement	Approximately 95m south of CNR	\$350,000	Additional funding for box culvert replacement
P17-121 Third Avenue Louth / Glass Avenue Bridge Rehabilitation	Fifteen Mile Creek	110,000	City's share of costs for engineering services to undertake bridge rehabilitation in conjunction with Town of Lincoln
P17-123 Pelham Rd Bridge	CNR	704,667	Partial construction funding for bridge rehabilitation
P17-124 Cindy Drive Culvert Replacement	Walker's Creek	2,700,000	Box culvert replacement
Lake Street Culvert Replacement	Beverly Hills Creek		
Old Coach Road Culvert Replacement	Spring Garden Creek		
Subtotal:		\$3,864,667	
Grand Total:		\$3,864,667	
Funding Source Breakdown:			
Capital Out of Revenue (10%)		\$137,000	
Grants - Federal Gas Tax		2,488,147	
Net Debenture Amount		1,239,520	
		\$3,864,667	

Corporate Infrastructure Schedule N

The City of St Catharines has budgeted \$280,000 for infrastructure upgrades that will benefit the whole Corporation.

Included in the works for 2019 are upgrades to the telephone infrastructure and updates to the corporate digital signage.

The funding sources for these costs are shown in Schedule N.

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET**SCHEDULE N: CORPORATE INFRASTRUCTURE**

<u>Project Details</u>	<u>Gross Cost</u>	<u>Remarks</u>
Telephone Infrastructure	\$200,000	Upgrade of legacy phone system to provide next generation unified communications. Replacement of end of life infrastructure
Digital Signage Update	80,000	Replacement and enhancement of digital signage located throughout the Corporation
	<u>\$280,000</u>	
<u>Funding Source Breakdown:</u>		
Capital Out of Revenue (10%)	\$28,000	
Net Debenture Amount	252,000	
	<u>\$280,000</u>	

Parking Lot Improvement Program Schedule O

This schedule identifies the key initiatives and programs that will be carried out to improve City owned parking lots.

The 2019 Budget has a \$350,000 allocation included for the reconstruction of Race Street parking lot. This lot was utilized as a temporary construction staging area during the construction of the First Ontario Performing Arts Centre which opened in 2015. As such, the parking lot experienced an increase in deterioration, and is in need of replacement.

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET

SCHEDULE O: PARKING SERVICES

<u>Project Details</u>	<u>Gross Cost</u>	<u>Remarks</u>
Race Street Parking Lot rehabilitation	\$350,000	Parking Lot Rehabilitation
	<u>\$350,000</u>	
<u>Funding Source Breakdown:</u>		
Capital Out of Revenue (10%)	\$35,000	
Net Debenture Amount	315,000	
	<u>\$350,000</u>	

Transit Commission Services Schedule P

Included in the 2019 capital budget is \$250,000 for a Para Transit vehicle for the St Catharines Transit Commission.

This vehicle was initially included in the St Catharines Transit Commission's 2019 operating budget request. The Budget Standing Committee, at the November 22, 2018 meeting, requested that the bus purchase be included in the 2019 capital budget and removed from the operating budget.

The debenture will be for a 5 year term and repaid to the City of St Catharines by the St Catharines Transit Commission.

ASSET IMPROVEMENTS FROM THE 2019 CAPITAL BUDGET**SCHEDULE P: TRANSIT COMMISSION CAPITAL EXPENDITURES**

<u>Project Details</u>	<u>Gross Cost</u>	<u>Remarks</u>
Paratransit vehicle	\$250,000	increase to fleet to accommodate expanded Paratransit service
	<u>\$250,000</u>	
<u>Funding Source Breakdown:</u>		
Capital Out of Revenue	\$50,000	
Net Debenture Amount	200,000	
	<u>\$250,000</u>	

Sanitary Sewer Improvement Program Schedule Q

The total length of combined and sanitary sewers in the City is approximately 570 km. The estimated replacement value of these sewers, is in the order of \$900 million, in 2019 dollars.

The proposed 2019 Sanitary Sewer Improvement Program, funded by the Wastewater Budget, amounts to \$2.5 million.

Sewers for replacement and rehabilitation have been selected on a priority basis. These priorities are set principally from the results of CCTV inspection reports. Information from Operations field personnel is also solicited when developing the program. Also when possible, sewer repair, replacement and rehabilitation works are carried out in conjunction with other works, such as road repairs and watermain replacement.

Deficiencies in the sewer system include blockages from collapsed pipe sections, grease and severe root infiltration, which restricts capacity and may lead to backup during periods of high flow. Rehabilitation of these sewers can ensure an adequate level of service and reduce the likelihood of basement flooding in adjacent and upstream areas of the sewer system.

A City-wide Zoom camera inspection of the sewer systems started in August 2017 with complete results anticipated in 2019. Preliminary data for approximately 28% of the system has indicated that approximately 13.3% of the existing sewer system is in a poor condition structurally or operationally, which resulted in an estimated infrastructure deficit of \$119.7 million.

Based on an estimated life cycle of 75 years for sewers, the annual reinvestment rate for the sanitary sewer system should be almost \$12 million. This year's construction program of \$2.5 million falls well short of this amount.

ASSET IMPROVEMENTS FROM THE 2019 WATER / WASTEWATER BUDGET

SCHEDULE Q: SANITARY SEWER IMPROVEMENT PROGRAM

<u>Project Name / Location</u>	<u>From</u>	<u>To</u>	<u>Gross Cost</u>	<u>Remarks</u>
Additional Budget Dollars for Previously Approved Projects				
P15-066 Moffatt Street Reconstruction	Disher Street	100m south of Disher Street	\$211,000	Sanitary Sewer replacement in conjunction with road reconstruction
P17-009 Yale Crescent Underground Improvements	Vine St.	Berryman Ave.	400,000	Additional funds for sewer replacement in conjunction with storm sewer and watermain replacement
P17-067 St Davids Road	Burleigh Hill Drive	Leeson Street	610,000	Install new sanitary sewer and replace existing sanitary sewers in conjunction with road reconstruction. This is a joint project with the City of Thorold.
P17-103 Haig/Ventura Underground Improvements	Garfield Lane	St Hildas Court	150,000	Sanitary sewer improvements in conjunction with watermain replacement
ST18-01 Wastewater Master Plan	City Wide		154,000	Rebudgeting of 2018 funds used for another sanitary sewer project as reported to council Dec 10,2018
Subtotal:			<u>\$1,525,000</u>	
Budget Dollars for New Projects				
P19-001 Clover/Niagara Sewer Improvements			\$355,000	
Niagara Street	McKenzie Street	Heywood Avenue		Abandon sanitary sewer in an easement and modify existing sewer laterals and plumbing to reconnect to other sanitary sewers

ASSET IMPROVEMENTS FROM THE 2019 WATER / WASTEWATER BUDGET

SCHEDULE Q: SANITARY SEWER IMPROVEMENT PROGRAM

<u>Project Name / Location</u>	<u>From</u>	<u>To</u>	<u>Gross Cost</u>	<u>Remarks</u>
Clover Street	Heywood Avenue			Replace and upgrade connection between City's sewer and a regional trunk sewer
P19-002 Hampstead Place Road reconstruction	Carlton Street	Manchester Avenue	245,000	Replace sanitary sewer in conjunction with roadworks, storm sewer installation and watermain replacement
P19-003 2019 Extraneous Flow Elimination	City Wide		50,000	Study to determine sources of inflow and infiltration into the sanitary sewer system
P19-011 2019 Sanitary Sewer Spot Repair Program	City Wide		100,000	Spot repairs for sanitary sewers as required
P19-012 2019 Sanitary Sewer Flushing & Reaming	City Wide		25,000	sewer flushing and reaming for physical cleaning of selected sewer sections with operational issues
P19-014 2019 CCTV Sewer Inspection	City Wide		150,000	CCTV inspection of existing sewers to identify condition and future works
P19-015 Design for 2020 Projects	City Wide		50,000	Engineering services for 2020 construction projects
Subtotal:			<u>\$975,000</u>	
Grand Total:			<u>\$2,500,000</u>	
Funding Source Breakdown:				
2019 Water/Wastewater Budget - Sewer Infrastructure			<u>\$2,500,000</u>	

ASSET IMPROVEMENTS FROM THE 2019 WATER / WASTEWATER BUDGET

SCHEDULE R: WATER SYSTEM IMPROVEMENT PROGRAM

<u>Project Name / Location</u>	<u>From</u>	<u>To</u>	<u>Gross Cost</u>	<u>Remarks</u>
Additional Budget Dollars for Previously Approved Projects				
P17-067 St Davids Road	Burleigh Hill Dr	250m East of Allanburg Rd	905,000	Replace existing D.I. watermain in conjunction with underground improvements and road reconstruction. Joint project with the City of Thorold.
P17-068 Terry Lane Reconstruction	Shelley Ave.	Graham Ave.	40,000	Additional funds to replace C.I. watermain in conjunction with sewer and road works.
P17-103 Haig Street Ventura Drive Underground Improvements	Cayuga Street Haig Street	Scott Street Haig Street	1,690,000	Additional funds for construction to replace existing C.I. watermain on Haig St and D.I. watermain on Ventura Dr
P18-100 Champa Drive Watermain Replacement	Scott Street	Agar Drive	430,000	Additional funds for construction to replace existing C.I. watermain.
P18-101 Rendale Avenue Watermain Replacement	Dorchester Blvd	Cushman Road	725,000	Additional funds for construction to replace existing C.I. watermain.
Subtotal:			<u>\$3,790,000</u>	
Budget Dollars for New Projects				
P19-002 Hampstead Place Road Reconstruction	Carlton Street	Manchester Ave	\$260,000	Replace existing C.I. watermain in conjunction with storm and sanitary sewer works.
P19-020 Avalon Place	Lakeport Road	South End	55,000	Engineering services to replace A.C. and C.I. watermain in conjunction with storm sewer works and road resurfacing
Bernhardt Street Embassy Avenue Underground Improvements	Avalon Place Lakeport Road	Embassy Avenue South End		
P19-100 Bradmon Drive watermain	Parnell Road	Lakeshore Road	670,000	replace existing C.I. watermain

ASSET IMPROVEMENTS FROM THE 2019 WATER / WASTEWATER BUDGET

SCHEDULE R: WATER SYSTEM IMPROVEMENT PROGRAM

<u>Project Name / Location</u>	<u>From</u>	<u>To</u>	<u>Gross Cost</u>	<u>Remarks</u>
P19-102				
Arran/Dixie watermain			700,000	
Arran Road	Sherman Drive	McCordick Drive		replace existing D.I. watermain
Dixie Road	Greenmeadow Court	Wood Street		replace existing C.I. watermain
P19-118				
Valves, Hydrants & Services	City Wide		50,000	Replace existing valves, hydrants and services etc. in conjunction with other works
P19-119				
Design for 2020 Projects	City Wide		50,000	Design for 2020 Projects
RN19-xxa				
Martindale Rd reconstruction	Martindale Road	Sawmill Road	125,000	Construction of a new watermain connection between watermain on Martindale Road and Sawmill Road in conjunction with Regional road reconstruction
Subtotal:			<u>\$1,910,000</u>	
Grand Total:			<u>\$5,700,000</u>	
Funding Source Breakdown:				
2019 Water / Wastewater Budget - Water Improvement Program			<u>\$5,700,000</u>	

Legend:

C.I - Cast Iron
D.I - Ductile Iron
A.C. - Asbestos Cement

ASSET IMPROVEMENTS APPROVED IN THE 2019 OPERATING BUDGET**SCHEDULE S: ROAD AND DRAINAGE IMPROVEMENT PROGRAM**

<u>Location</u>	<u>From</u>	<u>To</u>	<u>Gross Cost</u>	<u>Remarks</u>
P19-080				
2019 Resurfacing Program			\$1,501,000	
Albany Drive	St Columba Drive	Jefferson drive		Milling and road resurfacing
Bowstead Drive	Marsten Drive	Glenholme drive		Milling and road resurfacing
Brewery Street	End	Salina Street		Milling and road resurfacing
Chaffey Street	Jefferson Drive	Albany Drive		Spot curb repairs, milling and roa resurfacing
Dorothy Street	Meadowvale Drive	Dawn Road		Milling and road resurfacing
Dunvegan Road	Carriage Road	Woodrow Street		Spot curb repairs, milling and road resurfacing
Glenholme Drive	Geneva Street	Albany Drive		Milling and road resurfacing
Marsten Drive	Geneva Street	Albany Drive		Milling and road resurfacing
McGuire Street	St Paul Street W	St Paul Cres		Widening road for bike lanes, spot curb repairs, milling and resurfacing
Meredith Drive	Ancaster Boulevard	Rendale Avenue		Curb repairs and drainage improvements, milling and road resurfacing
Salina Street	Yates Street	Brewery Street		Spot curb repairs, milling and road resurfacing
St Columba Drive	Geneva Street	Albany Drive		Milling and road resurfacing
Tunis Street	Pelham Road	Hillview Road		Resurfacing following underground works in 2018
Yates Street	St Paul Street	Salina Street		Spot curb repairs, milling and road resurfacing
various				
P19-081				
King Street Hydro Relocation	James Street	Carlisle Street	100,000	City's share of costs to relocate hydro underground in conjunction with sidewalk replacement
P19-082				
2019 Concrete Base Repair Program	Various, City Wide		340,000	Annual Program
P19-083				
2019 Crack Sealing	Various, City Wide		100,000	Annual Program

ASSET IMPROVEMENTS APPROVED IN THE 2019 OPERATING BUDGET

SCHEDULE S: ROAD AND DRAINAGE IMPROVEMENT PROGRAM

<u>Location</u>	<u>From</u>	<u>To</u>	<u>Gross Cost</u>	<u>Remarks</u>
P19-085 2019 Guardrail Program	Various, City Wide		100,000	Annual Program to repair existing guardrail and / or install new guardrail
P19-086 Design for 2020 Projects	City Wide		25,000	Engineering services for future road construction projects
P19-092 2019 Boulevard Reinstatements	City Wide		10,000	Annual Program
ST19-05 Road Database and Software	Software updates		10,000	Annual update and software license
ST19-05-1 Road Database and Software - Curbs	City Wide		75,000	Add existing curb condition based on 2018 field inspection into road asset database to enhance decision making
ST19-05-2 Road Database and Software - Pavement Markings	City Wide		40,000	Add existing pavement markings based on 2018 field inspection into road asset inventory
ST19-06 Tremont Retaining Wall			30,000	Condition assessment and preliminary design for asset meeting life expectancy
			<u>\$2,331,000</u>	
<u>Funding Source Breakdown:</u>				
2019 Operating Budget - Road & Drainage Improvement Programs			<u>\$2,331,000</u>	

ASSET IMPROVEMENTS APPROVED IN THE 2019 OPERATING BUDGET

SCHEDULE T: BRIDGE AND CULVERT REHABILITATION PROGRAM

Project	Details	Gross Cost	Remarks
P19-120 2019 Bridge Rehabilitation Program	City Wide	\$90,000	Annual program - asphalt surface, approach, concrete and structural repairs, vegetation and debris removal
P19-121 2019 Large Culvert Rehabilitation Program	City Wide	30,000	Annual Program - structural and gabion repairs, slope stabilization, erosion and debris removal
P19-122 2019 Bridge and Large Culvert Inspection and Maintenance	City Wide	25,000	Annual Program - inspection and minor maintenance of bridges and >3m span culverts
		\$145,000	
<u>Funding Source Breakdown:</u>			
2019 Operating Budget - Bridge and Culvert Rehabilitation Program		\$145,000	

ASSET IMPROVEMENTS APPROVED IN THE 2019 OPERATING BUDGET

SCHEDULE U: SIDEWALK CONSTRUCTION PROGRAM

<u>Project Description</u>	<u>Location</u>	<u>From</u>	<u>To</u>	<u>Gross Cost</u>	<u>Remarks</u>
P19-040					
2019 Concrete Sidewalk Repair Program	City Wide			\$275,000	Annual Program, priority location(s) to be determined
P19-081					
King Street Hydro Relocation	King Street	James Street	Carlisle Street	75,000	Replace deficient sidewalk in conjunction with relocation of hydro from aerial to underground
				<u>\$350,000</u>	
<u>Funding Source Breakdown:</u>					
2019 Operating Budget - Sidewalk Construction Program				<u>\$350,000</u>	

SCHEDULE V

CITY OF ST. CATHARINES
2020 CAPITAL FORECAST

DESCRIPTION	ESTIMATED GROSS COSTS	GRANTS, RESERVES ETC.	RECOVERIES FROM OTHER FUNDS	NET DEBENTURE AMOUNT
<u>PARKS, RECREATION AND CULTURE SERVICES:</u>				
Playground Equipment - location tbd	125,000	125,000		
Basketball Court upgrades - location tbd	225,000	225,000		
Canada Summer Games	320,000		32,000	288,000
Merrittion Park - replace 2 high mast light poles	40,000		4,000	36,000
Montebello Park - rework service walkway	75,000		8,000	67,000
Tree Planting program	375,000	375,000	0	0
	1,160,000	725,000	44,000	391,000
<u>ROAD CONSTRUCTION AND IMPROVEMENTS:</u>				
2020 Priority Program	5,296,420	250,000	505,000	4,541,420
<u>STORM SEWER AND SEWER SEPARATION:</u>				
2020 Priority Program	2,625,162	550,000	208,000	1,867,162
<u>REGIONAL PROJECTS - CITY SHARE</u>				
St Paul Street Bridge at CNR	470,000		47,000	423,000
St David's Road - additional	29,785		3,000	26,785
	499,785	0	50,000	449,785
<u>NEW SIDEWALK CONSTRUCTION</u>				
St David's Road sidewalk	18,000		2,000	16,000
Ontario Street sidewalk	131,000		13,000	118,000
2020 New Sidewalk program	151,000		15,000	136,000
2019 Walkway Improvements	50,000	50,000	0	0
	350,000	50,000	30,000	270,000
<u>DEVELOPMENT CHARGES</u>				
no 2020 program	0	0	0	0
<u>SHORELINE PROTECTION:</u>				
2020 priority program	300,000		30,000	270,000
Christie St West	200,000		20,000	180,000
	500,000	0	50,000	450,000
<u>POLLUTION CONTROL AND BASEMENT FLOODING</u>				
2020 Program	1,000,000	400,000	60,000	540,000
	1,000,000	400,000	60,000	540,000
<u>FEDERAL GAS TAX FUNDED PROJECTS</u>				
Active Transportation Infrastructure	250,000	250,000		
Sanitary sewers - 2020 program	1,000,000	1,000,000		
Sanitary sewers - Region - St David's Rd	225,215	225,215		
Sidewalk - Region - St David's Rd	140,000	140,000		
Storm Sewers - 2020 program	1,000,000	1,000,000		
Storm Sewer - Region - St David's Rd	70,000	70,000		
Watermains - 2020 program	1,352,932	1,352,932		
	4,038,147	4,038,147	0	0
<u>BUILDING IMPROVEMENTS</u>				
Arena Improvement Program, 2020	2,105,000		211,000	1,894,000
Buchanan House - Security Implementation	100,000		10,000	90,000
Buchanan House-demolish greenhouse/repair adjoining wall	100,000		10,000	90,000
Building Improvement Program, 2020	895,000	650,000	25,000	220,000
City Hall - James St Atrium Repairs	250,000		25,000	225,000
Meridian Centre - convert ice surface lighting to LED	300,000		30,000	270,000
Meridian Centre - Life safety Systems upgrade	50,000		5,000	45,000
Port Dalhousie Senior Centre - air conditioning/heating	100,000		10,000	90,000
PRCS Administration Building-Demolition and Site Remediation	800,000		80,000	720,000
Sunset Beach - new washroom facility	600,000		60,000	540,000
	5,300,000	650,000	466,000	4,184,000
<u>ACCESSIBILITY PROGRAMMES</u>				
City Hall - PDOs - Third floor-various locations	15,000		2,000	13,000
Bill Burgoyne Arena - implement accessibility	120,000		12,000	108,000
Sunset Beach - new washroom facility	85,000		9,000	76,000
	220,000	0	23,000	197,000
<u>WATERCOURSES</u>				
2020 Watercourse Program	1,000,000		100,000	900,000
	1,000,000		100,000	900,000
<u>BRIDGES AND CULVERTS</u>				
SSR/Dunkirk/Dieppe at Cuchman Rd	800,000		80,000	720,000
Pelham Road	841,000		84,000	757,000
	1,641,000		164,000	1,477,000
<u>FIRE SERVICES</u>				
no 2020 fire projects			0	0
	0		0	0
<u>PARKING SERVICES</u>				
Market Square/City Hall Parking Lot	500,000		50,000	450,000
Carlisle Garage Membrane repairs	100,000		10,000	90,000
	600,000	0	60,000	540,000
<u>CORPORATE INFRASTRUCTURE</u>				
Network/Server Infrastructure	600,000		60,000	540,000
Wireless update	100,000		10,000	90,000
Website	350,000		35,000	315,000
	1,050,000	0	105,000	945,000
TOTAL 2020 CAPITAL FORECAST	\$25,280,514	\$6,663,147	\$1,865,000	\$16,752,367

SCHEDULE W

CITY OF ST. CATHARINES
2019 CAPITAL BUDGET AND FOUR YEAR FORECAST
SUMMARY OF CAPITAL EXPENDITURES AND SOURCES OF FINANCING

	2019	2020	2021	2022	2023
CAPITAL EXPENDITURES:					
Parks, Recreation and Culture Services	\$1,429,000	\$1,160,000	\$1,570,000	\$620,000	\$2,250,000
Road Construction and Improvements	5,946,420	5,296,420	5,437,808	5,645,320	5,861,133
Storm Sewer and Separation	3,241,000	2,625,162	2,730,168	2,730,168	2,730,168
Regional Projects	750,000	499,785	3,374,301	230,000	571,000
Niagara District Airport	181,800				
Sidewalk/Walkway Construction	350,000	350,000	350,000	350,000	362,000
Shoreline Protection	400,000	500,000	610,000	500,000	500,000
Local Improvements - watermain	128,000				
Pollution Control	2,000,000	1,000,000	500,000	800,000	1,000,000
Federal Gas Tax funded programs	4,038,147	4,038,147	4,221,699	4,221,699	4,405,251
Building Improvements	5,948,000	5,300,000	4,395,000	3,430,000	4,120,000
Accessibility Programme	250,000	220,000	500,000	260,000	400,000
Development Charges			2,710,000	760,000	480,000
Fire Services	4,000,000	0	0	3,400,000	3,200,000
Watercourses	420,000	1,000,000	500,000	500,000	1,000,000
Bridges and Culvert	1,376,520	1,641,000	975,000	0	255,000
Parking Improvements	350,000	600,000	600,000	100,000	700,000
Corporate Infrastructure	280,000	1,050,000	700,000	500,000	500,000
Transit Commission Services	250,000				
	<u>\$31,338,887</u>	<u>\$25,280,514</u>	<u>\$29,173,976</u>	<u>\$24,047,188</u>	<u>\$28,334,552</u>
SOURCES OF FINANCING:					
Grants, Reserve Funds etc.	\$13,401,847	\$6,663,147	\$8,369,699	\$7,223,699	\$7,447,091
Capital Out of Revenue Fund	2,057,000	1,865,000	2,081,000	1,683,000	2,089,000
Net Debenture Amount	15,880,040	16,752,367	18,723,277	15,140,489	18,798,461
	<u>\$31,338,887</u>	<u>\$25,280,514</u>	<u>\$29,173,976</u>	<u>\$24,047,188</u>	<u>\$28,334,552</u>

SCHEDULE X

CITY OF ST CATHARINES POTENTIAL CAPITAL PROJECTS

The list below includes the various capital projects that will require capital replacement or upgrades in years six through ten. This is not an exhaustive list. These projects have been listed without dollar amounts as the full scope of the capital needs is not yet completely assessed by Staff. Additionally, an estimate is less reliable the further out the year. Not included on the list are the regular on-going annual replacements to roads, sidewalks, sanitary sewers, watermains and park amenities.

CAPITAL PROJECTS 2024 and beyond

Arena Upgrades - Ice Plants, Bleachers, Other

City Facilities - parking lot upgrades

City's Operational Fleet - vehicle yard

Civic Square

Destination Pool

Firehall #1 Rehabilitation

Fire Master Plan Recommendations

Moffat Street - secondary access

Old Courthouse - meeting FAD standards

Park Amenities - Infrastructure Deficit

Pollution Control - various tanks

Port Dalhousie Pier - upgrades beyond DFO works

Port Weller Community Centre - meeting FAD standards

Road and Drainage Improvements - infrastructure deficit

Robertson School - meeting FAD standards

Transportation Master Plan - action items

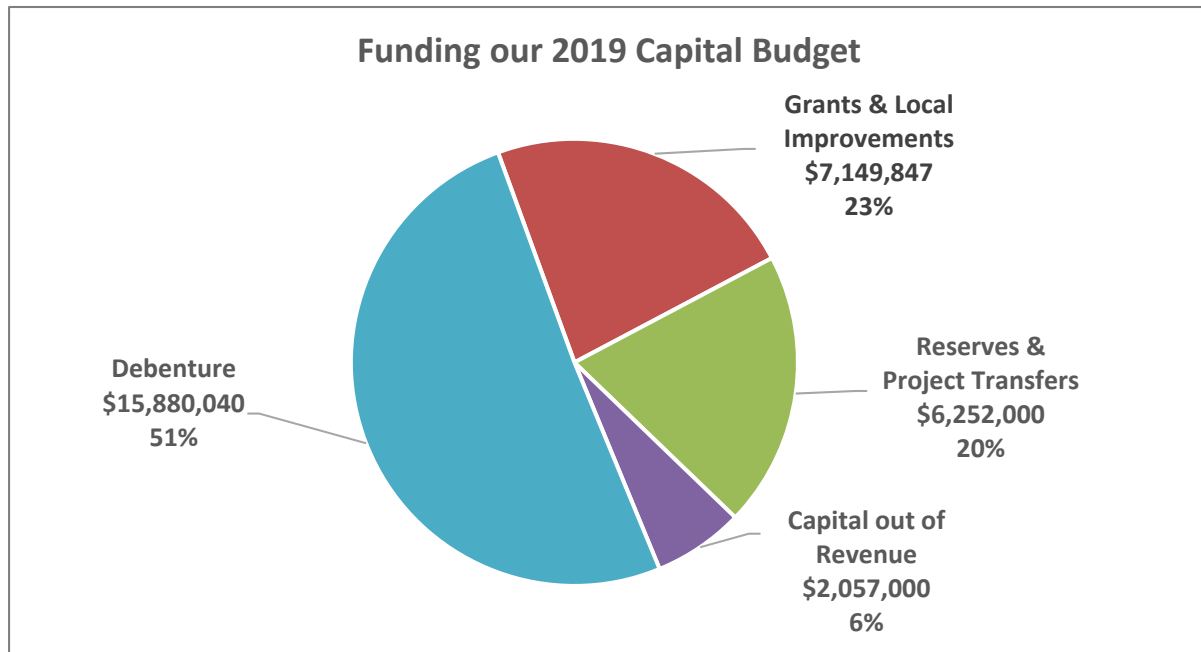
Watercourses - currently unfunded deficiencies

Watermain improvements - infrastructure deficit

Schedule Y

Funding our Capital Budget

There are four sources that fund the 2019 capital Budget for the City of St. Catharines:



Grants and Local Improvements - \$7,149,847

The City of St. Catharines receives various grants for specific work included in several projects throughout the City. The Federal Gas Tax Program (FGTP) is the largest of the grants received by the City for Capital projects.). The City's share of Gas Tax funding for 2019 is \$4,038,147 and is being put towards the Active Transportation Program among various other City programs that are discussed in further detail later in this schedule.

In addition to the FGTP, other external funding opportunities have been identified as funding sources for 2019 works. The Niagara Region offers funding through the Combined Sewer Overflow Control Policy Funding Program (CSO Map Fund) and the Province provides funding opportunities through the Enabling Accessibility Program. This program provides funding for accessibility projects. An accessibility project at Robertson School has been included within the 2019 capital budget and Staff are awaiting approval of the grant. Should the Province not approve the application, the project will be removed from the 2019 capital budget. Also included in the 2019 Capital Budget is a local improvement amount for a project on Old Fourth Avenue. The homeowners abutting this project are responsible for a portion of the cost of the new watermain. The CSO funding, the Provincial accessibility grant and the local improvement charge provide for an additional \$3,111,700 in funding.

Reserves and Project Transfers- \$6,252,000

The City of St. Catharines also uses reserves to fund capital works. The primary reserve is the Infrastructure Levy, first approved in 2016, which will contribute \$989,000 to projects being undertaken in 2019. The Building Reserve Fund makes up an additional \$650,000. In 2019, funding is also provided from previously approved capital budget works whose scope has since changed. Funds have been transferred from existing approved projects to fund the Fire Training Tower, Fire Training Centre and the Water Filling Stations in various City Parks. Additionally, funds from a previous Lake Street Service Centre Project and Fire Station 4 project are being repurposed for Carlisle Street Garage Fit-out. More details are found in Schedule AA.

Capital out of Revenue - \$2,057,000

Capital out of revenue is the portion of the project cost that is funded from the operating budget after other sources of funding are applied. As required by Council approved strategy, it represents 10% of the project, or one years' worth of debenture costs of the project.

Debentures - \$15,880,040

Debentures are the fourth and most significant funding source of the annual Capital Budget. Borrowing is a valid form of infrastructure financing and sets out broad parameters on how cities should borrow; there are many advantages with long term debt financing such as a smoother impact on the operating budget, allows for more project work to be conducted simultaneously and payments are spread out over a longer term (for those assets with longer life expectancy).

Schedule Z

Grant Funding Details

Federal Gas Tax Program (FGTP) - \$4,038,147

Council endorsed a five-year capital investment plan for FGTP revenues in 2009 which identified continued funding for combined sewer system upgrades (storm sewer separations) and watermain replacement in conjunction with the combined sewer system upgrade and various road resurfacing projects. Council subsequently shifted focus away from underground infrastructure to include facility energy related initiatives and non-resurfacing roadwork.

One of the requirements of the FGTP is that the expenditures are incremental, meaning that the funds cannot be used to displace current capital investment or reduce municipal taxes.

Below is a list of the 2019 capital projects funded by FGTP.

Schedule C – Road Construction

Active Transportation Infrastructure

- City Wide \$250,000

Schedule F – Water Improvements

- Water Meter Replacement Program 1,000,000

Schedule J – Building Improvements

- Ontario St Parking Garage Lighting Upgrades 300,000

Schedule M – Bridge and Culvert Rehabilitation

- Culvert Replacement – Cindy Dr/Lake St/Old Coach 2,488,147
\$4,038,147

Regional Cost Sharing Projects

This category includes the City's share of municipal works (such as sewer repairs, watermain replacements, sidewalk replacements and new storm sewer construction) on projects undertaken in cooperation with the Regional Municipality of Niagara. Coordinating the City's efforts in this way helps to minimize cost and disruption to the affected residents and businesses.

For 2019 Niagara Region has advised they will be proceeding with a construction tender for Martindale Road from Bridge Street to Fourth Avenue. The City's budget for its share of costs include improvements to the existing sidewalks and new sidewalks.

Niagara Region: Combined Sewer Overflow Control Policy Funding Program (CSO Map Fund) - \$2,091,200

The City has applied for \$2,091,200 of funding from Niagara Region under their Combined Sewer Overflow (CSO) Control Policy Funding Program. The applications include pollution control plan projects funded by the Capital Budget and two Storm Sewer projects on Else Avenue and Yale Crescent.

Project applications are ranked based on reduction, elimination and control of wet weather flows. Under the Region's policy, the Region's share of funding can range from 30% to 80% depending on the effectiveness of the project to meet the priorities set out in the policy.

To date, staff have not been informed if the City's applications have been successful.

Provincial: Enabling Accessibility Fund- \$1,000,000

As reported to Council on May 28, 2018 applications have been submitted to the Province for the Enabling Accessibility Fund program for accessibility projects at Robertson School and at the Old Courthouse. The application for the Courthouse was denied, but Staff is awaiting a decision on the Robertson School application. This project is included in the 2019 Capital Budget and the Grants section of the budget reflects the potential \$1million for the Robertson School Grant. If not successful, this project will not go forward and will be removed from the 2019 Capital Budget.

Local Improvements - \$20,500

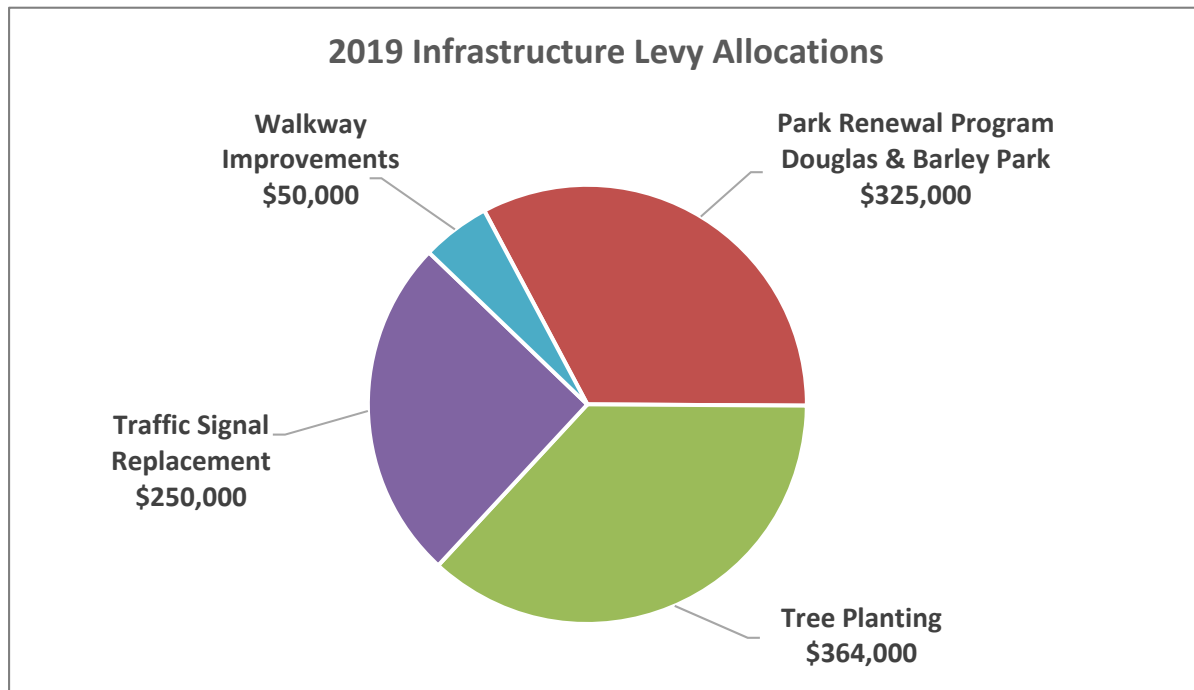
An expansion of the watermain on Fourth Avenue to address groundwater deficiencies is included in the 2019 draft Capital Budget. Local Improvement regulations apply for this project and allow for recovery of \$20,500 from abutting ratepayers.

Schedule AA

Funding from Reserves/Project Transfers - \$6,252,000

The primary reserve contributing to capital budget projects is funded from the one percent, infrastructure levy levied annually to help address the infrastructure deficit.

In 2019 the Infrastructure Levy will contribute \$989,000 to the various projects outlined below:



The uncommitted balance in this reserve will remain in the reserve for use in the future and is available for use as matching dollars for Federal or Provincial Infrastructure funding programs. Currently there are no unallocated funds in this reserve.

The Building Reserve Fund makes up \$650,000 of reserves used in the 2019 Capital Budget. Details of these projects are shown on Tab 2 - Schedule J. Also reflected in Tab 2 – Schedule J is the use of \$48,000 from the Meridian Centre Capital Reserve to fund security improvements at the Meridian Centre.

Fire Training Tower and Training Centre

The Fire Radio project was approved in 2016 and in 2018 in the amount of \$7million. This project was to upgrade the Fire Dispatch Service for the City's CAD partners throughout the Region. Following approval of the project, various partners decided to forego upgrading their radios, or to upgrade on their own. This decision change meant that more dollars were approved for this project than initially determined. In order to accommodate the Fire Training Tower and the Fire Training Centre in the 2019 Capital Budget, Staff have reallocated the funds from the Radio Project. This results in no additional debt to the City.

Water Filling Stations

Council, at the July 9, 2018 Council meeting, approved the following motion:

Further, that Council direct staff to install five water filling stations in 2019 at City owned facilities and to fund the associated costs from the 2019 Operating Budget.

Staff identified that there are unspent funds available in another capital project that are sufficient to accommodate this expenditure. Therefore, these five water filling stations will be part of the City's 2019 Capital Budget. The amount of \$50,000 is included within Schedule J of the capital budget.

Carlisle Street Garage Fit-out

As a continuation of the consolidation project which merged TES and PRCS operations, the next phase is to build out the Carlisle Street Garage to accommodate City staff. Included in previous budgets were project to address Lake Street Service Centre improvements and to build a new Fire Station. With these projects now complete, there are funds still available within the project budgets that can be repurposed to fund the Carlisle Street Garage Fit-out without increasing debt to the Corporation. The amount of \$900,000 is included within Schedule J of the capital budget.

Schedule AB

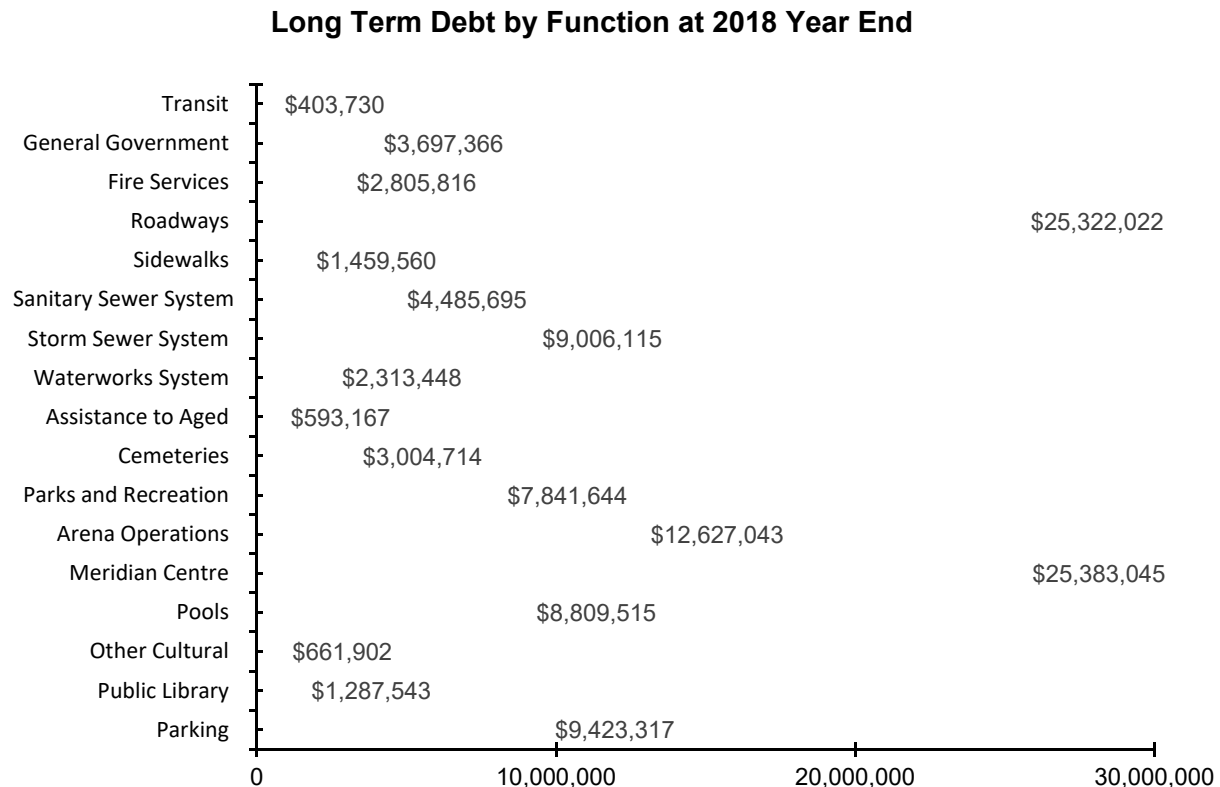
Capital Budget and Debt

A capital strategy has two priorities; the first to maintain existing infrastructure, and the second is to expand or replace existing assets aimed at improving services and programs for residents.

A pay-as-you-go method of financing infrastructure investments avoids interest payments and debt issuance costs, however it directly and significantly impacts the Operating Budget year-to-year. The ability to begin more than one large project is also very limited which could result in the underfunding of infrastructure. Debt financing allows for a more manageable impact on the Operating Budget as it allows for the payments to be spread over a longer term which is appropriate for assets with a longer life expectancy. For these reasons, the City uses debt financing to maintain its assets and manage its operating budget requirements.

Debt management is the process of planning for new debt at a level that optimizes borrowing costs without damaging the financial position of the municipality. For a municipality this means projecting the capital budget's impact on the operating budget and developing a balance between the two to support one another.

The following graph shows the total debt breakdown among the various functional areas for infrastructure and facilities



Schedule AC

Debt Management Strategy

During the 2018 Operating budget process, the BSC reviewed the City's established Debt Management Strategy. The BSC further directed staff to review funding alternatives for capital and to report back to the BSC during the 2019 budget process. Throughout 2018, staff participated in a debt risk management strategy working group with other Niagara Area CAOs and Treasurers. The group is working on a capital financing policy. This policy will provide an over arching policy which provides consistent messaging around debt and reserves. It also ties into asset management plans, and helps all municipalities capitalize on dollars flowing from Provincial and federal government programs. Details will be brought to Council once the policy is established. The currently established Debt Management Strategy, with Council's ongoing support, has the following criteria:

- 1) Eligibility: Not all capital projects are suited for debt financing. The City's strategy is to recommend debt financing only for infrastructure purposes. Large projects that provide assets with well-defined benefits to the community, have long asset lives and the ability to leverage additional financing elsewhere are eligible for debt financing.
- 2) Sustainable level of borrowing: For debt management purposes, the City has adopted a policy aimed at containing debt charges at approximately 10% of the City's total own expenditures. This constraint ensures that the debt service costs will not become an increasing burden and a larger portion of the City's budget, as it is essential the City ensure debt levels are sustainable within the Operating Budget.
- 3) Debt repayment: The City has a 10% down payment strategy which reduces the amount to be borrowed and at the same time, bringing the approximate costs of one year's debt service to the year of decision. As a result Council will raise, through property taxes, an amount which will be equivalent of future annual debt charges for each particular project. Debentures are taken for shorter timeframes versus longer timeframe which are typically five or ten years.

The debt issuance proposed in the 2019 Capital Budget complies with the current Debt Management Strategy.

Schedule AD

Debt and the Municipal Act

The Municipal Act requires that a Municipality have a five year capital financing forecast. Ontario Regulation 403/02 limits annual debt charges to no more than 25% of the City's net revenue fund revenues. Using this guideline, the City could increase its debt (approximately) by a further \$135.20 million based on an interest rate of 7%, which is the rate the Ministry of Municipal Affairs uses for illustrative purposes. (See calculation below)

Using the City's current estimated borrowing rate of 5%, the City's additional debt capacity would be \$150.2 million. Although there has never been a suggestion that this limit be approached, Council does have significant capacity available should circumstances warrant additional debt financing. The City Treasurer has updated the Debt Repayment Limit for the City, and the debenture amount proposed in the 2019 Capital Budget will not cause the City to exceed its limit.

In 2018, the City's debt charges were 8.95% of the City's total estimated expenditures. Each year, the Ministry of Municipal Affairs produces a financial indicator review that states debt costs between 5% and 10% are considered moderate risk – which is where the City of St Catharines currently resides. The chart below has the estimated impact of increased borrowing activities for the next five years.

	2018	2019	2020	2021	2022	2023
Outstanding Debt	\$119.1m	\$122.6m	\$124.4m	\$123.2m	\$122.9m	\$126.0m
Debt Per Household	\$2,030	\$2,089	\$2,121	\$2,100	\$2,095	\$2,149
Total Annual Debt Charges	\$15.8m	\$16.7m	\$17.7m	\$19.0m	\$20.6m	\$20.6m
As % of Operating Budget	8.95%	9.21%	9.52%	9.93%	10.50%	10.24%

The chart above shows the total debt charges as a percentage of operating budget slightly exceeding the 10% upper limit in 2022. There are many assumptions to determine this percentage, including the amount of debt to be issued, timing of the debt issuances and total amount of expenditures. This projection assumes all future debt being issued for a period of 10 years is at an interest rate of 4%.

The assumptions made are conservative. Any changes made to any of the assumptions – whether that is timing, term or interest rate for debt issuances, or the level of capital spending supported by debt, will have an impact on the debt charges as a percentage of expenditures, projected debt balance and future tax levy impacts.

As the City continues to refine its Asset Management Plan and formulate its future debt needs these forecasts will be updated so that debt servicing remains within the 10% upper limit.

The information on the following page is the 2019 updated debt and annual repayment limit for the City of St. Catharines.

2019 UPDATED DEBT AND ANNUAL REPAYMENT LIMIT

2019 Debt and Financial Obligation Limit (based on 2017 FIR schedule 81):

Calculation of Debt Charges:

Total Principal	10,870,508
Total Interest	3,513,586
	\$14,384,094
Less:	
Tile Drainage and Shoreline Assistance	
NET DEBT CHARGES	\$14,384,094

Calculation of Annual Repayment Limit:

Total Revenue Fund Revenue (sch10 9910)	\$211,760,164
Tile Drainage and Shoreline Assistance (SLC 12 1850 04)	0
Ontario Grants (SLC 10 0699 01+SLC 10 0810 01+ SLC10 0815 01)	(2,455,989)
Canada Grants (SLC 10 0820 01+SLC 10 0825 01)	(2,794,828)
Deferred Revenue Earned (Provincial Gas Tax) (SLC 10 830 01)	(1,444,647)
Deferred Revenue Earned (Canada Gas Tax) (SLC 10 831 01)	(6,802,272)
Other Municipalities including revenue for TCA (SLC 10 1098 01+SLC 101099 01)	(6,165,723)
Gain/Loss on sale of land & capital assets (SLC 10 1811 01)	(27,841)
Deferred revenue earned (Development Charges) (SLC 10 1812 01)	0
Deferred revenue earned (Recreation Land(the planning act) (SLC 10 1813 01)	(114,289)
Donated Tangible Capital Assets (SLC 53 0610 01)	(7,134,404)
Other Deferred revenue earned (SLC 10 1814 01)	(69,466)
Increase/Decrease in Gov't Business Enterprise equity (SLC 10 1905 01)	(17,776,335)
NET REVENUE FUND REVENUES	\$166,974,370
25% OF NET REVENUE FUND REVENUE	\$41,743,593

Adjustments to Annual Repayment Limit:

ANNUAL REPAYMENT LIMIT UNDER REGULATION	\$27,359,499
2018 Unfinanced Capital Projects	\$41,080,115
Capitalized Annual Repayment	
Years	10
Rate	7.00%
Factor	0.14237750
Annual Repayment Limit under Regulation	(5,848,884)
	27,359,499
ADJUSTED ANNUAL REPAYMENT LIMIT	21,510,614

Remaining Debt Capacity

Capitalized Debt Capacity	151,081,554
2019 Capital Budget	(15,880,040)
REMAINING DEBT CAPACITY	\$135,201,514

2018 Undebentured Capital Projects / Unissued Debt

Lakeside Skating Path	\$225,000
Arenas Improvement Program	2,450,856
Pearson Park	24,000
Happy Rolph's Bird Sanctuary	45,000
Lock III Complex	315,000
Senior and Community Centers	567,000
RCS Buildings - various	957,000
Lancaster Park - Diamond 1 washroom accessibility	45,000
Lakeside Park Improvements	810,000
Cemetery Buildings - Improvements	1,277,000
Heritage Buildings Restoration	3,392,500
TES/PRCS Operations Consolidation	1,368,000
Road Construction and Improvements	9,356,222
Storm Sewer and Separation	6,759,111
Watermains	1,358,944
Regional Projects - City's Share	131,200
Development Charges	46,028
Local Improvements	114,000
Fire	4,203,000
Pollution Control Plan	3,755,400
New Sidewalks	1,210,000
Watercourse Rehabilitation	454,000
Corporate Infrastructure	2,286,000
Parking Operations	1,750,000
Undebentured Capital Projects Total:	<u>\$41,080,115</u>

Schedule AE

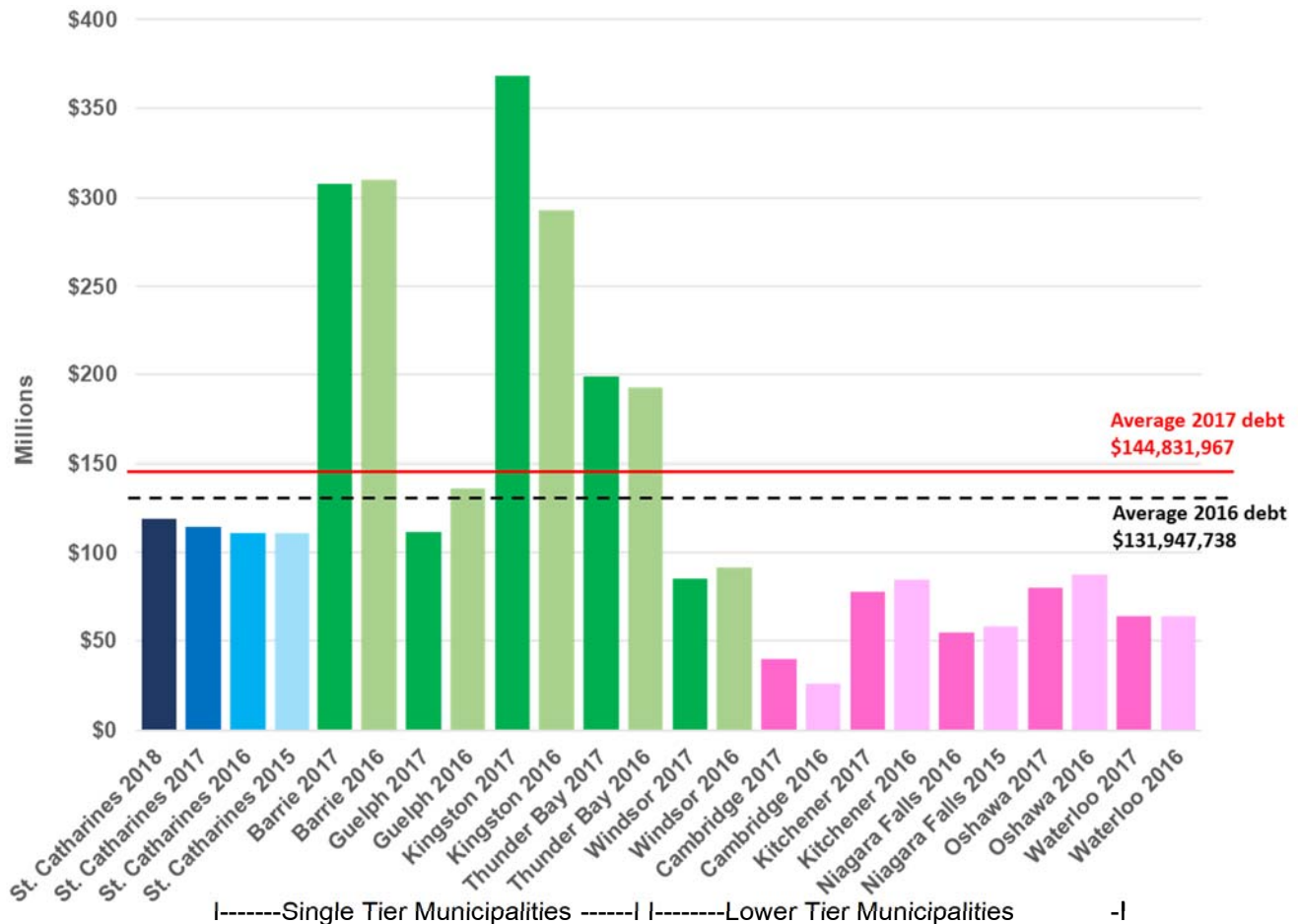
Capital Budget and Debt – Municipal Comparators

Unlike Federal and Provincial budgets, municipal budgets are very capital intensive. Borrowing for capital projects provides many economic benefits for the future of the municipality, especially so, as choosing to be debt-free would result in substantially less infrastructure being renewed and available to the public.

City of St. Catharines Debt Figures	
2015	\$110,893,734
2016	\$111,292,927
2017	\$114,600,837
2018	\$119,125,644

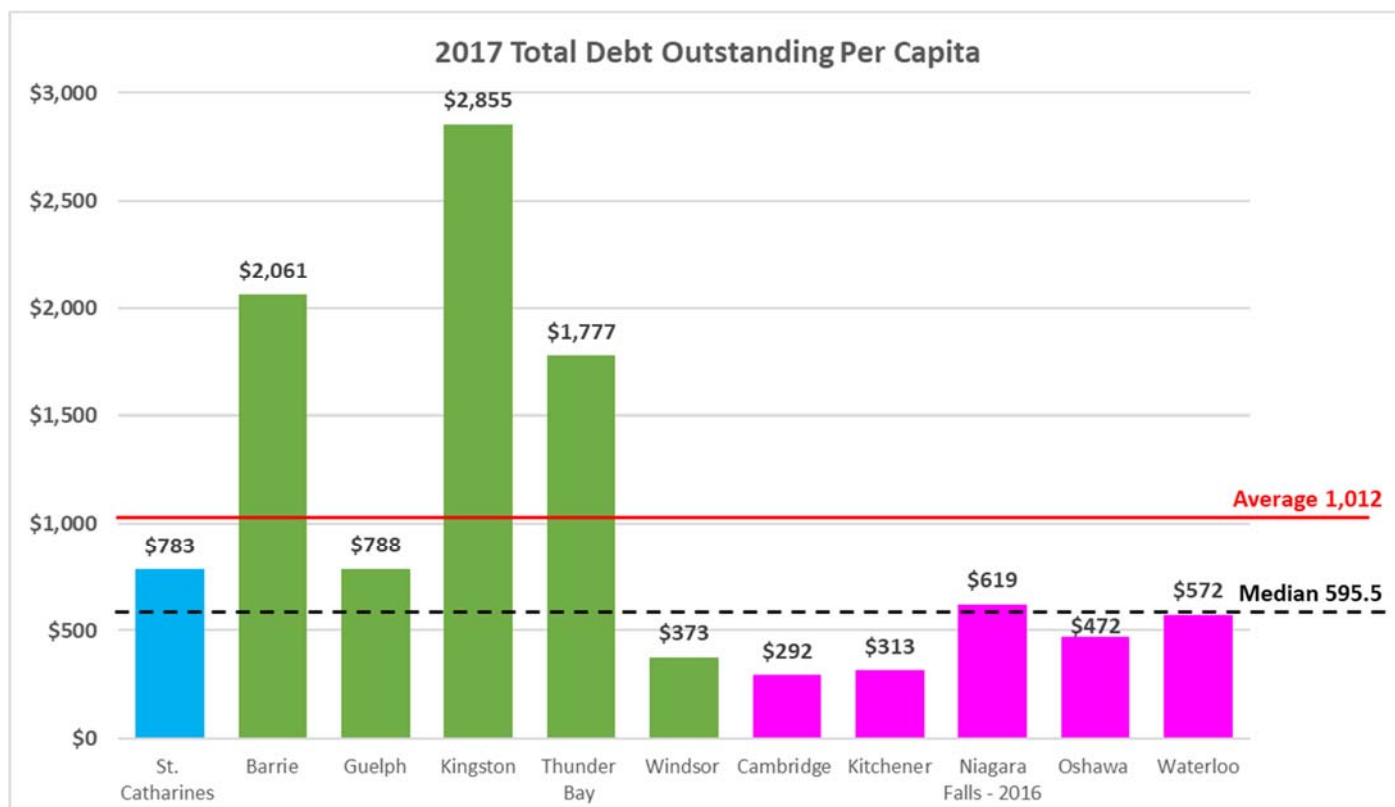
City Council has identified five single-tier and five lower-tier municipalities with similar characteristics to the City of St. Catharines. The chart below compares their debt burdens with the City of St. Catharines to demonstrate how our levels relate.

Debt Levels: Comparator Municipalities



Another common debt amount for debt analysis is debt per capita. The latest data available from the 2018 BMA Study based on 2017 FIRs is shown in the graph below.

Debt Outstanding per Capita from 2018 BMA Study based on 2017 FIR



The City of St. Catharines total debt outstanding per capita for 2017 was \$783, which is slightly above the average for all reporting municipalities in the BMA study. The 2017 average for municipalities in the 2018 BMA Study was \$731 and the median was \$555.

Looking amongst our ten comparators the data is a bit different. The comparator municipalities' average for 2017 was \$1,012 and the median was \$595.5. St. Catharines outstanding debt per capita of \$783 is below the average for the Council approved municipal comparators. The average for these eleven municipalities is \$1,012.

If the top and bottom two municipalities are removed from the average, the remaining eight municipalities' average is \$872 per capita. The per capita amount of St. Catharines would still be below this average. Therefore the City of St. Catharines debt is at a reasonable level in comparison to other municipalities.

Schedule AF

2018 Capital Budget and Four Year Forecast vs. 2019 Capital Budget and Four Year Forecast

This summary of capital expenditures compares the projected four year forecast (2019 - 2022) and capital budget completed in 2018 with the new figures projected in 2019 for the same forecast time period.

2018 CAPITAL BUDGET AND FOUR YEAR FORECAST vs 2019 CAPITAL BUDGET AND FOUR YEAR FORECAST

SUMMARY OF CAPITAL EXPENDITURES

	2019			2020			2021			2022		
	2018 Forecast	2019 Forecast	Variance \$	2018 Forecast	2019 Forecast	Variance \$	2018 Forecast	2019 Forecast	Variance \$	2018 Forecast	2019 Forecast	Variance \$
CAPITAL EXPENDITURES:												
Parks, Recreation and Culture Services	\$2,034,000	\$1,429,000	(\$605,000)	\$2,620,000	\$1,160,000	(\$1,460,000)	\$2,200,000	\$1,570,000	(\$630,000)	\$2,395,000	\$620,000	(\$1,775,000)
Road Construction and Improvements	\$5,046,420	\$5,946,420	\$900,000	\$5,238,277	\$5,296,420	\$58,143	\$5,437,808	\$5,437,808	\$0	\$5,238,277	\$5,645,320	\$407,043
Storm Sewer and Separation	2,524,195	\$3,241,000	\$716,805	\$2,625,162	\$2,625,162	\$0	\$2,730,168	\$2,730,168	\$0	\$2,839,375	\$2,730,168	(\$109,207)
Regional Projects	\$2,622,561	\$750,000	(\$1,872,561)	\$220,000	\$499,785	\$279,785	\$230,000	\$3,374,301	\$3,144,301	\$280,000	\$230,000	(\$50,000)
Niagara District Airport		\$181,800	\$181,800						\$0			\$0
Local Improvements		\$128,000	\$128,000									
Sidewalk/Walkway Construction	\$350,000	\$350,000	\$0	\$350,000	\$350,000	\$0	\$350,000	\$350,000	\$0	\$350,000	\$350,000	\$0
Shoreline Protection	\$100,000	\$400,000	\$300,000	\$30,000	\$500,000	\$470,000	\$110,000	\$610,000	\$500,000	\$110,000	\$500,000	\$390,000
Pollution Control	\$300,000	\$2,000,000	\$1,700,000	\$1,000,000	\$1,000,000	\$0		\$500,000	\$500,000	\$500,000	\$800,000	\$300,000
Heritage Building Rehabilitation						\$0						
Federal Gas Tax funded programs	\$4,185,215	\$4,038,147	(\$147,068)	\$4,185,216	\$4,038,147	(\$147,069)	\$4,185,216	\$4,221,699	\$36,483	\$4,185,216	\$4,221,699	\$36,483
Building Improvements	\$1,220,000	\$5,948,000	\$4,728,000	\$1,800,000	\$5,300,000	\$3,500,000	\$880,000	\$4,395,000	\$3,515,000	\$2,440,000	\$3,430,000	\$990,000
Accessibility Programme	\$250,000	\$250,000	\$0	\$250,000	\$220,000	(\$30,000)	\$1,500,000	\$500,000	(\$1,000,000)	\$300,000	\$260,000	(\$40,000)
Development Charges			\$0	\$3,350,000		(\$3,350,000)		\$2,710,000	\$2,710,000		\$760,000	\$760,000
Fire Services	\$1,800,000	\$4,000,000	\$2,200,000	\$2,000,000		(\$2,000,000)	\$2,000,000	\$0	(\$2,000,000)	\$2,800,000	\$3,400,000	\$600,000
Watercourses	\$650,000	\$420,000	(\$230,000)	\$1,000,000	\$1,000,000	\$0	\$500,000	\$500,000	\$0	\$500,000	\$500,000	\$0
Bridges and Culvert	\$1,970,000	\$1,376,520	(\$593,480)	\$885,000	\$1,641,000	\$756,000		\$975,000	\$975,000			\$0
New Buildings												
Parking Improvements	\$400,000	\$350,000	(\$50,000)	\$600,000	\$600,000	\$0	\$400,000	\$600,000	\$200,000	\$700,000	\$100,000	(\$600,000)
Corporate Infrastructure	\$1,330,000	\$280,000	(\$1,050,000)	\$700,000	\$1,050,000	\$350,000	\$500,000	\$700,000	\$200,000	\$500,000	\$500,000	\$0
St. Catharines Transit Commission		\$250,000	\$250,000			\$0			\$0			\$0
	\$24,782,391	\$31,338,887	\$6,556,496	\$26,853,655	\$25,280,514	-\$1,573,141	\$21,023,192	\$29,173,976	\$8,150,784	\$23,137,868	\$24,047,187	\$909,319

Projects Added/Increased

Park Renewal Program Increased	\$ 11,000
Yates Street (Oakhill Park) Added	\$ 150,000
Sunset Beach Washroom site Added	\$ 70,000
Merritton Park - Grandstands Added	\$ 150,000
Parks, various - water filling stations Added	\$ 50,000
Museum Roof Replacement Increased	\$ 75,000
Road Construction Increased	\$ 900,000
Storm Sewer and Separation Increased	\$ 716,805
Niagara Airport Capital Added	\$ 181,800
Local Imp. Old Fourth Ave watermain Added	\$ 128,000
Shoreline Protection Increased	\$ 300,000
Pollution Control Increased	\$ 1,700,000
Building Improvements Increased	\$ 2,853,000
Fire Tower - Training Centre Added	\$ 2,200,000
Transit - Paratransit Vehicle Added	\$ 250,000
Carlisle Street garage Fit-out Added	\$ 900,000
	\$ 10,635,605

Projects Added/Increased

Basketball Court Upgrades Increased	\$ 75,000
Arena Improvement Program Increased	\$ 1,105,000
Merritton Park - light poles Added	\$ 40,000
Montebello Park - Walkway Added	\$ 75,000
Road Construction/Improvements Increased	\$ 58,143
Regional Projects Increased	\$ 279,785
Shoreline Protection Increased	\$ 470,000
Building Improvements Determined & Increased	\$ 1,395,000
Bridges & Culverts Increased	\$ 756,000
Corp. Inf Determined, Website Added	\$ 350,000
Market Square/City Hall Lot Added	\$ 500,000
	\$ 5,103,928

Projects Added/Increased

Playground Equipment Increased	\$ 80,000
Basketball Court Upgrades Increased	\$ 75,000
Queen Elizabeth Centre - Parking Lot Added	\$ 500,000
Arena Improvement Program Increased	\$ 1,400,000
Port Dalhousie Seniors Scope Increased	\$ 275,000
Regional Projects Increased	\$ 3,144,301
Shoreline Protection Increased	\$ 500,000
Pollution Control Increased	\$ 500,000
Federal Gas Tax Project Increased	\$ 36,483
Building Improvements Increased	\$ 1,240,000
Development Charges Added	\$ 2,710,000
Bridges & Culverts Added	\$ 975,000
Garden Parking Lot Added	\$ 500,000
Corp Inf - Connected City Added	\$ 500,000
	\$ 12,435,784

Projects Added/Increased

Road Construction/Improvements Increased	\$ 407,043
Shoreline Portection Increased	\$ 390,000
Pollution Control Increased	\$ 300,000
Federal Gas Tax Project Increased	\$ 36,483
Arena Improvement Program Increased	\$ 300,000
Development Charges Added	\$ 760,000
Fire Station 2 - Rebuild Added and Increased	\$ 500,000
Fire Station 5 - Renovations Added	\$ 150,000
Fire Station 1 - Design/renovate/expand - Added	\$ 250,000
	\$ 3,093,526

Projects Deferred/Reduced

Tree Planting Program Reduced	\$ 11,000
Arena Improvement Program Reduced	\$ 125,000
Regional Projects Reduced	\$ 1,872,561
Federal Gas Tax Project Reduced	\$ 147,068
Watercourse Program Reduced	\$ 230,000
Bridges and Culverts Reduced	\$ 593,480
Carlisle Garage Membrane Deferred to 2020	\$ 50,000
Corp Inf - Network/Server deferred to 2020	\$ 600,000
Corp Inf - Wireless update deferred to 2020	\$ 100,000
Corp Inf - Website deferred to 2020	\$ 350,000
	\$ 4,079,109

Projects Deferred/Reduced

Playground Equipment Decreased and Deferred	\$ 75,000
Port Lions Pool Changerooms Deferred	\$ 550,000
Tree Planting Program Reduced	\$ 25,000
Federal Gas Tax Project Reduced	\$ 147,069
Accessibility Programme - Reduced	\$ 30,000
Development Charges Deferred	\$ 3,350,000
Fire Services Projects moved to 2019	\$ 2,000,000
Garden Parking Lot Deferred	\$ 500,000

Projects Deferred/Reduced

Tree Planting Program Reduced	\$ 25,000
Russell Ave Washroom Upgrades Deferred	\$ 660,000
Accessibility Programme - Reduced	\$ 1,000,000
Fire Services Projects Deferred	\$ 2,000,000
Market Square/City Hall Lot Moved to 2020	\$ 300,000
Corp Inf - Network/Server Reduced	\$ 300,000

Projects Deferred/Reduced

Montebello Park Walkway Deferred	\$ 75,000
West Park Lighting Controls Deferred	\$ 100,000
Tennis Court Upgrades Deferred	\$ 200,000
VLC Walkways at Holy Trinity Deferred	\$ 100,000
Burgoyne Woods Driveway Deferred	\$ 600,000
Building Improvements Decreased	\$ 10,000
Storm Sewer and Separation Reduced	\$ 109,207
Regional Projects Reduced	\$ 50,000
Fire Station 4 - Redundant UPS Removed	\$ 300,000
Accessibility Programme - Reduced	\$ 40,000
Garden Park Parking Lot Moved to 2021	\$ 600,000

\$ 4,079,109

\$ 6,556,496

\$ 6,677,069

\$ (1,573,141)

\$ 4,285,000

\$ 8,150,784

\$ 2,184,207

\$ 909,319

Schedule AG

2019 CONSTRUCTION PROGRAM - WARD LISTING - FACILITIES

PROJECT	FACILITY NAME	PROJECT TITLE	DESCRIPTION	BUDGET	WARD
P19-176	BARLEY DRIVE PARK	Basketball Court Renewal	Asset renewal to address identified deficiency	Building - Capital	1
P19-176	DOUGLAS PARK	Play Structure Renewal	Replace with structure having accessible features and paths meeting FADS	Building - Capital	1
Maint 2019-8	FIRE HALL #1 (GENEVA ST)	Roof Maintenance	Preventative maintenance	Building - Annual	1
ST19-009	FIRE HALL #1 (GENEVA ST)	Condition Assessment	To identify building deficiencies.	Building - Studies	1
Maint 2019-13	FIRE HALL #4 (MERRITT ST)	Roof Maintenance	To address identified deficiency.	Building - Annual	1
P19-152	FIRE TRAINING CENTRE	Fire Training Centre	New facility	Building - Capital	1
P19-152	FIRE TRAINING TOWER	Fire Training Tower	Asset renewal	Building - Capital	1
P17-161	GARDEN CITY ARENA	Low Roof and Steel Roof Replacements	Roof repairs, additional funds	Building - Capital	1
Maint 2019-19	MERRITTON COMMUNITY CENTRE	Roof Maintenance	To address identified deficiency.	Building - Annual	1
P19-135	MERRITTON COMMUNITY CENTRE	Roof Replacement	Asset renewal.	Building - Capital	1
P19-137-1	MERRITTON COMMUNITY PARK	Change Room Renovation	Renovate and add Barrier Free Washrooms	Building - Capital	1
P19-137-2	MERRITTON COMMUNITY PARK	Grandstand Improvements	Paint & repair Structure	Building - Capital	1
P19-161	MERRITTON COMMUNITY PARK	Storage Shed Roof Replacement	Asset renewal.	Building - Capital	1
P19-138	PORT DALHOUSIE LION'S PARK	Large Pool Maintenance	Sandblast and paint, preventative maintenance	Building - Capital	1
Maint 2019-21	ST. CATHARINES MUSEUM	Roof Maintenance	To address identified deficiency.	Building - Annual	1
P19-181	ST. CATHARINES MUSEUM	Roof Replacement	Replace steel roof and improve insulating value.	Building - Capital	1
P19-182	ST. CATHARINES MUSEUM	Concrete Walkway Replacement	To address deficiency.	Building - Capital	1
Maint 2019-20	VICTORIA LAWN CEMETERY	Garden of Memories - Low Roof Repairs	Caulking, flashing and roof repairs to address deficiencies	Building - Annual	1
P19-167	VICTORIA LAWN CEMETERY	Roof Replacement	Asset renewal.	Building - Capital	1
P19-177	BURGOYNE WOODS	Parks Water Filling Stations	Encourage use of refillable water bottles	Building - Capital	2
Maint 2019-11	FIREHALL #3 (PELHAM RD)	Bay Door Painting	To address identified deficiency.	Building - Annual	2
ST19-011	FIREHALL #3 (PELHAM RD)	Condition Assessment	To identify building deficiencies.	Building - Studies	2
Maint 2019-17	JOE MCCAFFERY PARK	Roof Maintenance	To address identified deficiency.	Building - Annual	2
P19-177	KIWANIS FIELD	Parks Water Filling Stations	Encourage use of refillable water bottles	Building - Capital	2
P19-188	MERIDIAN CENTRE	Security Improvements	To address identified deficiency.	Building - Capital	2
P19-189	MERIDIAN CENTRE	Elevator Improvements	Preventative maintenance.	Building - Capital	2
Maint 2019-22	MORNINGSTAR MILL	House and both Mill Buildings - Roof Maintenance	To address deficiency in cedar roofs	Building - Annual	2
P19-164	SEYMOUR HANNAH ARENA	Compressor Actuators Replacement	Preventative maintenance	Building - Capital	2
P19-144	BUCHANAN HOUSE	Partial Roof Replacement	Asset renewal.	Building - Capital	3
P19-175	KIWANIS ACQUATICS CENTRE AND LIBRARY	Basement Concrete Repairs	To address identified deficiency.	Building - Capital	3
P19-177	LANCASTER PARK	Parks Water Filling Stations	Encourage use of refillable water bottles	Building - Capital	3
P19-174	LESTER B PEARSON PARK	Splash Pad - Upgrade Water Distribution Centre	Improve user experience.	Building - Capital	3

PROJECT	FACILITY NAME	PROJECT TITLE	DESCRIPTION	BUDGET	WARD
P19-177	PEARSON PARK	Parks Water Filling Stations	Encourage use of refillable water bottles	Building - Capital	3
Maint 2019-6	CARLISLE STREET PARKING GARAGE	South End Roof Drain Repairs	To address identified deficiency.	Building - Annual	4
P19-151	CARLISLE STREET PARKING GARAGE	Waterproofing	Annual repairs	Building - Capital	4
P19-186	CARLISLE STREET PARKING GARAGE	Roof/Ceiling Survey	To identify clearances	Building - Annual	4
Maint 2019-23	CENTENNIAL PUBLIC LIBRARY	Roof Maintenance	To address identified deficiency.	Building - Annual	4
Maint 2019-1	CITY HALL	Roof Maintenance	To address identified deficiency.	Building - Annual	4
P19-140	CITY HALL	James Street Atrium Study / Preliminary Design	Engineering and Architectural services to address identified deficiencies in stairwell.	Building - Capital	4
P19-141	CITY HALL	UPS Capacitor Upgrade	To address identified deficiency.	Building - Capital	4
P19-184	CITY HALL UPGRADES	Ante Room Improvements	Replace flooring and paint to address building deficiency.	Building - Annual	4
Maint 2019-18	DUNLOP DRIVE SENIORS CENTRE	Roof Maintenance	To address identified deficiency.	Building - Annual	4
Maint 2019-12	FIREHALL #5 (MARTINDALE RD)	Bay Door Painting	To address identified deficiency.	Building - Annual	4
P19-158	FIREHALL #5 (MARTINDALE RD)	Roof Replacement	Asset renewal.	Building - Capital	4
Maint 2019-2	LAKE STREET SERVICE CENTRE	Wash Bay - Roof Maintenance	To address identified deficiency.	Building - Annual	4
P19-142	LAKE STREET SERVICE CENTRE	Security - Phase 2	Completion of Security Upgrade	Building - Capital	4
P19-134	MONTEBELLO PARK	Bandshell Repairs	To address deficiencies.	Building - Capital	4
P19-134	MONTEBELLO PARK	Bandshell Roof Replacement	Asset renewal.	Building - Capital	4
P19-134	MONTEBELLO PARK	Washroom Roof Replacement	Asset renewal.	Building - Capital	4
P19-177	MONTEBELLO PARK	Parks Water Filling Stations	Encourage use of refillable water bottles	Building - Capital	4
ST19-012	MONTEBELLO PARK	Condition Assessment	Assess pathways for accessibility compliance	Building - Studies	4
P17-135	OAKHILL PARK	Yates Street Stone Wall Repairs	Repair existing heritage wall, additional funds	Building - Capital	4
P19-143	OLD COURT HOUSE	Fire Alarm Panel Replacement	To address deficiency.	Building - Capital	4
P19-185	OLD COURT HOUSE	Misc. Improvements	Miscellaneous improvements following staff move	Building - Annual	4
Maint 2019-7	ONTARIO STREET PARKING GARAGE	Fire Alarm Panel Replacement	To address identified deficiency.	Building - Annual	4
P19-150	ONTARIO STREET PARKING GARAGE	LED Lighting Conversion	To improve energy efficiency.	Building - Capital	4
ST19-013	ONTARIO STREET PARKING GARAGE	Strand Monitoring	Annual Strand Monitoring Program	Building - Studies	4
P17-179	RACE STREET PARKING LOT	Parking Lot Rehabilitation	Reconstruct existing parking lot, construction funds	Building - Capital	4
Maint 2019-5	ROBERTSON SCHOOL (ANNEX)	Roof Maintenance	Preventative maintenance	Building - Annual	4
P19-148	ROBERTSON SCHOOL (ANNEX)	Replace Windows	To improve energy deficiency.	Building - Capital	4
P19-147	ROBERTSON SCHOOL (MAIN)	Partial Roof Replacement	Asset renewal.	Building - Capital	4
P19-149	ROBERTSON SCHOOL (MAIN)	Accessibility Upgrades, elevator, washrooms, etc.	Accessibility upgrades, subject to EAF funding	Building - Capital	4
P19-172	RUSSELL AVE COMMUNITY CENTRE	Splash Pad - Upgrade Water Distribution Centre	Improve user experience.	Building - Capital	4
P13-131	HAPPY ROLPH'S BIRD SANCTUARY	House Demolition	Abandon services, additional funds	Building - Capital	5
P19-170	HAPPY ROLPH'S BIRD SANCTUARY	Barn Roof Replacement	Asset renewal.	Building - Capital	5
P19-139	MALCOLMSON PARK	Office Building Roof Replacement	Asset renewal.	Building - Capital	5
P19-178	PORT WELLER COMMUNITY CENTRE	Window Replacement	Address deficiency and improve energy efficiency	Building - Capital	5

PROJECT	FACILITY NAME	PROJECT TITLE	DESCRIPTION	BUDGET	WARD
P19-179	SUNSET BEACH (formerly MUNICIPAL BEACH)	New Washroom - Design	Engineering and Architectural Services	Building - Capital	5
Maint 2019-14	BILL BURGOYNE ARENA	Roof Maintenance	To address deficiency.	Building - Annual	6
P19-165	BILL BURGOYNE ARENA	Ice Plant Rebuild	Asset renewal.	Building - Capital	6
P19-166	BILL BURGOYNE ARENA	Fire Alarm Panel Replacement	Asset renewal.	Building - Capital	6
Maint 2019-10	FIREHALL #2 (LINWELL RD)	Roof Maintenance	To address identified deficiency.	Building - Annual	6
Maint 2019-9	FIREHALL #2 (LINWELL RD)	Garage Bay Door Painting	Preventative maintenance	Building - Annual	6
ST19-010	FIREHALL #2 (LINWELL RD)	Condition Assessment	To identify building deficiencies.	Building - Studies	6
P19-133	JAYCEE PARK	Garage upgrades	To improve ventilation and address health and safety concerns.	Building - Capital	6
Maint 2019-16	JAYCEE PARK (GARAGE)	Roof Maintenance	To address identified deficiency.	Building - Annual	6
Maint 2019-24	LOCK TENDERS SHANTY	Roof Maintenance	To address deficiency in cedar roof	Building - Annual	6
P19-138	MERRITTON COMMUNITY PARK	Small Pool Maintenance	Sandblast and paint, preventative maintenance	Building - Capital	6
P19-183	PORT DALHOUSIE LIBRARY	Entrance Ramp Improvements	Replace or install new ramp to meet FADS	Building - Capital	6
Maint 2019-3	REAR INNER RANGE LIGHTHOUSE	Crawlspace - Fan installation	To prevent further moisture damage	Building - Annual	6
Maint 2019-4	REAR INNER RANGE LIGHTHOUSE	Roof Maintenance	Preventative maintenance	Building - Annual	6
Maint 2019-15	SEYMOUR HANNAH	Roof Maintenance	To address identified deficiency.	Building - Annual	6
ST19-001	GENERAL CONSULTING	Roof Reviews	Annual review.	Building - Studies	7
ST19-002	GENERAL CONSULTING	Fall Arrest Inspections	Annual review.	Building - Studies	7
ST19-003	GENERAL CONSULTING	Asbestos Annual Update Inspections	Annual review.	Building - Studies	7
ST19-004	GENERAL CONSULTING	Energy Audits	Annual review.	Building - Studies	7
ST19-006	GENERAL CONSULTING	Misc. Building Studies	Annual review.	Building - Studies	7
ST19-014	GENERAL CONSULTING	Refrigeration Contract	Refrigeration Contract renewal.	Building - Studies	7
P17-180	VARIOUS	CORPORATE CONSOLIDATION (PHASE 2)	Additional funds	Building - Capital	7

Ward
1 – Merritton
2 – St. Andrews
3 – St. Georges
4 – St. Patricks
5 – Grantham
6 – Port Dalhousie
7 – City Wide

Schedule AH

2019 CONSTRUCTION PROGRAM - WARD LISTING - NON FACILITIES

PROJECT	PROJECT TITLE	LOCATION	FROM	TO	BUDGET	WARD
P19-067	ARGLYE CRESCENT ROAD IMPROVEMENTS	Argyle Cres	Collier Street	Westchester Cres	Road - Capital	1
P19-080	2019 RESURFACING PROGRAM	Brackencrest Road	Woodrow Street	St Augustine Drive	Road - Capital	1
P19-130	CLIFFORD CREEK PARK - WATERCOURSE PROTECTION	Clifford Creek Park	—		Watercourses	1
P19-080	2019 RESURFACING PROGRAM	Dunvegan Road	Carriage Road	Woodrow Street	Road - Annual	1
P18-061	GALE CRESCENT ROAD IMPROVEMENTS	Gale Cres	Geneva Street	Oakdale Avenue	Road - Capital	1
P13-019	BURLEIGH HILL / GLENDALE AVENUE CSO CONTROL	Glengarry Road	Glendale Avenue	Glengarry Park	Pollution Control Plan - Capital	1
P18-063	MERRITT STREET ROAD IMPROVEMENTS	Merritt Street	Almond Lane	Walnut Street	Road - Capital	1
P15-066	MOFFATT STREET RECONSTRUCTION	Moffatt Street	North Limit	South Limit	Road - Capital	1
P15-066	MOFFATT STREET RECONSTRUCTION	Moffatt Street	Disher Street	100m south of Disher Street	Sanitary Sewer - Annual	1
P19-066-1	MOFFATT STREET SECONDARY ACCESS	Moffatt Street	Location to be determined	—	Road - Capital	1
P17-022	PAGE STREET SEWER RECONNECTION	Page Street	Cross Street	#10 Page Street	Storm Sewer - Capital	1
P19-131	OLD WELLAND CANAL LOCKWALL STABILIZATION	Second Welland Canal	Lock 9		Watercourses	1
P19-131	OLD WELLAND CANAL LOCKWALL STABILIZATION	Second Welland Canal	Lock 7		Watercourses	1
P19-131	OLD WELLAND CANAL LOCKWALL STABILIZATION	Second Welland Canal	Lock 6		Watercourses	1
RN19-xxb	REGION - ST. DAVIDS ROAD RECONSTRUCTION	St Davids Road	Barbican Gate	Burleigh Hill Drive	Sidewalk Replacement - Gas Tax - Capital	1
P17-067	ST DAVIDS ROAD	St Davids Road	Burleigh Hill Drive	Townline Road	Road - Capital	1
P17-067	ST DAVIDS ROAD	St Davids Road	Burleigh Hill Drive	Leeson Street	Sanitary Sewer - Annual	1
P17-067	ST DAVIDS ROAD	St Davids Road	Burleigh Hill Drive	250m East of Allanburg Road	Water	1
ST19-06	TREMONT RETAINING WALL	Tremont Drive	Allandale Drive	Approx. 90m west of Allandale Drive	Road - Annual	1
P17-009	YALE CRESCENT UNDERGROUND IMPROVEMENTS	Yale Cres	Yale Cres	Berryman Avenue	Sanitary Sewer - Annual	1
P17-009	YALE CRESCENT UNDERGROUND IMPROVEMENTS	Yale Cres	Yale Cres	Berryman Avenue	Storm Sewer - Capital	1
P19-080	2019 RESURFACING PROGRAM	Glen Morris Drive	Jacobson Avenue	Briarsdale Drive	Road - Capital	2
P19-080	2019 RESURFACING PROGRAM	Merigold Street	St Paul Street W	Chetwood Street	Road - Capital	2
P17-123	PELHAM RD BRIDGE	Pelham Road	CNR	—	Bridge & Culvert - Replacement - Capital	2
P19-065	RIDLEY ROAD REALIGNMENT AND ROAD IMPROVEMENTS	Ridley Road	Louth Street	Henrietta Street	Road - Capital	2
P19-080	2019 RESURFACING PROGRAM	Tunis Street	Pelham Road	Hillview Road	Road - Annual	2
P19-102	ARRAN/DIXIE WATERMAIN	Arran Drive	Sherman Drive	Mccordick Drive	Water	3
P19-102	ARRAN/DIXIE WATERMAIN	Dixie Road	Greenmeadow Court	Wood Street	Water	3
P19-080	2019 RESURFACING PROGRAM	Dorchester Blvd	Carlton Street	Meredith Drive	Road - Capital	3
P19-080	2019 RESURFACING PROGRAM	Meredith Drive	Ancaster Blvd	Rendale Avenue	Road - Annual	3
P19-080	2019 RESURFACING PROGRAM	Meredith Drive	Rendale Avenue	Ancaster Blvd	Road - Capital	3
P18-101	RENDALE AVENUE WATERMAIN	Rendale Avenue	Dorchester Blvd	Cushman Road	Water	3
P19-080	2019 RESURFACING PROGRAM	Welland Avenue	Dieppe Road	Grantham Avenue	Road - Capital	3
P19-080	2019 RESURFACING PROGRAM	Brewery Street	Cul-de-sac	Salina Street	Road - Annual	4
P19-080	2019 RESURFACING PROGRAM	Dorothy Street	Meadowvale Drive	Dawn Road	Road - Annual	4

2019 CONSTRUCTION PROGRAM - WARD LISTING - NON FACILITIES

PROJECT	PROJECT TITLE	LOCATION	FROM	TO	BUDGET	WARD
P19-101	FOURTH AVENUE WATER SERVICING	Fourth Avenue	Old Fourth Avenue	Fourth Avenue	Water - Capital	4
P17-103	HAIG / VENTURA UNDERGROUND IMPROVEMENTS	Haig Street	Cayuga Street	Scott Street	Water	4
P19-002	HAMPSTEAD PLACE ROAD RECONSTRUCTION	Hampstead Place	Carlton Street	Manchester Avenue	Road - Capital	4
P19-002	HAMPSTEAD PLACE ROAD RECONSTRUCTION	Hampstead Place	Carlton Street	Manchester Avenue	Sanitary Sewer - Annual	4
P19-002	HAMPSTEAD PLACE ROAD RECONSTRUCTION	Hampstead Place	Carlton Street	Manchester Avenue	Storm Sewer - Capital	4
P19-002	HAMPSTEAD PLACE ROAD RECONSTRUCTION	Hampstead Place	Carlton Street	Manchester Avenue	Water	4
P19-080	2019 RESURFACING PROGRAM	Heritage Court	Huntington Lane	Cul-de-sac	Road - Capital	4
P19-081	KING STREET HYDRO RELOCATION	King Street	James Street	Carlisle Street	Road - Annual	4
P19-081	KING STREET HYDRO RELOCATION	King Street	James Street	Carlisle Street	Sidewalk - Replacement Annual	4
P18-062	LAKE STREET RECONSTRUCTION	Lake Street	Welland Avenue	Queen Street	Road - Capital	4
RN19-xxa	REGION - MARTINDALE ROAD RECONSTRUCTION	Martindale Road	Bridge Street	Fourth Avenue	Regional Cost Sharing - Capital	4
RN19-xxa	REGION - MARTINDALE ROAD RECONSTRUCTION	Martindale Road	Bridge Street	Approx. 75m north of Fourth Avenue	Sidewalk - New or Replacement - Capital	4
RN19-xxa	REGION - MARTINDALE ROAD RECONSTRUCTION	Martindale Road	Martindale Road	Sawmill Road	Water	4
P19-080	2019 RESURFACING PROGRAM	McGuire Street	St Paul Street W	St Paul Cres	Road - Annual	4
P19-101-1	LOCAL IMPROVEMENT CHARGES FOR FOURTH AVENUE WATER SERVICING	Old Fourth Avenue	Fourth Avenue	#1507 Fourth Avenue	Water - Capital	4
P19-080	2019 RESURFACING PROGRAM	Salina Street	Yates Street	Brewery Street	Road - Annual	4
P18-045	ST. PAUL STREET AT WILLIAM STREET - INTERSECTION AND PLAZA IMPROVEMENTS	St Paul Street	William Street		Road - Capital	4
P17-121	THIRD AVENUE LOUTH / GLASS AVENUE BRIDGE REHABILITATION	Third Avenue	Fifteen Mile Creek	—	Bridge & Culvert - Replacement - Capital	4
P15-122	THIRD STREET LOUTH CULVERT REPLACEMENT	Third Street	Approx. 95m south of CNR		Bridge & Culvert - Replacement - Capital	4
P15-067	THIRD STREET LOUTH ROAD IMPROVEMENTS	Third Street	South Service Road	Hutton Road	Road - Capital	4
P17-103	HAIG / VENTURA UNDERGROUND IMPROVEMENTS	Ventura Drive	Garfield Lane	St Hildas Court	Sanitary Sewer - Annual	4
P17-103	HAIG / VENTURA UNDERGROUND IMPROVEMENTS	Ventura Drive	Haig Street	Haig Street	Water	4
P19-080	2019 RESURFACING PROGRAM	Yates Street	St Paul Street	Salina Street	Road - Annual	4
P19-100	BRADMON DRIVE WATERMAIN	Bradmon Drive	Parnell Road	Lakeshore Road	Water	5
P18-100	CHAMPA DRIVE WATERMAIN	Champa Drive	Scott Street	Agar Drive	Water	5
P17-124	CINDY / LAKE / OLD COACH CULVERT REPLACEMENTS	Cindy Drive	Walker's Creek		Bridge and Culvert - Gas Tax - Capital	5
P17-124	CINDY / LAKE / OLD COACH CULVERT REPLACEMENTS	Cindy Drive	Walker's Creek		Bridge & Culvert - Replacement - Capital	5
P19-001	CLOVER / NIAGARA SEWER IMPROVEMENTS	Clover Street	Heywood Avenue	—	Sanitary Sewer - Annual	5
P19-001	CLOVER / NIAGARA SEWER IMPROVEMENTS	Easement Parallel to Niagara Street	Mckenzie Street	Heywood Avenue	Sanitary Sewer - Annual	5
P16-003	ELSE STREET	Else Street	Linwell Road	Scott Street	Storm Sewer - Capital	5
P17-124	CINDY / LAKE / OLD COACH CULVERT REPLACEMENTS	Old Coach Road	Spring Garden Creek		Bridge and Culvert - Gas Tax - Capital	5
P17-124	CINDY / LAKE / OLD COACH CULVERT REPLACEMENTS	Old Coach Road	Spring Garden Creek		Bridge & Culvert - Replacement - Capital	5
P19-080	2019 RESURFACING PROGRAM	Albany Drive	St Columba Drive	Jefferson Drive	Road - Annual	6
P19-020	AVALON / BERNHARDT / EMBASSY ROAD AND UNDERGROUND IMPROVEMENTS	Avalon Place	Lakeport Road	South Limit	Storm Sewer - Capital	6
P19-020	AVALON / BERNHARDT / EMBASSY ROAD AND UNDERGROUND IMPROVEMENTS	Avalon Place	Lakeport Road	South Limit	Water	6

2019 CONSTRUCTION PROGRAM - WARD LISTING - NON FACILITIES

PROJECT	PROJECT TITLE	LOCATION	FROM	TO	BUDGET	WARD
P19-020	AVALON / BERNHARDT / EMBASSY ROAD AND UNDERGROUND IMPROVEMENTS	Bernhardt Street	Avalon Place	Embassy Avenue	Storm Sewer - Capital	6
P19-020	AVALON / BERNHARDT / EMBASSY ROAD AND UNDERGROUND IMPROVEMENTS	Bernhardt Street	Avalon Place	Embassy Avenue	Water	6
P19-080	2019 RESURFACING PROGRAM	Bowstead Drive	Marsten Drive	Glenholme Drive	Road - Annual	6
P19-080	2019 RESURFACING PROGRAM	Chaffey Street	Jefferson Drive	Albany Drive	Road - Annual	6
P19-020	AVALON / BERNHARDT / EMBASSY ROAD AND UNDERGROUND IMPROVEMENTS	Embassy Avenue	Lakeport Road	South Limit	Storm Sewer - Capital	6
P19-020	AVALON / BERNHARDT / EMBASSY ROAD AND UNDERGROUND IMPROVEMENTS	Embassy Avenue	Lakeport Road	South Limit	Water	6
P19-080	2019 RESURFACING PROGRAM	Glenholme Drive	Geneva Street	Albany Drive	Road - Annual	6
P19-132	2019 SHORELINE PRIORITY PROGRAM	Lake Ontario	Christie Street	Colton Avenue	Shoreline - Capital	6
P19-132	2019 SHORELINE PRIORITY PROGRAM	Lake Ontario	Various	—	Shoreline - Capital	6
P17-124	CINDY / LAKE / OLD COACH CULVERT REPLACEMENTS	Lake Street	Beverly Hills Creek	—	Bridge and Culvert - Gas Tax - Capital	6
P17-124	CINDY / LAKE / OLD COACH CULVERT REPLACEMENTS	Lake Street	Beverly Hills Creek	—	Bridge & Culvert - Replacement - Capital	6
P19-080	2019 RESURFACING PROGRAM	Marsten Drive	Geneva Street	Albany Drive	Road - Annual	6
P19-080	2019 RESURFACING PROGRAM	St Columba Drive	Geneva Street	Albany Drive	Road - Annual	6
P17-068	TERRY LANE RECONSTRUCTION	Terry Lane	Shelley Avenue	Graham Avenue	Storm Sewer - Capital	6
P17-068	TERRY LANE RECONSTRUCTION	Terry Lane	Shelley Avenue	Graham Avenue	Water	6
P19-092	2019 BOULEVARD REINSTATEMENTS	City Wide	—	—	Road - Annual	7
P19-122	2019 BRIDGE AND LARGE CULVERT INSPECTION AND MAINTENANCE	City Wide	—	—	Bridge & Culvert - Repair - Annual	7
P19-120	2019 BRIDGE REHABILITATION PROGRAM	City Wide	—	—	Bridge & Culvert - Repair - Annual	7
P19-014	2019 CCTV SEWER INSPECTIONS	City Wide	—	—	Sanitary Sewer - Annual	7
P19-082	2019 CONCRETE BASE REPAIR PROGRAM	City Wide	—	—	Road - Annual	7
P19-083	2019 CRACK SEALING PROGRAM	City Wide	—	—	Road - Annual	7
P19-003	2019 EXTRANEIOUS FLOW ELIMINATION	City Wide	—	—	Sanitary Sewer - Annual	7
P19-085	2019 GUIDERAIL PROGRAM	City Wide	—	—	Road - Annual	7
P19-121	2019 LARGE CULVERT REHABILITATION PROGRAM	City Wide	—	—	Bridge & Culvert - Repair - Annual	7
P19-041	2019 NEW SIDEWALK PROGRAM	City Wide	—	—	Sidewalk - New or Replacement - Capital	7
P19-012	2019 SANITARY SEWER FLUSHING & REAMING	City Wide	—	—	Sanitary Sewer - Annual	7
P19-011	2019 SANITARY SEWER SPOT REPAIR PROGRAM	City Wide	—	—	Sanitary Sewer - Annual	7
P19-040	2019 SIDEWALK REPAIRS	City Wide	—	—	Sidewalk - Replacement - Annual	7
P19-040-1	2019 WALKWAY REPAIRS	City Wide	—	—	Sidewalk - New or Replacement - Capital	7
P19-069	ACTIVE TRANSPORTATION	City Wide	—	—	Road - Gas Tax - Capital	7
P19-006	CATCH BASIN AND STORM SERVICE INSTALLATIONS	City Wide	—	—	Storm Sewer - Capital	7
P19-086	DESIGN FOR 2020 PROJECTS	City Wide	—	—	Road - Annual	7
P19-015	DESIGN FOR 2020 SEWER PROJECTS	City Wide	—	—	Sanitary Sewer - Annual	7
P19-015	DESIGN FOR 2020 SEWER PROJECTS	City Wide	—	—	Storm Sewer - Capital	7
P19-119	DESIGN FOR 2020 WATERWORKS PROJECTS	City Wide	—	—	Water	7

2019 CONSTRUCTION PROGRAM - WARD LISTING - NON FACILITIES

PROJECT	PROJECT TITLE	LOCATION	FROM	TO	BUDGET	WARD
ST19-05-1	ROAD DATA BASE AND SOFTWARE (CURBS)	City Wide	–	–	Road - Annual	7
ST19-05-2	ROAD DATA BASE AND SOFTWARE (PAVEMENT MARKINGS)	City Wide	–	–	Road - Annual	7
P19-016	STORM SEWER SPOT REPAIRS	City Wide	–	–	Storm Sewer - Capital	7
P19-118	VALVES, HYDRANTS & SERVICES	City Wide	–	–	Water	7
ST18-01	WASTEWATER MASTERPLAN	City Wide	–	–	Sanitary Sewer - Annual	7
P15-117	WATER METER REPLACEMENT	City Wide	–	–	Water - Gas Tax - Capital	7
ST19-05	ROAD DATA BASE AND SOFTWARE	Software Updates			Road - Annual	7
P19-080	2019 RESURFACING PROGRAM	Various	–	–	Road - Annual	7
P19-080	2019 RESURFACING PROGRAM	Various	–	–	Road - Capital	7

Ward
1 – Merritton
2 – St. Andrews
3 – St. Georges
4 – St. Patricks
5 – Grantham
6 – Port Dalhousie
7 – City Wide

Appendix 2**Asset Management Working Group (AMWG) Mandate and Membership****Formation Details**

The Asset Management Working Group (AMWG) was established in 2018 to achieve regulatory compliance with Reg 588/17 and to promote Corporate Asset Management utilizing various functional areas' skills from across the Corporation.

Goals

1. Through sharing expertise and working collaboratively within a multi-functional group, build and influence corporate processes and drive changes that improve;
 - the integration of asset management with corporate strategy, budgeting and planning
 - the provision of reliable information including risk analysis, to inform decisions about balancing levels of service delivered by assets and related long term affordability
 - the resiliency of service delivery in the face of changing climate and citizen needs
 - accountability and transparency of the City's stewardship of the its assets
2. To strengthen communication, education, and engagement with citizens and other stakeholders related to Corporate Asset Management.
3. To strengthen communication, education, and engagement with senior management (SMT) and Council related to Corporate Asset Management.

Governance

AMWG will report to the Deputy Chief Administrative Officer (Deputy CAO).

Membership/Composition

The Committee will have a core membership and then will invite specific additional members as listed below depending upon the specific agenda for a meeting.

Core Membership

The core working group consists of the following 8 representatives:

Title	Current Incumbent
Director Transportation & Environmental Services (TES)	Dan Dillon
Director of Financial Management Services and City Treasurer (FMS)	Kristine Douglas
Director of Parks, Recreation and Culture Services (PRCS)	Phil Cristi
Manager of Engineering and Construction (TES)	Christine Adams
Senior Manager, Facilities and Energy (TES)	Anthony Martuccio
Manager of Accounting and Payroll (FMS)	Adam Smith
Manager of Corporate Communications (Office of CAO)	Maggie Riopelle
Corporate Asset Manager (FMS)	Anne Tourigny

The Deputy CAO may also attend any of the meetings and vote as a member of quorum.

Additional Members

Title	Current Incumbent
Director of Planning and Building Services (PBS)	Tammi Kitay
Fire Chief (FEMS)	Jeff McCormick (Acting)
Senior Manager of Information Technology, (CSS)	Karthik Venkataraman
Accessibility Coordinator, (CSS)	Linda Murray (Acting)
Manager, Geomatics (TES)	Dave Stringer
Senior Operations Manager (Fleet) (TES)	Jim Thompson



CITY OF
ST. CATHARINES

Memorandum

To: Budget Standing Committee

cc: S. Chemnitz; D. Oakes; K. Douglas; M. Kreuk

From: Dan Dillon, P.Eng. – Director-TES

Date: November 28, 2018

Subject: Facer Street EA

At its meeting of March 26th, Council passed the following motion:

“That Council direct staff to prepare a report on the costs and terms of an Environmental Assessment for Facer Street which would implement the Community Vision in the 8-80 Cities Study with an emphasis on a European Design for the street as approved as Council’s vision for the street at the Council Meeting of July 10, 2017, and the other 21 recommendations in the 8-80 Cities Study which was received by Council.”

Background

At its meeting of January 12, 2017, the Budget Standing Committee received a memo (attached as Appendix 1) from the Director of TES regarding the potential revitalization/reconstruction of Facer Street.

In that previous memo, the approximate costs and general terms of reference for an Environmental Assessment (EA) were outlined. As indicated in the earlier memo, an EA process would entail consultation with various regulatory agencies, stakeholders and members of the public in order to:

- identify and define the problem,
- consider and evaluate alternatives, and
- identify ways to reduce any potentially unfavorable outcomes

Input from this consultation process would assist staff in weighing the many factors involved with the project and helping to identify the best solution.

The preliminary estimated cost for an EA for Facer St is approximately \$100,000. At its meeting of March 5, 2018, Council considered, as part of the deliberations for the 2018

Capital Budget, including an amount of \$100,000 for an EA for Facer Street, however that motion was lost.

Report

In 2017, the City engaged the services of 8 80 Cities to prepare a Community Engagement Report on “The Future of Facer Street”. This report was presented to Council at its meeting of July 10, 2017. As a result of this community engagement process, a list of 21 recommendations was prepared, broken into short, medium and long-term improvements that may be led by either the City or the Community. This list of improvements was also characterized in terms of general cost and impact. Staff have expanded upon the list (refer to Appendix 2) to further approximate costs (where known) and provide additional comments.

While many of the longer term improvements (such as some types of entrance features, cross-walk upgrades, lighting upgrades, curb bulb-outs and street and/or sidewalk pavers) would be better considered as part of a larger reconstruction project which would initially require an EA to scope the final design, there are many other initiatives or improvements that could be undertaken more quickly and perhaps still have significant impact.

As an example, in 2018, staff worked with the Facer Street community representatives to supply and install historic street name signs along portions of Facer Street. As part of their Draft 2019 Work Plan, the Economic, Development & Tourism Services Department has indicated their intent to work with the Facer Street community representatives to install banners along Facer Street. Other opportunities exist in working with the community to install improved seating, garbage receptacles, planters, etc. An opportunity may exist in working with the Green Committee to install a “Welcome to Facer Street District” entrance sign, similar to the “Welcome to ...” flower beds and signs that have been installed in the Merritton, Western Hill and Glen Ridge areas over the years.

As indicated in the January 12, 2017 memo, from a purely pavement or asset management perspective, the recommended approach would be to resurface Facer Street at the appropriate time, perhaps within the next five years. There is no other underground sewer or watermain work required at this time that would accelerate the need to reconstruct Facer Street.

If there is a desire to reconstruct Facer Street in order to improve the aesthetics and provide other streetscape enhancements, it should be noted that the actual construction timing would be pushed off well beyond the next five years in order to maximize the residual life of the road. The need for an EA would be premature at this time and, as such, the Draft 2019 Operating and Capital Budgets do not have an allocation for an EA for Facer Street.



CITY OF
ST. CATHARINES

Memorandum

To: Budget Standing Committee

cc: D. Carnegie; B. Shynal; S. Chemnitz; K. Douglas; M. Kreuk

From: Dan Dillon, P.Eng. – Director-TES

Date: January 6, 2017

Subject: December 19, 2016 – Council Agenda – Item 9.1 - Facer Street Revitalization and Reconstruction Motion Referral

At its meeting of December 19, 2016, City Council referred the following motion to the City Budget Standing Committee for consideration at its first meeting in January 2017:

Facer Street Revitalization and Reconstruction

Councillor Britton will present the following motion:

WHEREAS a sustainable community is one that respects and embraces its heritage to create a sense of identity and pride; and

WHEREAS Policy 4.1 of the Official Plan recognizes that Urban Design principles should be based on sustainable design principles including:

- To help create a strong sense of identity and place*
- To be stimulating, have an attractive and safe public realm, be a compact, walkable, bikeable and a well-connected community*
- To consist of mixed use nodes and corridors*
- To have complete streets of all users*
- To have building design to support street life; and*

WHEREAS within the Facer Street area there has been a renewed interest in improving the street and overall businesses as evidenced by the successful Facer Street European Festival held in August of 2016; and

WHEREAS it is important to seek the public's view on the ultimate vision of how Facer Street should look aesthetically and function practically at the street predesign stage;

THEREFORE BE IT RESOLVED that the City commit \$60,000 from the Civic Project Fund towards seeking public input and develop the City's vision and

design phase of the Facer Street revitalization and reconstruction by the end of the first quarter of 2017.

During discussion of the motion, Council also asked for additional information on the following items:

- Confirmation of expected costs of the consultant
- Alternative funding sources
- Indication of what work would have to be postponed if staff were to complete this study in-house.

Background

Road Condition and Rehabilitation Options/Costs

In our Pavement Management System, Facer Street is currently split into two sections:

- Facer Street: Niagara Street – Augusta Avenue – Current Pavement Condition Rating (PCR) - 56
- Facer Street: Augusta Avenue – Grantham Avenue – Current PCR - 67

Both sections of Facer Street are composed of asphalt on a concrete base and are at or nearing the trigger points where we would recommend resurfacing the street. Given our current level of funding and the condition of other streets throughout the City, the section from Niagara Street to Augusta Avenue would come up for resurfacing within the next couple of years with the section from Augusta Avenue to Grantham Avenue within the next five years. Both sections were last resurfaced in 2005.

Standard resurfacing would include undertaking spot curb repairs, milling and resurfacing the roadway with 50mm of asphalt. At the same time, we would undertake spot sidewalk repairs as required. Road reconstruction would involve the complete removal and replacement of the existing road base, including curbs and sidewalks and repaving the road.

Attached as Appendix 1 is a summary table providing the estimated costs for various rehabilitation strategies, from resurfacing to standard asphalt road reconstruction, concrete road reconstruction, coloured concrete and stamped coloured concrete road reconstruction. Also included as part of that table is the relative cost difference compared to a road resurfacing or standard road reconstruction. While reconstruction with coloured and/or stamped concrete would not normally be considered for the entire length of a road, these options were presented to reflect the premium associated with any enhancements to the aesthetics of the road driving surface.

Streetscaping Enhancements/Costs

Any streetscaping enhancements would be in addition to these costs and are difficult to price as they are very much dependent upon what is being proposed. Enhancements could include wider sidewalks, decorative coloured concrete edging along sidewalks, landscape planters, overhead arches/signs, etc.

Streetlighting Costs

Along the entire length of Facer Street, there are 32 existing hydro poles with cobra head streetlights on them. As part of our LED streetlight conversion project, these lights are being replaced at a cost of approximately \$9,000. If there is a desire for a more decorative street lights along Facer Street (similar to the lights along St Paul Street as an example), the costs would be significantly greater. Decorative street lights are typically installed at a lower height which would involve the installation of new poles (and wiring), on both sides of the street, spaced more closely together. The poles and fixtures themselves are also much more expensive. A very rough estimate to provide decorative streetlighting along the entire length of Facer Street would be approximately \$1.5 Million. This could vary significantly depending upon the type and style of streetlight pole and fixture selected.

Property Acquisition/Utility Relocation Costs

These costs do not include any allowance for property acquisitions or utility relocations, or burying overhead utilities underground. The existing road allowance width along Facer Street varies and is quite narrow for a road that functions as a collector street. If Facer Street is reconstructed and if there is a desire to include other amenities (wider sidewalks, wider boulevards, bike lanes, etc) there may be a need for property acquisitions throughout which are difficult to quantify at this point.

Horizon Utilities is undertaking the replacement of much of their pole and overhead wiring infrastructure in this area this year. Staff have had preliminary discussions with Horizon and the other utilities to determine the cost to bury their overhead wiring – the additional cost to the City for burying the existing overhead wires would be approximately \$2.0 Million.

Construction Timing

As indicated earlier, the road condition on Facer Street is at a point where our Pavement Management System is recommending resurfacing the road within the next 1-5 years. From a purely pavement or asset management perspective, the recommended approach would be to resurface the road at the appropriate time. There are no other underground sewer or watermain works required at this time that would accelerate the need to reconstruct Facer Street.

If there is a desire to reconstruct Facer Street in order to improve the aesthetics and provide other streetscape enhancements, it should be noted that, along with the additional construction costs outlined above, the actual construction timing would be pushed off well beyond the next five years in order to maximize the residual life of the road.

Project Design/Consultant Costs

A road reconstruction project would typically follow the requirements set out in the Municipal Class Environmental Assessment process. As part of this process and

depending upon the nature of the project, consultation would be undertaken with various regulatory agencies, stakeholders and members of the public in order to:

- identify and define the problem,
- consider and evaluate alternatives, and
- identify ways to reduce any potentially unfavorable outcomes

Input from the consultation process would assist staff in weighing the many factors involved with the project and helping to identify the best solution.

The deliverables that would normally be associated with such a design assignment would include a full set of detailed (tender ready) plans and specifications along with an updated cost estimate. Several preliminary concept designs and cost estimates may also be presented throughout the evaluation process. The cost associated with this type of design assignment would typically be approximately 5-7% of the value of the project – assuming a standard asphalt road reconstruction cost of \$1.5 Million, staff would expect the design costs for this project to be approximately \$75-100,000, perhaps higher if additional aesthetic or streetscaping initiatives are being investigated.

The proposed motion refers to the desire to “... seek the public’s view on the ultimate vision of how Facer Street should look aesthetically and function practically at the street predesign stage” and to “...develop the City’s vision and design phase of the Facer Street revitalization and reconstruction”. If the deliverables expected from such a study are comparable to those described above, then the cost estimates indicated above would also be reflective of that.

If the intent of the study is meant to be more of a visioning exercise, then the cost may be somewhat less. As an example, in 2012-2013, the Region of Niagara undertook a Complete Streets Visioning Exercise that looked at complete streets concepts in general and considered a section of Queenston Street from Frank Street to just east of Woodburn Avenue as a pilot project. A link to the final study is included below:

<http://www.niagararegion.ca/living/icp/complete-streets/PDF/complete-streets-visualization.pdf>

The resulting visualizations provided some general concepts of what the street could look like at certain selected locations, however it did not provide an overall plan or cost estimates. The cost for this study, which was completed with the assistance of a consultant, was approximately \$25,000 with the visualization costs being approximately \$5,000-10,000.

Alternative Funding Sources

The proposed motion suggests that funding for the design phase of the Facer Street revitalization and reconstruction would come from the Civic Project Fund. Designs for future road reconstruction projects would typically be funded from the Road and Drainage Improvement Program component of the Capital Budget.

Implications of Completing Study with In-House Staff

Staff are currently working on several road reconstruction projects that were approved as part of previous budgets. These include works on:

Project #	Street	From	To	Status
P13-062	St Paul St	James St	Carlisle St	Design – Construction – 2017
P13-062	Carlisle St	St Paul St	McGuire St	Design – Construction – 2017
P13-066	Queenston St	Page St	Eastchester Ave	Design – Construction – 2017
P14-067	Lorne St	Queenston St	Eastchester Ave	Design – Construction – 2017
P15-065	Lowell Ave	Thomas St	Lake St	Design – Construction – 2017
P15-066	Moffatt St	N. Limit	S. Limit	Design – Construction – 2018
P15-067	Third St Louth	Fourth Ave	Lakeshore Rd	Design – Construction – 2018
P16-062	Churchill St	Chetwood St	Rykert St	Design – Construction – 2018
P16-063	St George St	Facer St	Dieppe Rd	Design – Construction – 2018
P16-064	Ridgewood Rd	Highland Ave	Riverdale Ave	Design – Construction – 2018
P16-065	Church St	King St	Niagara St	Design – Construction – 2018
P16-065	Geneva St	Church St	St Paul St	Design – Construction – 2018
P16-065	Niagara St	Geneva St	Church St	Design – Construction – 2018
P16-065	St Paul St	Court St	Geneva St	Design – Construction – 2018
P16-065	Queenston St	Geneva St	Riordon St	Design – Construction – 2018

These road reconstruction projects are in various stages of preliminary or detailed design with construction anticipated in 2017 or 2018 as noted. All of the above noted projects are being undertaken with the assistance of consultants, however they still occupy a significant component of staff's time in managing the project. These projects do not include any proposed 2017 budgeted works.

There is not the internal staffing capacity in TES to undertake and complete an in-house predesign study of Facer Street by the end of the first quarter of 2017 without further delaying many existing projects. The visioning aspect of any study would also include the involvement of staff from PBS. Given the request to have this study completed by the first quarter of 2017, work on either the Port Dalhousie Secondary Plan or the Infill and Intensification Study would be delayed.

Conclusion

The motion regarding the Facer Street Revitalization and Reconstruction that has been referred to the Budget Standing Committee is asking *"that the City commit \$60,000 from the Civic Project Fund towards seeking public input and develop the City's vision and design phase of the Facer Street revitalization and reconstruction by the end of the first quarter of 2017."*

Implied as part of the motion is that Facer Street be reconstructed. From a purely pavement or asset management perspective, the recommended approach would be to resurface the road at the appropriate time (within the next 1-5 years). If there is a desire

to instead reconstruct Facer Street, the construction costs would be significantly higher and the construction timeframe would extend well beyond the next five years.

If Facer Street is to be reconstructed for the purposes of improving the aesthetics and providing other streetscape enhancements, then a visioning or predesign exercise would be beneficial in better defining the scope of the work prior to proceeding with detailed design. Staff would suggest that this work be prioritized with other existing and proposed works to better align with the anticipated construction timeframe.

P15-085	Queenston St	Queenston St	Design - Construction - 2018
P15-086	St Paul St	Geneva St	Design - Construction - 2018
P15-087	Madame St	Geneva St	Design - Construction - 2018
P15-088	Geneva St	Church St	Design - Construction - 2018
P15-089	Geneva St	Church St	Design - Construction - 2018
P15-090	Geneva St	Church St	Design - Construction - 2018
P15-091	Geneva St	Church St	Design - Construction - 2018
P15-092	Geneva St	Church St	Design - Construction - 2018
P15-093	Geneva St	Church St	Design - Construction - 2018
P15-094	Geneva St	Church St	Design - Construction - 2018
P15-095	Geneva St	Church St	Design - Construction - 2018
P15-096	Geneva St	Church St	Design - Construction - 2018
P15-097	Geneva St	Church St	Design - Construction - 2018
P15-098	Geneva St	Church St	Design - Construction - 2018
P15-099	Geneva St	Church St	Design - Construction - 2018
P15-100	Geneva St	Church St	Design - Construction - 2018

These road reconstruction projects are in various stages of preliminary or detailed design with construction anticipated in 2017 or 2018 as noted. All of the above noted projects are being undertaken with the assistance of consultants, however they still occupy a significant component of staff's time in managing the project. These projects do not include any proposed 2017 budgeted work.

There is not the internal staffing capacity in TES to undertake and complete an in-house predesign study of Facer Street by the end of the first quarter of 2017 without further delaying many existing projects. The visioning aspect of any study would also include the involvement of staff from PBS. Given the request to have this study completed by the first quarter of 2017, work on either the Port Dalhousie Secondary Plan or the Infill and Intensification Study would be delayed.

Conclusion

The motion regarding the Facer Street Revitalization and Reconstruction that has been referred to the Budget Setting Committee is asking that the City commit \$50,000 from the Civic Project Fund towards seeking public input and develop the City's vision and design phase of the Facer Street revitalization and reconstruction by the end of the first quarter of 2017.

implied as part of the motion is that Facer Street be reconstructed. From a purely pavement or asset management perspective, the recommended approach would be to resurface the road at the appropriate time (within the next 1-5 years). If there is a desire

1

**Facer Street
Preliminary Road Costs - 2016 Budget Estimate Costs**

Rehabilitation Strategy	Section		Total	% Premium vs Resurfacing	% Premium vs Asphalt Road Reconstruction
	Niagara St - Augusta Ave	Augusta Ave - Grantham Ave			
Spot curb and sidewalk repairs, asphalt milling and resurfacing	\$236,000	\$150,000	\$386,000	N/A	N/A
Asphalt Road reconstruction including replacement of all sidewalks	\$961,000	\$611,000	\$1,572,000	407%	N/A
Concrete Road reconstruction including replacement of all sidewalks	\$1,410,000	\$896,000	\$2,306,000	597%	147%
Coloured Concrete Road reconstruction including replacement of all sidewalks	\$1,662,000	\$1,056,000	\$2,718,000	704%	173%
Coloured Stamped Concrete Road reconstruction including replacement of all sidewalks	\$2,090,000	\$1,328,000	\$3,418,000	885%	217%

Facer St Area
Preliminary Costing for Various Initiatives
Source: *The Future of Facer Street - Recommendations Report - June 2017 - Page 35*
Prepared by: 8-80 Cities

Improvement	Timeline	Cost		Impact	Project Lead	Comments
Banners	Short-term	\$	+/- \$120/ea	Medium	Community	Banner only - Based on DTA costs
Murals	Short-term	\$	Unknown	Medium	Community	Community/business based initiative
Vinyl Wraps	Short-term	\$	Unknown	Low	Community	Community/business based initiative
Hanging Planters	Short-term	\$-\$	+/- \$250/ea	Medium	Community	City? - Current cost/hanging basket - also consider option thru Green Committee
Seating	Short-term	\$-\$	+/- \$2,200-\$3,500 ea	Medium	City	Proposed 2019 Rates for Memorial Benches
Garbage Cans	Short-term	\$	Nominal	Medium	City	Nominal cost for regular cans - perhaps investigate more decorative installation such as in Port Dalhousie?
Holiday Celebrations	Short-term	\$	Unknown	High	Community	Community/business based initiative
Summer Markets	Short-term	\$	Unknown	Medium	Community	Community/business based initiative
Patios	Short-term	\$-\$	Unknown	Medium	Community	Community/business based initiative
Entrance Feature	Medium-term	\$-\$-\$	+/- \$10,000 to \$100,000-\$250,000 (*)	Medium	City	Could range from "Welcome to Facer District" sign/planting to an archway across the road
Historical Plaques	Medium-term	\$-\$	+/- \$1000/ea	Low	Community	Cost could vary depending upon complexity
Sidewalk Plaques or Medallions	Medium-term	\$	+/- \$1000/ea	Low	Community	Cost could vary depending upon complexity
Cross walks	Medium-term	\$	*	Medium	City	As part of larger reconstruction project
Shade Trees	Medium-term	\$-\$-\$	+/- \$1,000/ea	High	City	Could do indepentantly or as part of larger reconstruction project
Lighting	Medium-term	\$	*	High	Community	Should be City Project Lead? - As part of larger reconstruction project
Facade Grant	Medium-term	\$	Unknown	Medium	City	PBS
Pop-Up Shops	Medium-term	\$	Unknown	Medium	Community	Community/business based initiative
Branding Strategy	Long-term	\$	Unknown	High	Community	Community/business based initiative
Curb bulb outs	Long-term	\$	*	High	City	As part of larger reconstruction project
Sidewalk maintenance	Long-term	\$	**	High	City	On-going maintenance
Street and sidewalk pavers	Long-term	\$	*	High	City	As part of larger reconstruction project

\$ = Up to \$10,000 \$ = Between \$10,000 and \$100,000

\$-\$ = Between \$100,000 and \$1,000,000 \$-\$-\$ = \$1,000,000 and above

Identified as City project lead

* - Costs can vary significantly depending upon complexity - would be incorporated as part of a larger reconstruction project - would initially require a design Environmental Assessment (EA) to scope final design - EA Cost - +/- \$100,000

** - Costs for required sidewalk maintenance included as part of city's operating budget - will vary depending upon necessity



By-laws to be considered Monday, January 28, 2019

- (a) A By-law to authorize a contract with Precise ParkLink Inc. (One reading - with respect to the supply, delivery, installation and support of parking meter equipment. Delegation By-law No. 2004-277, as amended.)
- (b) A By-law to authorize a Data License & Reciprocation Agreement with The Regional Municipality of Niagara. (One reading - with respect to Niagara 9-1-1 Road Network and Water Hydrant data. Delegation By-law No. 2004-277, as amended.)
- (c) A By-law to authorize a Letter of Agreement with Her Majesty the Queen in right of the Province of Ontario, represented by the Minister of Transportation for the Province of Ontario. (One reading - with respect to the Dedicated Gas Tax Funds for Public Transportation Program. Delegation By-law No. 2004-277, as amended.)
- (d) A By-law to amend By-law No. 2013-283 entitled "A By-law to regulate the use of land, the bulk, height, location, erection and use of buildings and structures, the provision of parking spaces and other associated matters in the City of St. Catharines." (One reading – with respect to 10 Benfield Drive and lands located opposite the intersection of Louth Street and Crestcombe Road. To be considered by Council, January 28, 2019.)
- (e) A By-law to Adopt the Capital Budget for the City of St. Catharines for the Year 2019. (One reading – with respect to 2019 Capital Budget. To be considered by Council, January 28, 2019.)
- (f) A By-law to authorize an extension of the Guarantee of a Line of Credit for the Niagara Symphony Association. (One reading - with respect to the Niagara Symphony Association's operating line of credit. To be considered by General Committee, January 28, 2019.)
- (g) A By-law to authorize a Pole Attachment Agreement with Alectra Utilities Corporation. (One reading – with respect to the erection of signage on utility poles. To be considered by General Committee, January 28, 2019.)
- (h) A By-law to re-appoint John Mascarin of the law firm Aird & Berlis LLP as the Integrity Commissioner for The Corporation of the City of St. Catharines. (One reading – with respect to appointment of the Integrity Commissioner. To be considered by General Committee, January 28, 2019.)
- (i) A By-law to amend By-law No. 2004-277 entitled "A By-law to authorize delegation of certain matters to staff." (One reading – with respect to out of province travel. To be considered by General Committee, January 28, 2019.)



- (j) A By-law to confirm the proceedings of council at its meeting held on the 28th day of January, 2019. (One reading - with respect to confirming the proceedings of the meeting held on January 28, 2019.)