



**The Corporation of the City of St. Catharines
CITY COUNCIL AGENDA
Special Public Meeting, Tuesday, December 12, 2017
FirstOntario Performing Arts Centre, 250 St. Paul Street, 6:00 PM**

His Worship Mayor Walter Sendzik takes the Chair and opens the meeting

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- 1. Mayor's Report**
- 2. Adoption of the Agenda as Presented**
- 3. Declarations of Interest**
- 4. Public Meeting Pursuant to Planning Act**

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- 4.1 Planning Act Public Meeting - Information Report
Application for Official Plan Amendment and Zoning By-law Amendment
at 16 Lock Street (12 Lock St., 12 and 12A Lakeport Rd.); Owner: Union
Waterfront Inc.; Agent: John Ariens and Mike Crough, IBI Group

- 5. Adjournment**



CITY OF
ST. CATHARINES

Corporate Report

Report from Planning and Building Services, Planning Services

Date of Report: November 22, 2017

Date of Meeting: December 12, 2017

Report Number: PBS-312-2017

File: 60.30.331 and 60.35.1029

Subject: *Planning Act Public Meeting - Information Report*

Application for Official Plan Amendment and Zoning By-law Amendment at 16 Lock Street (12 Lock St., 12 and 12A Lakeport Rd.);

Owner: Union Waterfront Inc.

Agent: John Ariens and Mike Crough, IBI Group

Recommendation

That Council receive this report for information purposes; and,

That Council consider the application for approval of an Official Plan Amendment and Zoning By-law Amendment at a future meeting of Council pursuant to the 2-step process for planning applications that was approved by Council on May 8, 2017.
FORTHWITH

Summary

This Information Report is to provide Council and the public with information concerning the application for an Official Plan Amendment and Zoning By-law Amendment at 16 Lock Street (also includes 12 Lock St., 12 and 12A Lakeport Rd.) to be considered at a Public Meeting scheduled for December 12, 2017. This Information Report provides an overview of the application, and a summary of applicable planning policies and circulation comments.

The applicant is requesting changes to the site-specific Official Plan Policies and Zoning By-law requirements related to these lands in order to facilitate the construction of a fourteen-storey mixed-use building. The following amendments are proposed:

Official Plan: Retain the Community Commercial designation and amend site-specific policies to reduce the maximum permitted height from 17 storeys to 14 storeys; to remove a theater and a hotel as being required uses on the site, but to retain the hotel as an optional use; and to add service commercial and cultural facility uses as being required uses on the site.

Zoning By-law: Retain the Community Commercial (C2) category and amend the existing Special Provisions and Holding (H) designation that apply to these lands.

The Special Provisions and Holding designation would also be extended to 12 Lock Street, which did not form part of the previous approvals. Changes to the Special Provisions include revised building height, building setbacks, required mix of uses, maximum number of dwelling units, parking requirements, and public open space. Changes to the Holding designation would update the mix and extent of non-residential uses that must be constructed on site prior to residential occupancy.

A Recommendation Report containing a planning analysis, addressing any outstanding concerns raised at the Public Meeting, and providing a staff recommendation will be considered by Council at a future meeting.

Further applications for Site Plan Approval, Draft Plan of Condominium Approval, Heritage Easement Agreement, and Heritage Permits will also be required. Council will also need to consider and approve the transfer of a portion of Hogan's Alley should the applications be approved. The conceptual proposed Site Plan (Appendix 2) is attached for Council's reference.

Background

On May 8th, 2017, Council approved a 2-step process for planning applications requiring a Public Meeting in accordance with the Planning Act. The first step is for an Information Report to be presented to Council on the date of the Public Meeting when public input will be gathered. The second step is for Council to consider a Recommendation Report at a subsequent Council meeting. This report is the Information Report and will be followed by a Recommendation Report at a future Council meeting.

The existing development permissions on the subject lands permitting a seventeen-storey mixed-use complex were approved by order of the Ontario Municipal Board in 2009. The Board approved amendments to the Official Plan and Zoning By-law, along with approval of the site plan agreement and heritage permits for that project. The 2009 Board approval was carried forward into the City's new Official Plan (2012) and new Zoning By-law (2013) and remain in effect.

Report

Proposed Development

The applicant seeks approval for an Official Plan and Zoning By-law amendment for the lands identified in Appendix 1 to permit the construction of a fourteen-storey mixed-use complex that includes 220 residential dwelling units, 2,152 m² (23,161 ft²) of commercial and community floor space, and 258 underground parking spaces. The plans submitted in support of the application identify an ultimate building height of 51.75 metres adjacent to Main Street, with building heights stepping down towards Lock Street and Lakeport Road.

The original existing Austin House building at 16 Lock Street and the former Spice of Life Building at 12 Lock Street are proposed to be retained and integrated into the new

development. Specific uses for these buildings have not yet been identified. The existing Old Jail is proposed to be relocated 10 m northward, adjacent to the site. Ownership and operation of the jail building is proposed to be transferred to the City. The proposal includes three levels of below grade parking, accessed from Main Street, and a central courtyard that is proposed as publicly-accessible open space.

A 219 m² community space is included within the proposed building design adjacent to Lakeside Park and the relocated jail. The applicant has proposed that this space be used by the City as a community facility, including potential uses such as a library, day care, seniors centre or similar use to serve the community.

Hogan's Alley is proposed to be maintained and improved as a pedestrian corridor. The applicant is proposing to purchase a portion of Hogan's Alley (12A Lakeport Road) to construct underground parking beneath these lands, as well as to construct residential dwelling units that would bridge across Hogan's Alley between 16 Lock Street and 12 Lakeport Road. A portion of Hogan's Alley would be widened as a pedestrian connection and the existing steep slope towards Lakeside Park would be replaced by a wide staircase.

The applicant's proposed site plan, landscape plan, building elevation plans, and architectural renderings are attached as Appendices 2-5 respectively.

Location and Site Description

The subject lands are located on the eastern corner of Main Street and Lock Street, in the City's North Planning District. The subject lands also include a portion of Hogan's Alley and a parcel at the northern extent of Lakeport Road (former Port Mansion lands). A location map is attached as Appendix 1.

The entire subject lands are approximately 0.5 hectares in size, with frontage along Lock Street, Main Street, Hogan's Alley, Lakeport Road, and Lakeside Park. The lands are located within the Port Dalhousie Heritage Conservation District. Lock Street is designated as Regional Arterial, whereas the remaining site frontages are local streets. The portions of Main Street and Lakeport Road to the south of Lock Street are also Regional Arterials. The subject lands are served by one transit route, route 301.

Surrounding land uses include:

North:	Lakeside Park;
South:	Commercial uses fronting onto Lock Street;
East:	Commercial uses fronting onto Lakeport Road and the Dalhousie Yacht Club; and
West:	Low-rise residential dwellings.

Supporting Documentation

The applicant has submitted a number of plans and studies in support of the proposed policy amendments, including the following materials:

- Planning Justification Report, prepared by IBI Group, dated June 2, 2017;
- Urban Design Brief, prepared by Kirkor Architects & Planners, dated June 2, 2017
- Heritage Impact Assessment, prepared by GCBA Architects, dated June 2, 2017
- Conservation Plan, prepared by GCBA Architects, dated June 2, 2017
- Transportation Impact Study, prepared by Paradigm Transportation Solutions Ltd., dated June 2017, with addendum in October, 2017;
- Preliminary Geotechnical Feasibility Report, prepared by Terraprobe Inc., dated June 6, 2017
- Functional Servicing and Stormwater Management Report, prepared by The Municipal Infrastructure Group Ltd., dated June 2017; and
- Stage 1 Archeological Resource Assessment, prepared by ASI, dated June 2, 2017;
- Qualitative Pedestrian Level Wind Assessment, prepared by Gradient Wind Engineering Inc., dated June 2, 2017

Planning staff have requested additional information from the applicant, including an assessment of the projected economic effects of the proposal. A summary of the submitted documentation and a brief description of their intended purpose and associated conclusions is included as Appendix 6 to the report. Digital copies of the various documents are also available for viewing on the City's website, at the following address: <http://www.stcatharines.ca/development>.

Planning Policy Context

Provincial and Regional Policy Context

The subject lands are located within the St. Catharines Urban Area, as defined by the Regional Official Plan (ROP). The St. Catharines Urban Area is considered as a Settlement Area by the 2014 Provincial Policy Statement (PPS). The subject lands are identified as Built-Up Area with the 2017 Growth Plan for the Greater Golden Horseshoe (Growth Plan). The ROP, the PPS and the Growth Plan together direct development to take place in urban areas and delineated built-up areas to support intensified development and to build upon existing servicing and infrastructure. Both Regional and Provincial policy place an emphasis on intensification and infill to foster the development of complete communities that have a mix of diverse land uses and housing choices, improved social equity and quality of life, new and expanded access to multiple forms of transportation, the provision of spaces that are vibrant and resilient in their design, and the conservation of built and cultural heritage resources.

The subject lands are located within the Provincially designated Built-up Area. The residential portion of the proposed development is considered as Intensification and would contribute towards the City's annual residential intensification target of 95%, as set by the Regional Official Plan.

Official Plan (Garden City Plan)

The subject lands are designated as Commercial (Appendix 7) by the City's Official Plan, The Garden City Plan (GCP). The North Planning District further identifies the subject lands as being Community Commercial (Appendix 8). The Community Commercial designation of the GCP provides for a range of commercial uses that are primarily intended to provide concentrations of commercial facilities to support day to day and weekly shopping and service needs for the local surrounding community population. Other uses permitted within the Community Commercial designation include institutional, civic, cultural, indoor recreation, entertainment and residential apartment dwelling uses.

Notwithstanding the general policies related to lands designated as Community Commercial, the North District policies of GCP (Section 15.1.1b) provide additional direction with respect to lands within the Port Dalhousie Commercial Core, as follows:

- a) "an appropriate range and mix of medium or higher density residential housing, commercial, employment, institutional and indoor recreation uses are permitted, to support regional based tourist facilities to enhance the economic vitality of the area;
- b) no ground floor residential dwelling units are permitted;
- c) office uses which do not generate a significant amount of pedestrian traffic or require a ground floor location are encouraged to locate on upper floors; and
- d) retail functions are permitted on upper floors only where in conjunction with a ground floor commercial use".

In addition to the above noted policies, the North District policies go on to describe that the subject lands, with the exception of 12 Lock Street (former Spice of Life building) which was not part of the previous OMB approval. These policies state that "a multi-use commercial/residential building is permitted having a 17 storey height limit to a maximum of 62 metres, provided that:

- i) "the residential component is part of a multi-use commercial redevelopment scheme including retail, office, restaurant, hotel and theatre uses, and
- ii) appropriate development standards are established in the related zoning amendment, site plan agreement, heritage easement agreement and any other approval or applicable development agreement, and
- iii) a Holding (H) designation shall be applied to the residential component; and buildings and structures may be erected on the subject lands prior to the removal of the specific Holding (H) designation, however, the residential component shall not be used for residential purposes until such time as the provisions of the zoning by-law are met, including all conditions related to the Holding (H) conditions".

The Official Plan Amendment application seeks to amend the maximum height limit, reducing it to 14 storeys, to a maximum of 56 metres, and to change the mix of uses within the required multi-use development scheme to no longer require the construction

of hotel and theatre uses. Cultural facility uses and service commercial uses are proposed to be added as required components. The hotel use is proposed to be retained as an optional component. The proposed Official Plan Amendment is included as Appendix 9

Section 16.1 of the GCP details specific matters that must be considered when considering applications of an Official Plan Amendment. Every application for amendment is to be evaluated on the basis of the below general considerations and any other considerations that are pertinent to the particular application. These matters will be considered by staff in the future recommendation report to Council.

- i) “the degree of conformity of the proposed amendment to the general intent, purpose and philosophy of the Plan, particularly the vision, planning principles and general policies of the Plan;
- ii) consistency with Provincial and upper tier government plans, policies and legislation;
- iii) the availability and suitability of land already designated for the proposed use, and the need for, and the feasibility of the proposed use;
- iv) the compatibility of the proposal, or the adequacy of proposed mechanisms for achieving compatibility, with adjacent and planned uses;
- v) the potential of the proposal to cause instability within an area intended to remain stable;
- vi) the ability of the City’s infrastructure and public service facilities to accommodate the proposal without costly expansion, upgrading, or required deferral of other planned infrastructure and public service facility improvements in other areas of the City;
- vii) the financial implications, both costs and revenues, to the City; and
- viii) the degree to which approval of the amendment would establish an undesirable precedent”.

Port Dalhousie Secondary Plan and Heritage Conservation District Study

The City is in the process of preparing a Secondary Plan for the commercial core and harbour areas of Port Dalhousie which will include urban design parameters to help identify and shape the character of the area and will address parking and transportation issues. Concurrently, the City has also initiated the preparation of a new Heritage Conservation Sub-District Plan for the non-residential portion of the Port Dalhousie community. A final community workshop was held on October 25th, 2017 to gather input on the recommended preferred approach to these documents. The draft documents are now being finalized and staff anticipate that the plans will be considered by Council in early 2018.

As the Official Plan Amendment and Zoning By-law amendment that are the subject of this report were submitted and deemed to be complete before Council consideration of

the new policy documents, the applications must therefore be evaluated under the existing approved policy framework.

Zoning By-law

Zoning By-law 2013-283 zones the lands Community Commercial, Special Provision 8 and Holding designations 4 and 5 (C2-8-H4, H5) (see Appendix 10). The C2 Zone permits a broad range of commercial uses, including retail stores and restaurants, subject to the applicable zoning provisions. Office, service commercial and cultural facility uses are also permitted, as are apartment dwelling units. Special Provision 8 and the two holding designations do not currently apply to 12 Lock Street as those lands did not form part of the previous development approvals.

Special Provision 8 (Appendix 11) details additional use restrictions on the lands, as well site-specific height, setback, coverage, and parking requirements. The proposed Zoning By-law Amendment (Appendix 12) seeks to change these special provisions to reflect the proposed building design and new proposed mix of uses within the building. Proposed changes to Special Provision 8 are detailed below.

Special Zone Provision	Current Special Zone Requirement	Proposed Special Zone Change
<i>Permitted Uses</i>		
Only the following uses are permitted:	<ul style="list-style-type: none"> • Office • Restaurant • Retail store • A Theatre • Service Commercial • Apartment Dwelling Units over and behind any foregoing use • A residential building up to a maximum of 80 dwellings units and up to 17 storeys, but not to exceed 62 m in height, only in conjunction with the following: <ul style="list-style-type: none"> ○ A minimum of 3,600 m2 of retail, restaurant and office space, and ○ A hotel with a minimum of 70 rooms; and ○ A theatre with a minimum seating capacity of 400 seats. 	<ul style="list-style-type: none"> • Office • Restaurant • Retail Store • Service Commercial • A Hotel to a maximum GFA of 700 m2 • Cultural Facility • Apartment Dwelling Units over and behind any foregoing use • A residential building up to a maximum of 220 dwelling units and up to 14 storeys, but not to exceed 50 m in height to the roof of the highest residential floor, and 56 m in height including a mechanical penthouse to a maximum height of 6 m, only in conjunction with the following: <ul style="list-style-type: none"> ○ A minimum of 2150 m2 GLFA of retail, restaurant, office, service commercial or cultural facility space
<i>Building Height</i>		

Maximum building height, to top of rooftop mechanical	62m, measured from the central courtyard	56m, measured from the central courtyard
Setbacks		
Minimum building setbacks	Refer to setbacks Schedule 3b in Appendix 11	Revised. Refer to setbacks Schedule 3b in Appendix 12
Permitted encroachments into required setbacks	None	Balconies, canopies, awnings, signage and building projections below, at or above grade may encroach into required yards to a maximum of 1.7 m
Coverage and Open Space		
Maximum lot coverage	100% (includes outdoor plaza and public observation terrace)	100% (includes central Plaza at Hogan's Court)
Minimum open space accessible to the public	27% of the total site area, including the outdoor plaza and public observation terrace	24% of the total site area, including the central plaza at Hogan's Court
Exclusive use recreational space	Recreational facilities shall be provided as common facilities for the hotel and residential uses	No requirement for shared recreation facilities.
Publicly-accessible open space	All privately owned open space uses, namely the outdoor plaza and public observation terrace, shall be accessible to the public by easement	All privately owned open space uses, at or below grade, namely the outdoor plaza at Hogan's Court and associated stairs, shall be accessible to the public by easement
Minimum Required Parking		
Condominium dwelling unit	1.75 spaces per dwelling unit	1 space per dwelling unit, and no parking space required for the first 4 dwelling units
Rental dwelling unit	1.5 spaces per dwelling unit	1 space per dwelling unit, and no parking space required for the first 4 dwelling units
Visitor parking	None	0.17 spaces per residential dwelling unit. All residential visitor parking spaces located on the same property shall also operate as parking for permitted retail store, office, restaurant, service commercial and cultural facility uses
Office, retail, restaurant, service commercial and cultural facility uses	No exemption. Standard parking rates apply as follows: Office: 1 space / 28 m ² Retail: 1 space / 20 m ² Restaurant: 1 space / 20 m ²	No minimum parking requirement

	Service commercial: 1 space / 20 m ² Cultural facility: 1 space / 65 m ²	
Definitions		
Height	Height shall mean the vertical distance of a building between grade, as defined, and the highest point of the roof.	Height shall mean the vertical distance of a building between grade, as defined, to roof of highest residential floor, plus up to 6m for mechanical penthouse, including parapets, and excluding projections like ladders, smoke stack, mechanical equipment, etc.

Two Holding provisions apply to the subject lands: H4 and H5.

The H4 Holding provision requires that a site plan agreement be approved by City Council (including public right-of-way easement); that heritage easements, under the Ontario Heritage Act, are finalized to ensure restoration and preservation of heritage buildings within the site; and that required road allowances, lanes and alleys be closed and conveyed. No changes to Holding Provision 4 is requested.

Holding provisions 5 applies only to the residential tower element of the site and the Hold shall be lifted by City Council once the various required non-residential uses and required parking are constructed. The proposed Zoning By-law Amendment seeks to amend this Holding provision to reflect the new mix of uses proposed. The details of the existing and the proposed H5 designation are detailed below.

Current Holding Provision	Proposed Amended Holding Provision
<p>An additional Holding (H) designation shall apply to area 5, as identified on Schedules C 1a and C 1b, (the 17 storey portion of the building) which shall be lifted by City Council once the following additional condition is met:</p> <p><i>Prior to the residential use of the lands ... all commercial uses, including a minimum of 3,600 m² of retail, office and restaurant space, and a hotel with a minimum of 70 rooms, and a minimum 400 seat theatre and 210 underground parking spaces, and required parking for the theatre use..., shall be constructed, and such construction shall include, but not be limited to, complete exterior finishing and interior demising walls of all buildings, and in the case of the theatre, the following interior elements: the circulation and access system (including lobby, stairs, elevators and corridors), the theatre stage,</i></p>	<p>An additional Holding (H) designation shall apply to areas 1-4, as identified on Schedules C 1a to 1k, which shall be lifted by City Council once the following additional condition is met</p> <p><i>Prior to the residential use of the lands, all commercial uses, including a minimum of 2,150 m² of retail, office, restaurant, service commercial and cultural facility uses, and required structured and underground parking spaces, shall be constructed, and such construction shall include, but not be limited to, complete exterior finishing and interior demising walls of all buildings.</i></p>

the platforms onto which theatre seats will be mounted and the backstage theatre support space.	
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The 12 Lock Street property (former Spice of Life building) was not subject to the previous development approvals and is not presently subject to Special Provision 8 or the Holding policies. 12 Lock Street is subject to Special Provision 9, the provisions that apply to the balance of the Port Dalhousie commercial core properties, which includes an 11 m height limit, an exemption for non-residential parking, a restriction on ground floor dwelling units and permission for a hotel use. The site-specific policies that apply to the balance of the proposed development would be extended to include 12 Lock Street. The existing Special Provision 9 designation on 12 Lock Street would be removed as each of the provisions described in that special provision would also be addressed through special provision 8. No increased building height is proposed on the 12 Lock Street property and the existing building is proposed to be retained.

Site Plan Approval

There is an existing site plan agreement approved for the subject lands (with the exception of 12 Lock Street), which was previously approved by the Ontario Municipal Board. The proposed changes to the building and site design, as well as the inclusion of 12 Lock Street, would necessitate a major amendment to this existing approval. An application to amend the existing site plan agreement has not yet been made.

Heritage Easement Agreement

There is an existing Heritage Easement Agreement registered on title of the subject lands which describes and commits the owner to the specific heritage conservation strategy that was contemplated by the existing development approvals. The Heritage Easement secures the restoration and conservation work related to defined heritage buildings, including the Lakeside Hotel (Austin House), Port Mansion (McGrath and Union House Hotels) and the Old Jail. The Heritage Easement Agreement provides the City with a means to better ensure that the heritage protection is implemented substantially in accordance with the approved Heritage Conservation Strategy. This Heritage Easement Agreement will need to be amended to reflect the proposed new conservation strategy, if approved.

Circulation of Applications

The applications were circulated to all relevant departments and agencies in accordance with the Planning Act. No department or agency has objected to the proposed Official Plan and Zoning By-law Amendments. The following feedback has been received:

Regional Municipality of Niagara (Summary)

The complete letter comments from Niagara Region is attached as Appendix 13.

Land Use Policy: The provision of mixed–use development within urban environments is encouraged and supported by Regional and Provincial policy. The Lock Street/Main Street/Lakeport Road area of Port Dalhousie serves as the commercial centre for the community, and is the most appropriate area for commercial and mixed use development to be directed, as demonstrated through its historic function as an informal downtown, and the policy and development direction of the City's Plans and By-laws. The permitted uses for the site appear to be generally in keeping with what the OMB had approved previously, and other policies of the local Official Plan.

Regional Planning and Development Services staff offer no objection to the proposed Official Plan Amendment and Zoning By-law Amendment as the proposed amendments are considered to align with the intent and direction Regional and Provincial policy, represent good urban design and, generally, do not appear to have any significant impacts on Regional infrastructure.

Given the site specific nature of the application, the Official Plan Amendment is exempt from Regional approval in accordance with the Memorandum of Understanding with Area Municipalities and Regional Official Plan Policy 14.E.7.

Archeology: The applicant has submitted a Stage 1 Archaeological Assessment. The Consultant is of the opinion that the lands have a low cultural heritage value and has not recommended any further study. The inclusion of standard warning clauses regarding deeply buried archaeological resources and/or human remains will be required as conditions of Site Plan approval.

Wastewater: The Lakeside Sewage Pumping Station has a current capacity of 95 L/sec. The proposed development has a calculated a peak flow of 5.25 L/sec. Based on the current flows and predicted flow, the Regional Sewage Pumping Station will have sufficient capacity to service the development. During the site plan stage the Region will request that warning clauses be added acknowledging the potential for regular operating noise and odour from the pumping station, which is located in lakeside Park, near the jail.

Transportation: The Traffic Impacts Study notes that the Regional intersection of Main Street and Lock Street may experience delays during summer peaks and currently there is no suggestion for improvements at this time. In previous reports there was a warrant for a left turn lane at Lock Street and Lakeport Road but this change is no longer recommended in the current study. Since this proposed development has the potential to create warrant for operational changes, Regional staff will request a condition to the site plan agreement (at a later date) to have the developer confirm the predicted traffic patterns and that the existing intersection configurations can accommodate the proposed traffic once the development has reached substantial occupancy.

Urban Design: The proposed development is 3 storeys shorter than the previous design. However, the lesser height of the proposed development is offset by a substantial increase to the bulk of the building. Where the previous development

comprised a traditional podium and tower configuration, the proposed comprises a podium and longer centre slab block with terracing wings. This increased bulk of the proposed will have a greater visual impact on the surrounding context, in the sense that there is more building mass at heights above 3 storeys than the former design. Notwithstanding, the overall design of the proposed building is substantially more attractive than the previous design. A more attractive building design composed of appropriate materials, treatments, gradation, and terracing can lessen the visual impact of the proposed building by making it pleasing to the eye and diminishing the contrast between existing and new. The proposed building is larger than anything in its vicinity. However, the proposed building successfully employs a number of architectural design measures that seek to achieve visual harmony within its existing and planned context.

From an urban design perspective the design of the podium level represents a good approach to dealing with the existing features of the site including existing buildings, spaces, and the grading challenges. The proposal provides active uses at grade along almost the entire perimeter of the building which can help enliven the streetscapes that interface with the development. The courtyard space within the development provides an extension of the public realm into the site making this space an important public feature of the design.

Along Regional Road 87 (Lock Street), the boulevard between the road curb and the proposed building is limited in width. To create the appearance of a wider sidewalk and improve the overall appearance of the proposal and streetscape, Regional staff suggests that the existing overhead utilities be buried, and the existing sidewalks along Lock Street be widened. Should this development be approved, Regional staff will request a streetscape landscape plan as part of any future site plan application.

Niagara Peninsula Conservation Authority (NPCA)

Through preconsultation, the NPCA advised that their mapping identifies the property as being impacted by shoreline hazards associated with Lake Ontario and that upon further review, NPCA staff are satisfied that the subject property is setback from the Lake Ontario shoreline, outside of the erosion hazard, and not impacted by any NPCA regulated features.

Regional Core Natural Heritage mapping identifies an Environmental Conservation Area (ECA) for Valley Shoreline adjacent to (within 50 metres) the subject property. NPCA staff are satisfied that the subject property is separated from the ECA feature by Lakeport Road and existing structures to the southeast. As such, development on the subject property will have no significant negative impact on the identified feature.

The NPCA has no stormwater management requirements for the subject proposal. Based on the above, the NPCA has no concerns with regards to the proposed mixed use development for 16 Lock Street.

Parks, Recreation and Community Services (PRCS)

PRCS staff have advised that City ownership/operation of the proposed community room will require further discussion to identify a possible end use and to address associated terms and conditions.

Additional design and construction matters will need to be address through the future site plan application, including maintaining or increasing the number of trees along Lock Street; landscaping design within the courtyard and streetscapes; improvements and maintenance responsibilities related the Department of Fisheries and Oceans' (DFO) lands around the relocated jail. A construction/staging plan will also be required to ensure that construction does not impact the public's access to Lakeside Park and the Lakeside Park parking lot.

Transportation and Environmental Services (TES) - Traffic Division

TES staff have no concerns with respect to traffic impact on local streets and note that potentially-impacted intersections are under Regional Jurisdiction. Staff advise that a two-way conversion of traffic on Main Street could help to ease traffic on Lock Street by reducing left turns at Lock and Lakeport and have recommended that the applicant assess of the impact of this potential change.

TES staff have identified some concerns with sightlines for vehicles exiting the parking structure and being able to see pedestrians on the sidewalk. The parking ramp will need to be reconfigured to include a flat platform before the sidewalk, with unobstructed sight lines.

With respect to parking, TES staff noted concerns with the applicant's justification for the extent of parking reduction requested. Staff note that Port Dalhousie is not considered to be transit hub and that, while not explicitly stated, the existing 1.75 space per unit standard was intended to include a visitor parking compliment. Staff have less concern with transitory parking (visitors) utilizing public parking facilities but that it can be more problematic if the streets/lots are used by residents to meet their primary parking needs.

Planning and Building Services (PBS) – Development Division

Streets: Main Street and Lakeport Road are designated as a Local roads in the City's Official Plan. The current widths of Main Street and Lakeport Road are sufficient and no additional widenings will be required. Any boulevard and road improvements required will be at the discretion of the City and will be reviewed through the Site Plan Agreement process.

Wastewater: A Functional Servicing Report was submitted by the applicant. Staff are satisfied that the proposed development's peak post-development sanitary flows shall be accommodated within the Region's pumping station. There are no dry-weather flow issues related to the proposed increased dry weather effluent flows proposed from the development with respect to the City's CSO storage tank on Lakeport Road.

Stormwater: There is no existing storm sewers fronting this property along all streets. Historic flows that have been generated from rain events have long been conveyed

overland towards Lakeside Park and into the harbor. Stormwater flows from the development are proposed to be controlled to pre-development flows up to the 5-year event, whereas post-development flows in excess of the pre-development 5-year event up to the 100-year event are to be controlled by an on-site underground storage tank. Staff have requested that overland flows toward Lakeside Park be dispersed rather than concentrated.

Hogan's Alley: The southerly portion of Hogan's Alley will need to be held by the Municipality, since the adjacent properties fronting Lakeport Road are currently serviced through this section (water & utilities). Also, this alley provides access to the rear of these properties via pedestrian and vehicle access/egress. Detailed plans indicating how the foundations would be constructed and the lands re-graded without affecting the existing services and adjacent buildings shall be provided for review and comment as part of the site plan review.

Any proposed re-grading of Hogan's Alley must take into consideration and maintain the required depth of existing services, as well as utility plant infrastructure, within the road allowance and must maintain access to all adjacent properties. Detailed plans indicating how the underground parking and the re-grading scheme of the retained portion of Hogan's Alley will be required. The detailed design and approvals process for these works could impact the design and extent of the underground parking structure, thereby impacting the total number of parking spaces.

Geotechnical: A preliminary geotechnical investigation report has been prepared in support of the application. The study found that foundations for the proposed development are considered feasible. This study recommends that a further detailed geotechnical report be completed during the detailed design stages of this development. This will be further addressed through site plan.

Fire Services

Fire Services has requested additional information on specific matters to confirm firefighting requirements for the project and to confirm if Hogan's Alley will need to be designed to function as a fire access route for the proposed development.

St. Catharines Heritage Advisory Committee

The City's Heritage Advisory Committee will be asked to provide comment on the proposed new development and conservation strategy following receipt the peer review for the Heritage Impact Assessment and Conservation Plan by the City. Staff anticipate that the proposal will be considered at the Committee's January meeting.

Alectra Utilities

Alectra Utilities has advised that a minimum 4m horizontal clearance must be maintained from the existing overhead lines and that the relocation, modification, or removal of any existing hydro facilities will be at the owner's expense. The developer will be responsible for the cost of civil work associated with duct structures, transformer

foundations, and all related distribution equipment. Transformer vault access and locations will require approval by Alectra Utilities.

Enbridge Gas

Enbridge has advised that they have no concerns.

Public Open House

A public open house was hosted by Planning and Building Services on September 26th, 2017 at the Port Dalhousie Lions Club. The purpose of the open house was to share information with the community regarding the proposed development applications consisting of requested Official Plan and Zoning By-law amendments and to gather feedback from those in attendance. The applicant and members of their consulting team were in attendance to answer questions. The open house was well attended, with 260 members of the public completing the sign-in sheet.

Prior to the City's open house, the applicant had hosted an open house event at the Royal Canadian Legion on August 27th, 2017, to provide the community with an additional opportunity to gather information about the proposal and to provide the applicant with first impressions.

Following the City's open house, staff have also received correspondence from several members of the community in response to the applications. Copies of this correspondence is attached as Appendix 15. Concerns raised at the open house and through follow-up correspondence are summarized in Appendix 14 and are grouped by theme, with corresponding staff responses, where available at this time.

The most frequently cited matters of concern related to the application centered on the issues of traffic impacts, parking impacts, the appropriateness of the proposed building height and design, heritage conservation, the proposed residential density, the proposed removal of the theatre and hotel elements, and servicing capacity.

Staff also received numerous letters of support for the application, which identify, among other matters: that the proposal is appropriate and desirable for the site, that the development is necessary to help stabilize and enliven the commercial core; that the building design is attractive, that the heritage conservation approach is desirable, that traffic volumes are rarely an issue, that the proposed courtyard space and Hogan's Alley improvement are supported; and that development of the lands is overdue and needs to proceed.

Public Notice

In accordance with established procedures, notice for the public meeting have been circulated.

Financial Implications

All costs associated with this development will be borne by the developer. No direct financial impacts to the City are triggered by these approvals. If approved and constructed, additional tax revenues would be generated by the development. Proposed City operation of the community room and jail buildings may have capital and operational cost implications and these elements will require further consideration.

Relationship to Strategic Plan

The proposed development has the potential to tie into several aspects of the City's Strategic Plan. These related goals and actions are listed below. The degree to which the below goals of the Strategic Plan will be achieved will be evaluated as part of the future recommendation report.

Economic Sustainability

- Goal: Attract public and private investment, support local businesses and provide excellent customer service to demonstrate we are open for business
 - Action: Prioritize redevelopment initiatives consistent with provincial planning legislation and the City's Official Plan to intensify mixed-use residential developments and ultimately enhance the property tax base and support job creation.
 - Action: Focus on the redevelopment of the commercial core of Port Dalhousie, the General Motors and Hotel Dieu sites on Ontario Street, and the former General Hospital site on Queenston Street, setting target dates for redevelopment for each project with quarterly updates to Council.

Social Sustainability

- Goal: Strive for the highest quality of life for all citizens.
- Goal: Connect people, places and neighbourhoods

Cultural Sustainability

- Goal: Embrace our diversity and celebrate our heritage and history
- Goal: Support cultural festivals and events that build civic pride, encourage local engagement and attract people to the community.

Conclusion

In summary, this report provides the planning context and explains the applications for Official Plan Amendment and Zoning By-law Amendment for 16 Lock Street (12 Lock St., 12 and 12A Lakeport Rd.). The amendments would facilitate the construction of a different building form and mix of uses on the subject lands than was previously approved. A Recommendation Report will be presented to Council at a future meeting, to be scheduled.

Notification

It is in order to advise John Ariens and Mike Crough, IBI Group, 200-360 James Street North, Hamilton, ON, L8L 1H5, the owner's agent.

Prepared by:

Scott Ritchie, MCIP RPP
Urban Design Planner

Submitted by:

Judy Pihach, MCIP, RPP
Manager, Planning Services

Approved by:


James N. Riddell, M.PL., MCIP, RPP
Director, Planning and Building Services

List of Appendices

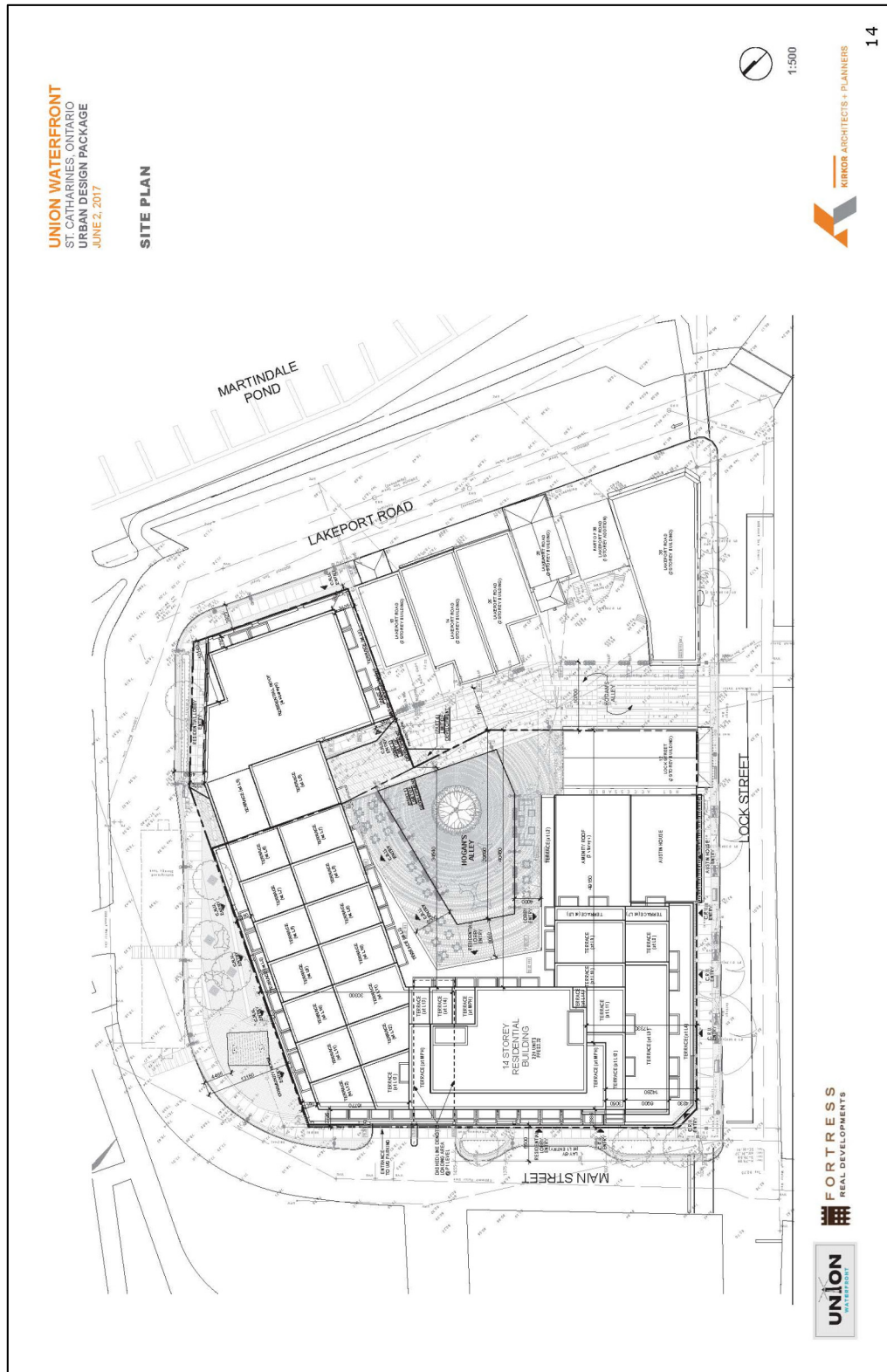
1. Location Map
2. Proposed Site Plan
3. Proposed Landscape Plan
4. Proposed Building Elevations
5. Architectural Renderings of Proposed Development
6. Summary of Supporting Documentation
7. Official Plan – Schedule D1, General Land Use Plan Map
8. Official Plan – Schedule E1, North Planning District
9. Proposed Official Plan Amendment
10. Zoning By-law 2013-283, Schedule A7
11. Zoning By-law, Existing Special Provision 8
12. Zoning By-law, Proposed Special Provision 8
13. Letter of Comments from Niagara Region
14. Community Correspondence Received to December 1st, 2017
15. Summary of Community Concerns

Appendix 1 Location Map



 **Subject Lands**
16 Lock Street (12 Lock Street;
12 & 12A Lakeport Road)
File: 60.30.331 & 60.35.1029

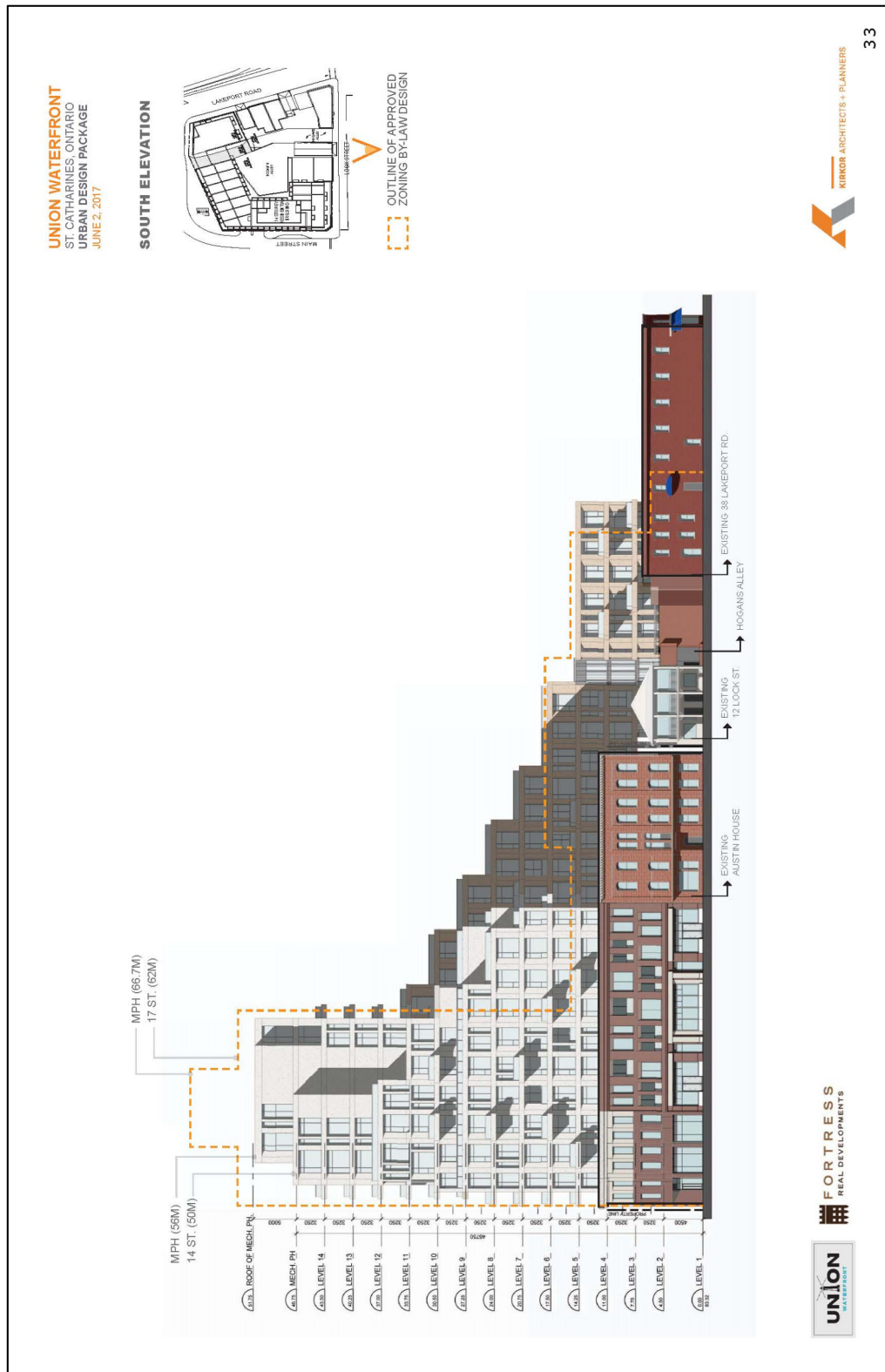
Appendix 2 Proposed Site Plan



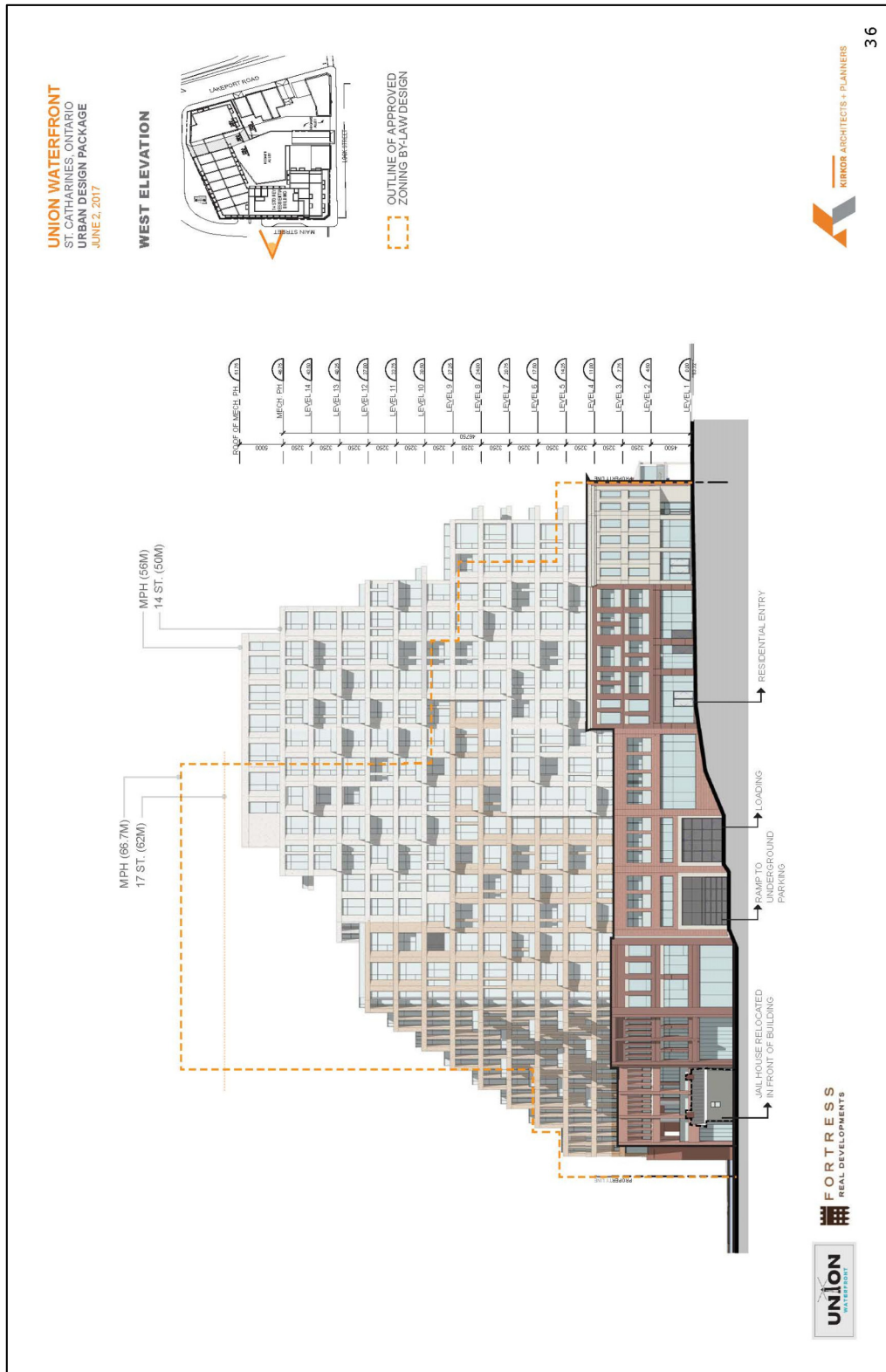
Appendix 3 Proposed Landscape Plan



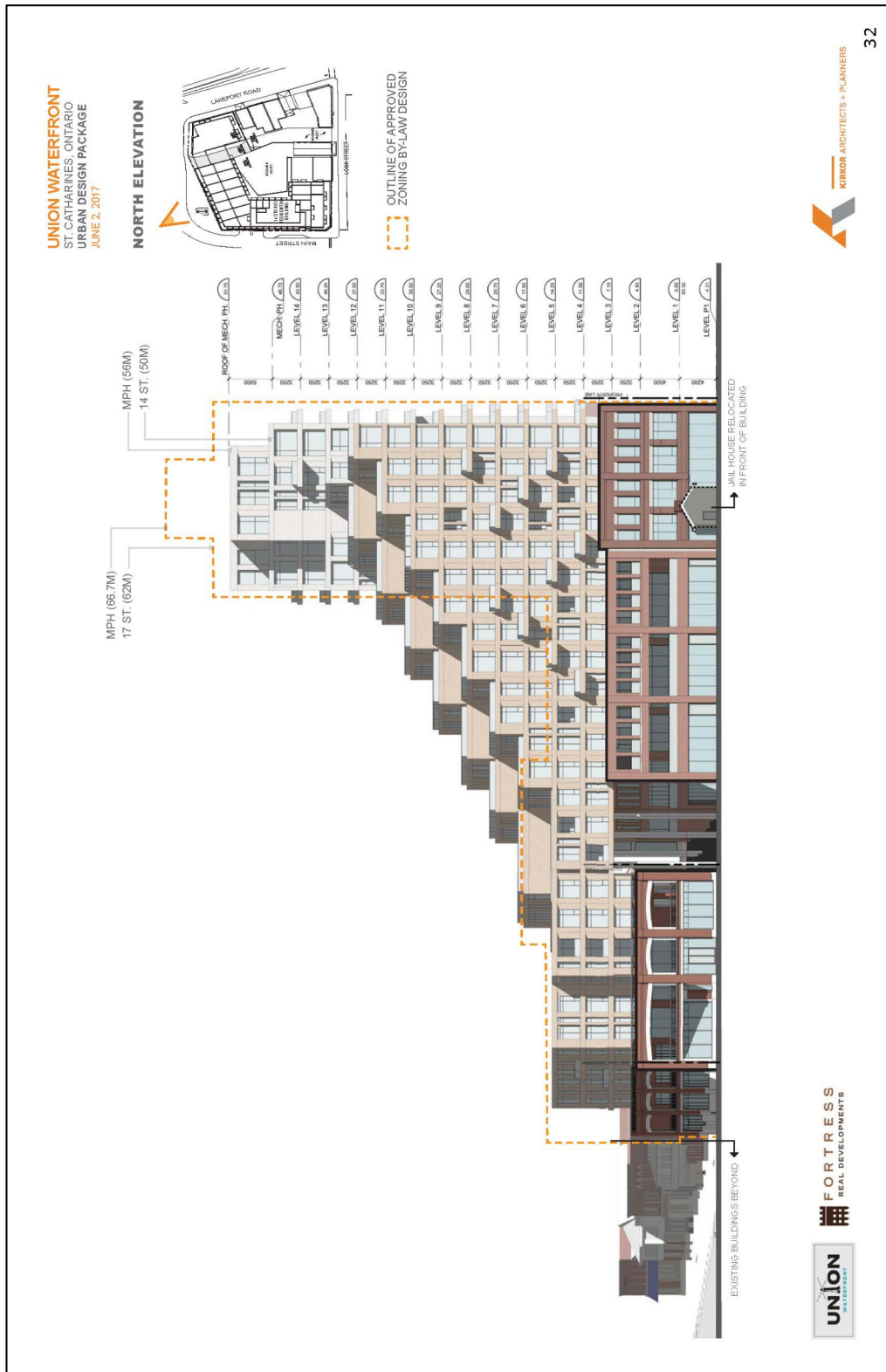
Appendix 4 Proposed Lock Street Building Elevation



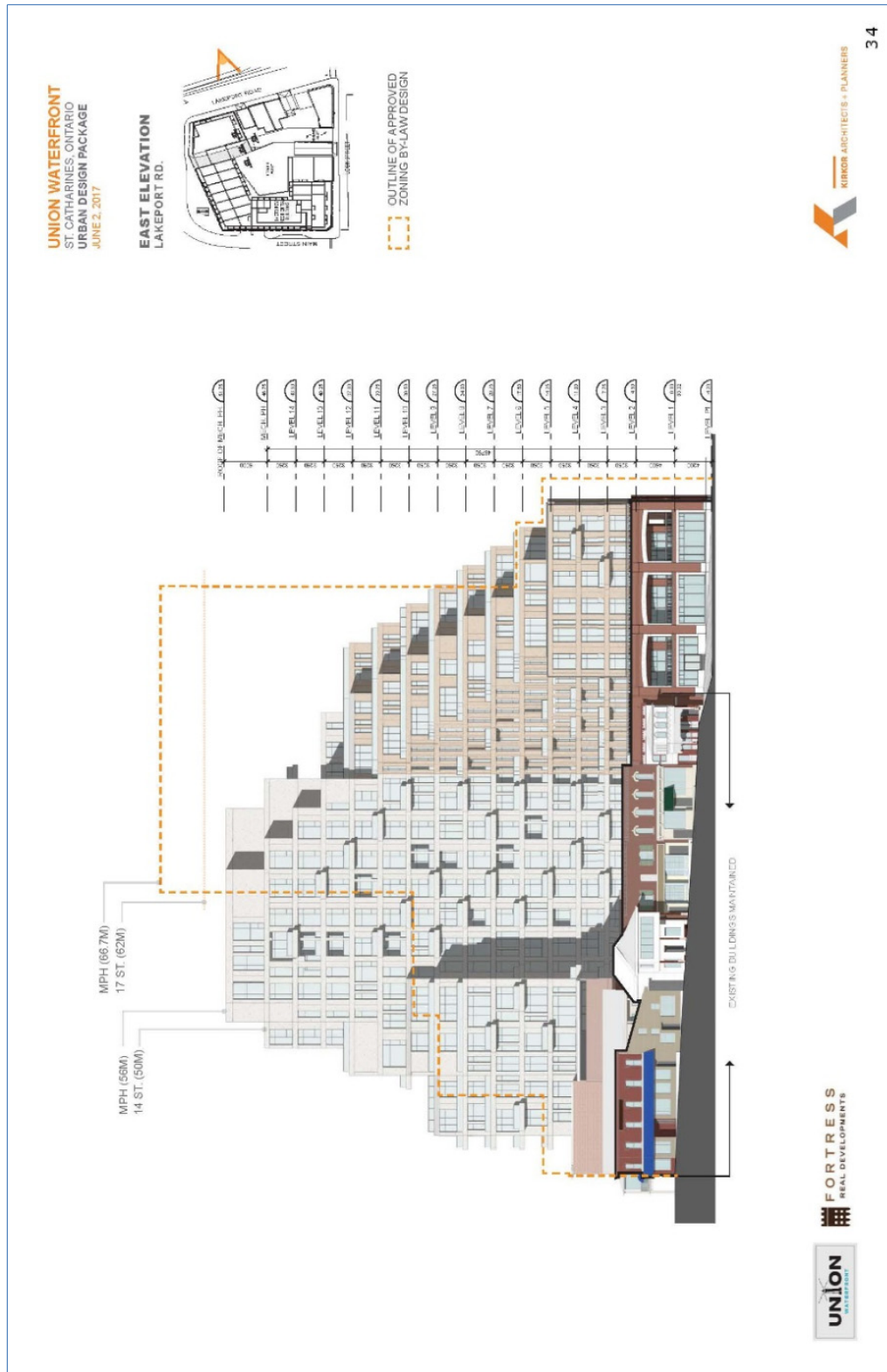
Appendix 4 Proposed Main Street Building Elevation



Appendix 4 Proposed Lakeside Park Street Building Elevation



Appendix 4 Proposed Lakeport Road Street Building Elevation



Appendix 5 Architectural Renderings of Proposed Development



View from Lock Street at Hogan's Alley

Appendix 5 Architectural Renderings of Proposed Development



View from North along Harbour Walk

Appendix 5 Architectural Renderings of Proposed Development



View from Central Courtyard

Appendix 6

Summary of Supporting Documentation

Documentation	Description
Planning Justification Report, prepared by IBI Group, dated June 2, 2017	<p>The Planning Justification Report provides an overview of the existing planning status of the subject lands and a review and evaluation of the relevant Provincial and local policies that relate to the application. The Report details that applicant's review of the Provincial Policy Statement (2014), the Growth Plan for the Greater Golden Horseshoe (June 2013 Consolidation), the Garden City Plan, City of St. Catharines Urban Design Guidelines, the Port Dalhousie Secondary Plan Study and Heritage Conservation Guidelines update, Zoning By-law 2013-283, the context of the surrounding area and general appropriateness of the proposal from a planning perspective. The report also provides an overview of the previous Ontario Municipal Board (OMB) Decision to permit high density mixed use development on the subject lands.</p> <p>The Report concludes that the proposal:</p> <ul style="list-style-type: none"> • is consistent with and conforms to Provincial policy; • supports residential, housing and economic development objectives of the Region of Niagara; • supports Garden City Plan objectives through offering additional housing choice throughout the City and expanding the residential and employment population within the Port Dalhousie commercial area; • encourages the retention of identified cultural heritage assets through re-use and incorporation; • animates the street frontage through providing new pedestrian linkages, revitalizing Hogan's Alley, proposing a new central plaza, and offering new community amenity space; • maintains the general use permissions and policies currently in place for the subject lands and represents a viable development option for development on the subject lands; • applies to lands suitable for the proposed uses; • constitutes good land use planning; and • is in the public interest.
Urban Design Brief, prepared by Kirkor Architects & Planners, dated June 2, 2017	<p>An Urban Design Brief was prepared to detail urban design features of the proposed development and to outline the potential impacts, including shadowing, on adjacent properties. The Urban Design Brief has used the City's Downtown Urban Design Guidelines for reference in addressing matters such as setbacks, massing, building design and streetscape design. The Brief seeks to demonstrate how the proposed development addresses key urban design considerations.</p> <p>Shadow impacts from the proposed building design are expected to minimize shadow casting of the building onto surrounding properties, as well as inside Hogan's Alley. Shadow impacts are largely similar between those of the existing height permissions and those of the</p>

	current proposal, with some variation along Lock Street and within Lakeside Park.
Heritage Impact Assessment, prepared by GCBA Architects, dated June 2, 2017	<p>The Heritage Impact Assessment (HIA) assesses heritage impacts from the proposed development with respect to the Port Dalhousie Heritage Conservation District (PDHCD). All buildings on the site, as well as those in the immediate context, are designated under Part V of the Ontario Heritage Act.</p> <p>The HIA describes the heritage attributes to the Austin House Hotel and Old Jail and provides an evaluation of the development proposal with respect to its impact on the conservation and reuse of the buildings, as well as on the surrounding streetscape. The HIA supports the proposed project design and the proposal to relocate the jail building. The HIA notes that the current vacancy of the former Port Mansion site provides an opportunity to develop a new and sensitive building addition to the property, rather than a reconstruction replicating a historical appearance. The HIA suggests that the new building's impact will be positive.</p> <p>The report concludes by noting that the proposed development represents good planning by incorporating a new development on a site that contains multiple heritage resources. Introducing new scale, typology and massing into an existing heritage setting will inevitably change the established context, yet can be accomplished in a manner that is respectful of and complementary to the heritage character. No significant impacts are foreseen to the built heritage resources on site and the project will strike an appropriate balance between various planning and heritage policies in a manner that will preserve the existing character adjacent to the site and provide an example of the site's commercial evolution into the 21st century.</p> <p>The HIA does not address the existing building at 12 Lock Street (former Spice of Life building). City staff have initiated a peer review of the Heritage Impact Assessment.</p>
Conservation Plan, prepared by GCBA Architects, dated June 2, 2017	<p>The Conservation Plan for the proposed development details how specific cultural heritage resources will be conserved, specifically the Austin House and the Old Jail. The Austin House will be preserved and rehabilitated as a mixed-use space, with the existing rear addition removed. Some interior and exterior restoration work is proposed. The Old Jail is proposed to be preserved and rehabilitated as an interpretive centre. The roof is proposed to be restored to a more appropriate finish and the building is proposed to be relocated approximately 10m north of its current site. The proposed relocation will provide for the required site excavation work, will reduce the impacts and risks arising from construction, and will also offer visual strength from the public realm.</p> <p>City staff have initiated a peer review of the Conservation Plan.</p>
Transportation Impact Study,	The Transportation Impact Study provides a review the existing road and transportation networks, assess existing and future traffic

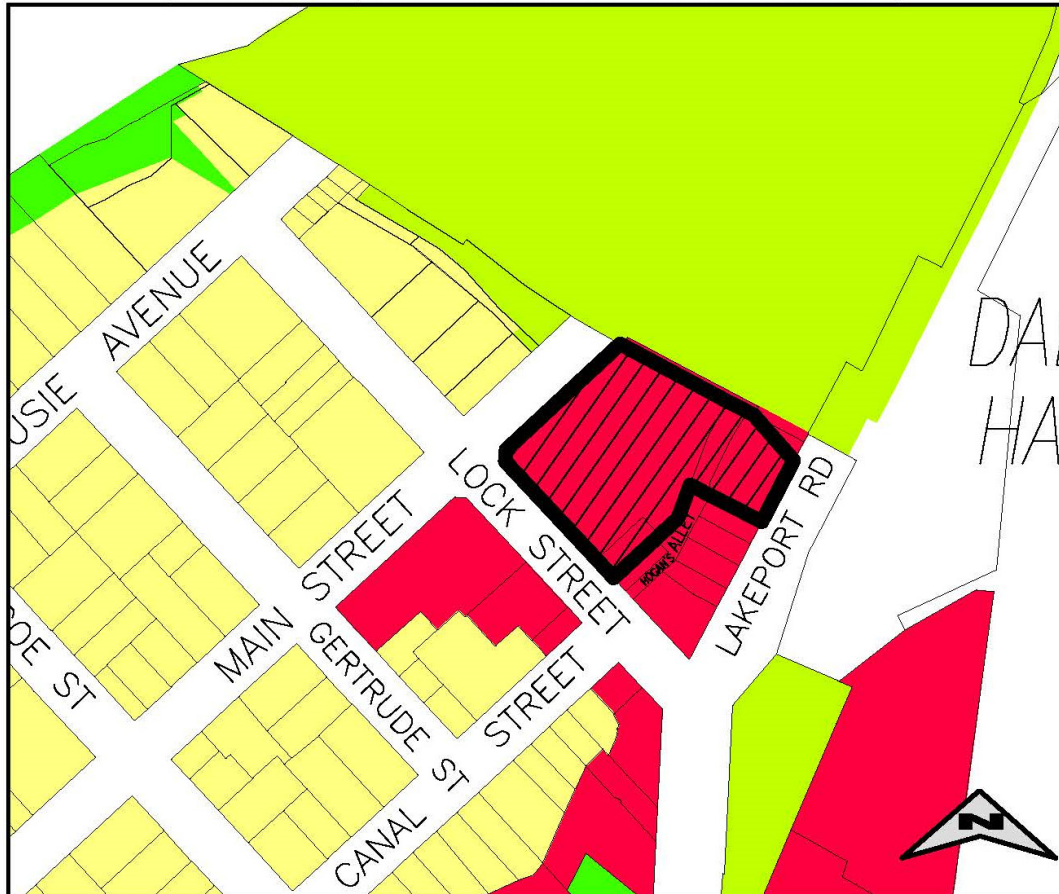
<p>prepared by Paradigm Transportation Solutions Ltd., dated June 2017</p>	<p>conditions and assess parking requirements resulting from the proposed redevelopment of the subject lands.</p> <p>Under projected future (2022) background traffic conditions, traffic movements are forecast to continue operating within capacity. The proposed development is forecast to generate 119 trips during the AM peak hour, 218 trips during the PM peak hour, and 260 trips during the Saturday peak hour. During the Saturday peak hour for summer months, poor operations are forecast at the all-way stop controlled intersection of Lock Street and Main Street. Notwithstanding the Saturday analysis results, no intersection capacity improvements are recommended as widening or signaling this intersection is not considered to be feasible. The consultant suggests that the poor operating conditions are only seen to occur on a Saturday peak hour summer condition, which represents a relatively small number of hours during the year. The report notes that the addition of traffic generated by the subject development would not result in significantly different conditions during the summer weekday AM and PM peak hours.</p> <p>An addendum to the TIS was submitted on October 13th, 2017 to update the study based on new traffic counts that were obtained between August 12th and August 19th, 2017. The new actual summer traffic counts were generally lower than those predicted in the original study. The addendum also studied parking utilization in and around the commercial core.</p> <p>With respect to parking, the report suggests that a reduction in the residential parking requirement from 1.75 spaces per unit is appropriate given that the City's 2013 comprehensive Zoning By-law set a new parking requirement for apartment dwelling units within mix-use buildings (1.0 spaces per unit) elsewhere in the City. The report also suggests that the reduced residential parking ratio is intended to contribute to the higher use of non-auto transportation modes by future residents. Visitor parking at a rate of 0.17 spaces per unit is proposed to be provided. The parking assessment concludes that the proposed on-site automobile parking should be adequate for the subject development.</p>
<p>Functional Servicing and Stormwater Management Report, prepared by The Municipal Infrastructure Group Ltd., dated June 2017</p>	<p>The Functional Servicing and Stormwater Management Report provides information on proposed development conditions and details related to grading and municipal services for the development.</p> <p>The report details the proposed approach to stormwater management. As there is no storm sewer infrastructure in the vicinity of the site, the proposed Stormwater management design includes the construction of a holding tank to control the rate of run-off, which will continue to discharge overland to the harbour.</p> <p>With respect to sanitary services, the report identifies that there is sufficient capacity in the Lakeside Park pump station to accommodate the anticipated additional flow during dry weather conditions. It is estimated that storm events larger than the 2-year storm may exceed</p>

	<p>the capacity of the pumping station which may result in overflow drainage discharging into the harbour. However, this is an existing condition and the additional sanitary flow is expected to have a minimal impact. The report notes that there are potentially direct combined service connections discharging storm drainage from the existing site buildings into the combined sewer. These flows to the combined sewer would be removed in post development conditions, potentially providing additional capacity within the sewer and pumping station. The report findings determine that the existing municipal infrastructure can accommodate the flows anticipated from the proposed development without the need for external upgrades or retrofits.</p> <p>The water service connection for the project will be connected into the existing watermain stubs at the northwest side of the property. Under proposed conditions the development is anticipated to have a maximum required fire flow demand of 339 L/s and the anticipated domestic flow under proposed conditions was calculated at 6.37 L/s. It is expected that the 200mm diameter watermain within Main Street will more than adequately provide the required flow rate.</p>
Preliminary Geotechnical Feasibility Report, prepared by Terraprobe Inc., dated June 6, 2017	Based on data obtained from previous site investigations, the Preliminary Geotechnical Feasibility Report concludes that the proposed development is feasible from a geotechnical engineering perspective. Foundations for the proposed development are considered feasible from a geotechnical design perspective. The report details specific recommendation related to the construction foundations and the parking garage, which will sit below the ground water table.
Stage 1 Archeological Resource Assessment, prepared by ASI, dated June 2, 2017	The Archaeological Resource Assessment concludes that there is no potential for the presence of significant precontact or Euro-Canadian archaeological resources that may be impacted by site preparation of construction activities associated with the proposed development.
Qualitative Pedestrian Level Wind Assessment, prepared by Gradient Wind Engineering Inc., dated June 2, 2017	<p>The purpose of the Wind Assessment is to identify building elements which may adversely impact pedestrian activities and comfort within the site and surrounding area. The study recommends some mitigating features to lessen the impacts of occasional wind gusts along the west side of the north elevation and along the east side of the central plaza space in the form of vertical wind barriers or coniferous plantings. Mitigating measures are also recommended along Lock and Main Streets in the form of recessed building entrances and/or the use of vestibules. Wind impacts may result in the Lock Street and Main Street sidewalk areas being uncomfortable for sitting activates at times.</p> <p>The report suggests that the proposed development is not expected to significantly influence pedestrian wind comfort over neighbouring areas at grade.</p>

Appendix 7 Official Plan – Schedule D1, General Land Use Plan Map

Existing Land Use Designation

(The Garden City Plan - General Land Use Plan, Schedule D1)



Subject Lands

16 Lock Street (12 Lock Street;
12 & 12A Lakeport Road)

Land Use Designations

Neighbourhood Residential
Commercial

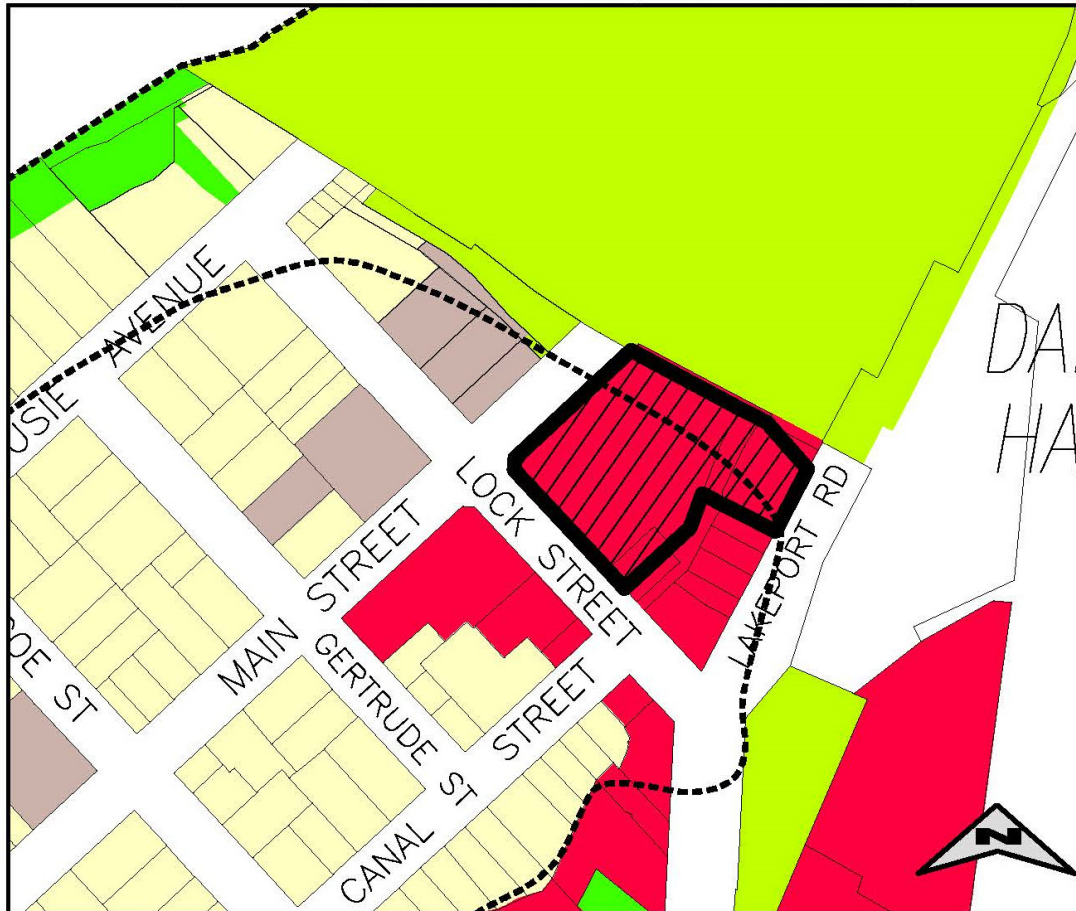
Natural Areas
Parkland & Open Space

File: 60.30.331 & 60.35.1029

Appendix 8 **Official Plan – Schedule E1, North Planning District**

Existing Land Use Designation

(The Garden City Plan - North Planning District, Schedule E1)



Subject Lands

16 Lock Street (12 Lock Street;
 12 & 12A Lakeport Road)

Land Use Designations

Low Density Residential
 Medium Density Residential
 Community Commercial

Parkland & Open Space
 Natural Areas
 Natural Area Extent Line

File: 60.30.331 & 60.35.1029

Appendix 9 Proposed Official Plan Amendment

Garden City Official Plan Amendment No

The following text constitutes Official Plan Amendment No. ## to the Garden City Plan, being the Official Plan for the City of St Catharines.

1. Purpose and Effect

The purpose and effect of the amendment is to modify the existing site-specific policies within Subsection 15.1 of the Plan, being the provisions that apply to lands designated as Community Commercial located west of Lakeport Road and North of Lock Street. The amendment will facilitate redevelopment of the subject lands for a mixed-use building consisting of a 14 storey, 50 m high residential apartment building (56 m including mechanical penthouse), together with a maximum of 2150 m² of commercial and cultural use space with associated structured underground parking. The amendment to the existing site-specific policies is required to permit the proposed development.

2. Basis

The basis for permitting the amendment is as follows:

1. The proposed Amendment is consistent with the Provincial Policy Statement and Conforms to the Growth Plan for the Greater Golden Horseshoe as it will facilitate residential and commercial intensification; will efficiently use existing services; and provide a range and mix of uses that will contribute to and support the vitality of the area.
2. The proposed Amendment is consistent with the goals, objectives and policies of the Garden City Plan and the site-specific policies

3. Map Changes

None

4. Text Changes

Subsection 15.1 is modified in the following way:

15.1. North District

1. Schedule E1

b)

- ii) In addition to Section 15.1..1 b) i) above, the following applies to the lands designated Community Commercial west of Lakeport Road, north of Lock Street, subject to Official Plan Amendment and Zoning By-law Amendment applications ## and ## and Council Decision ##### on #####.
 - a) A multi-use commercial/residential building is permitted, having a 14-storey height limit to a maximum of 50 meters (56 m including mechanical penthouse), provided that:
 - i) The residential component is part of a multi-use commercial redevelopment scheme including retail, office, restaurant, service commercial and cultural facility uses, and which may also include a hotel
 - ii) Appropriate development standards are established in the related zoning amendment, site plan agreement, heritage easement agreement and any other approval or applicable development agreement; and
 - iii) A Holding (H) designation shall be applied to the residential component; and buildings and structures may be erected on the subject lands prior to the removal of the specific Holding (H) designation, however, the residential component shall not be used for residential purposes until such time as the provisions of the zoning by-law are met, including all conditions related to the Holding (H) conditions

5. Implementation

An implementing Zoning By-law Amendment will give effect to the intended uses, built form and parking requirements on the subject lands.

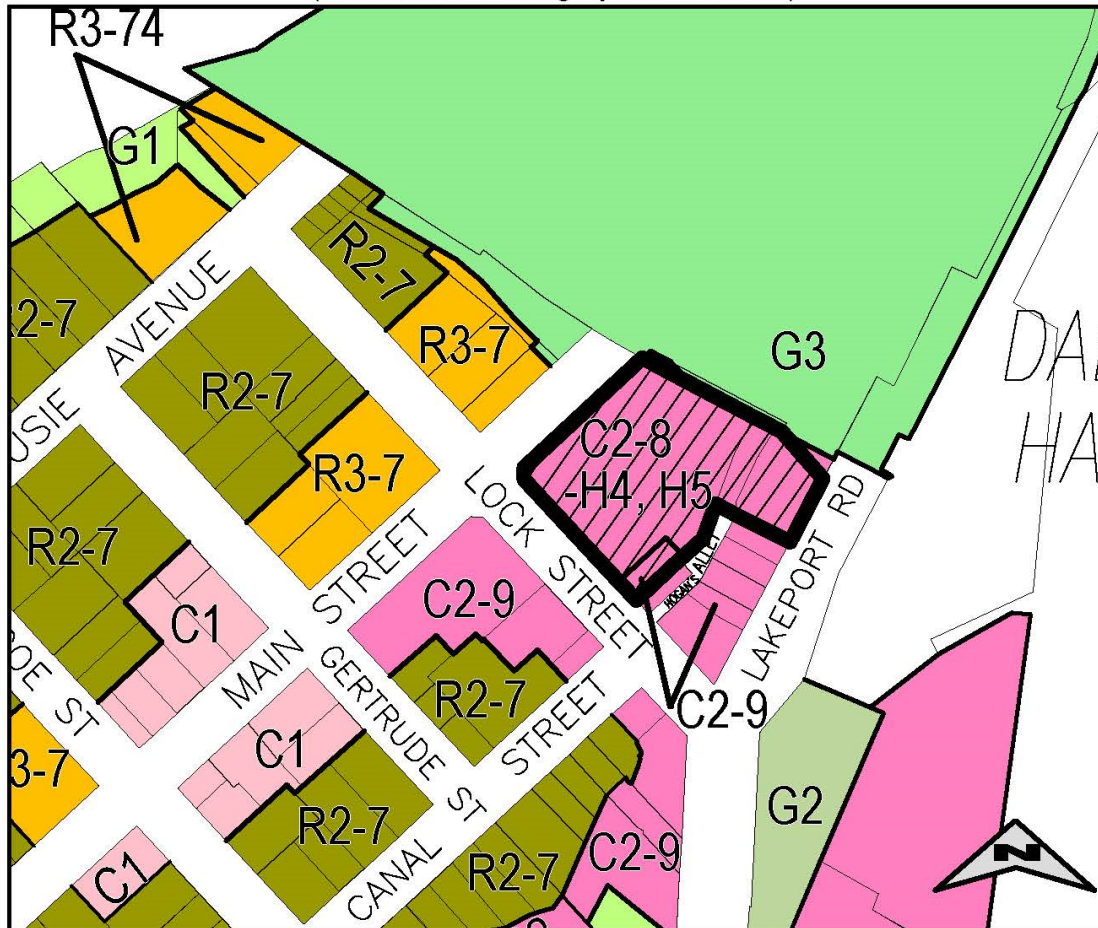
This is "Schedule 1" to By-law No. _____ passed on this day of _____, 2017.

The City of St Catharines

Appendix 10 Zoning By-law 2013-283, Schedule A7

Existing Zoning

(Schedule A - Zoning By-law 2013-283)



Subject Lands

16 Lock Street (12 Lock Street;
12 & 12A Lakeport Road)

Zones

- R2** Low Density Residential
- Traditional Neighbourhood
- R3** Medium Density Residential
- C1** Local Convenience Commercial
- C2** Community Commercial

- G1** Conservation / Natural Area
- G2** Minor Green Space
- G3** Major Green Space

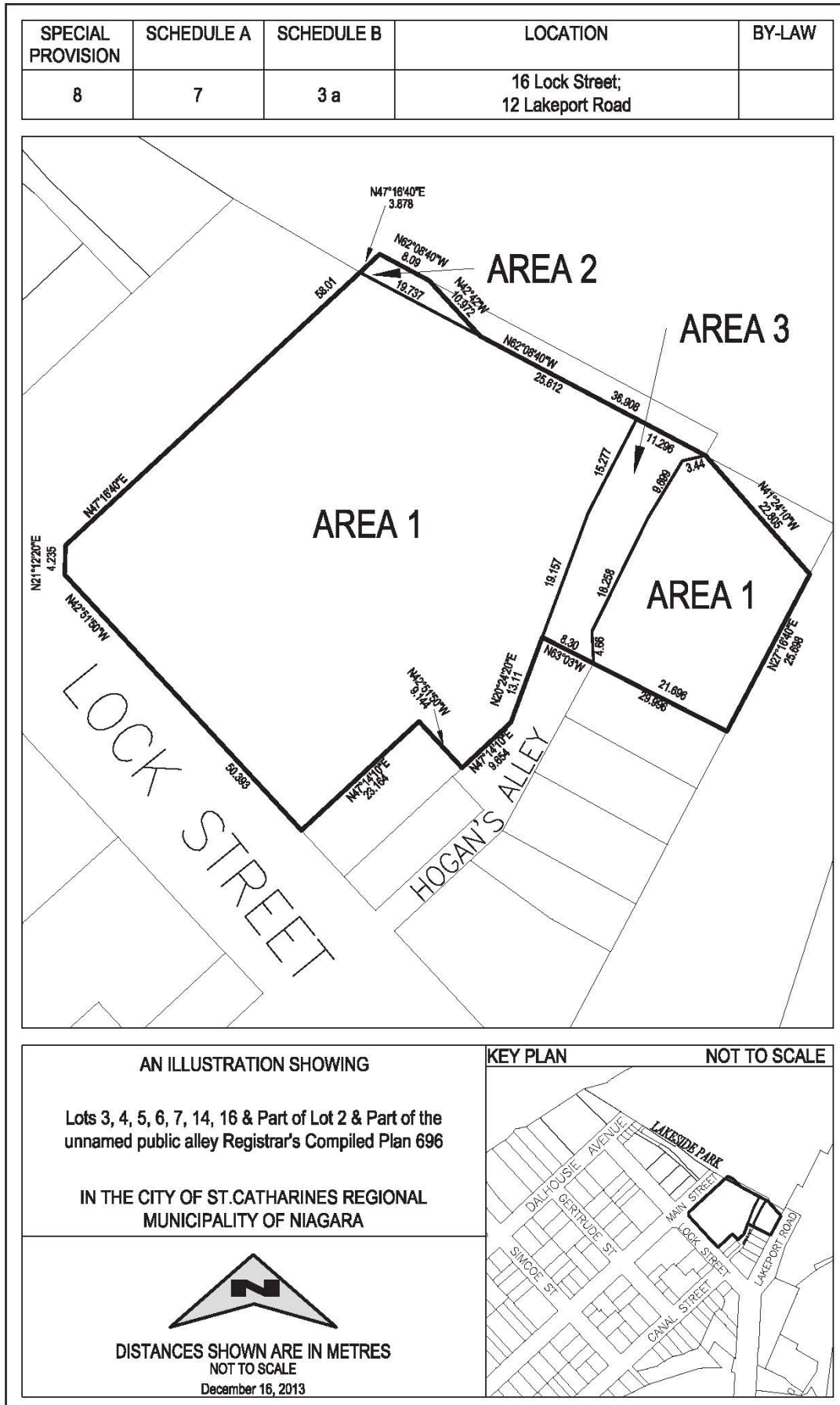
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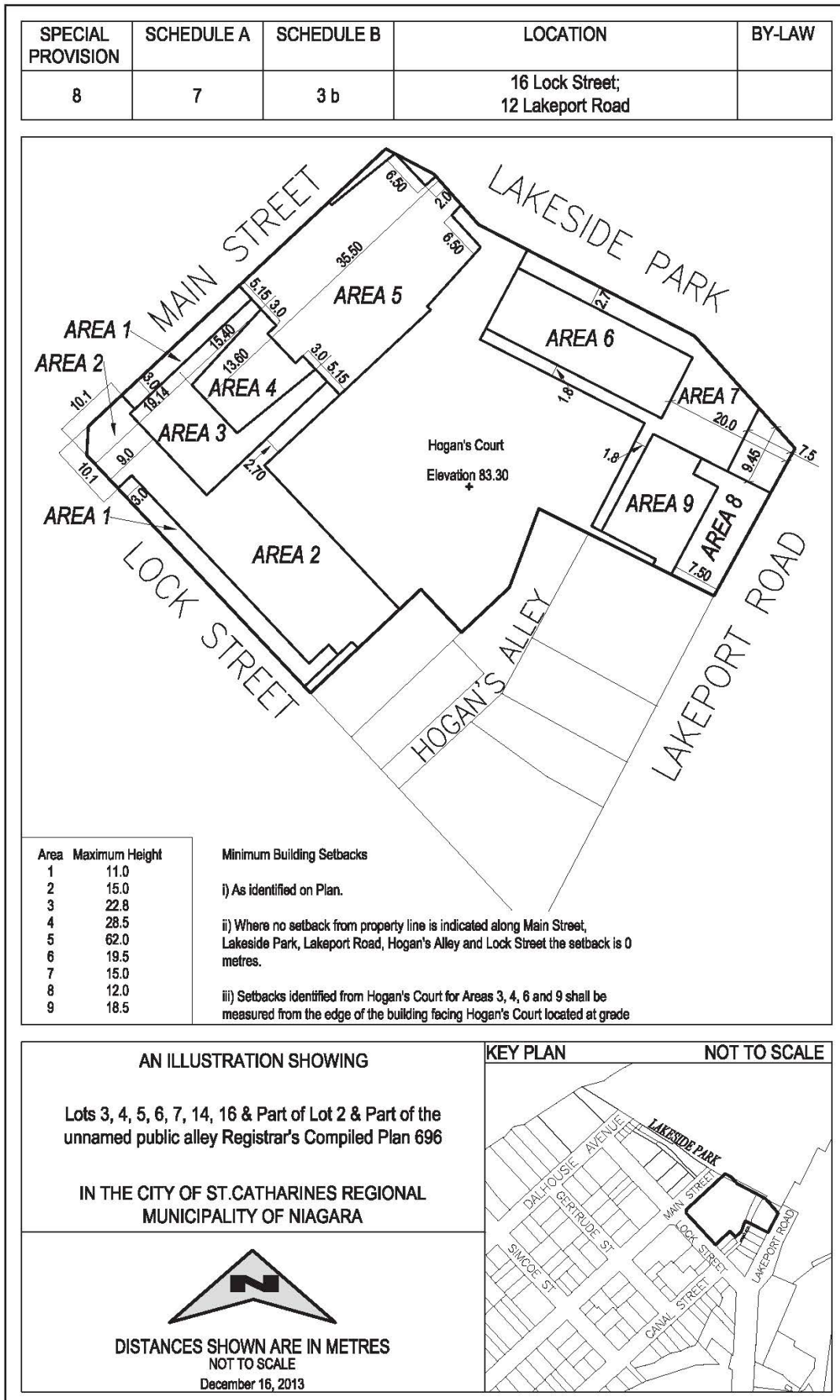
Appendix 11

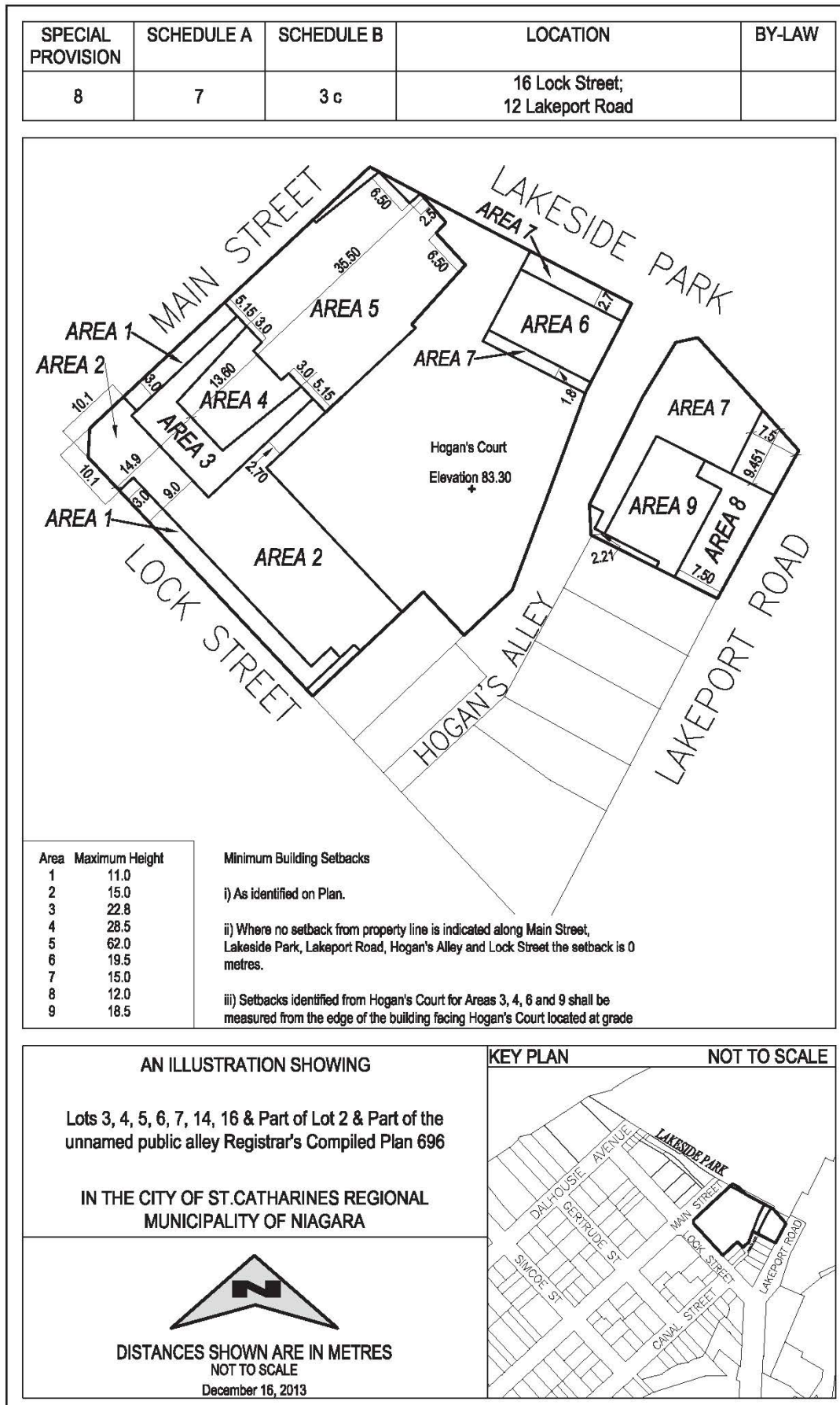
Zoning By-law, Existing Special Provision 8

Special Provision	Zone	Schedule A	Schedule B	Location	By-law
8	C2	7	3a, b, & c	16 Lock St 12 Lakeport Road	
1.	Areas 1, 2 and 3 identified on Schedule B 3a, shall be subject to additional provisions, as follows:				
	a)	Only the following permitted uses - Office - Restaurant - Retail store - A Theatre - Service Commercial - Apartment Dwelling Units over and behind any foregoing use - A residential building up to a maximum of 80 dwellings units and up to 17 storeys, but not to exceed 62 m in height, <i>only</i> in conjunction with the following: - A minimum of 3,600 m ² of retail, restaurant and office space, and - A hotel with a minimum of 70 rooms; and - A theatre with a minimum seating capacity of 400 seats.			
	b)	The following site specific provisions shall apply to Areas 1, 2 and 3, identified on Schedule B 3a:			
	i)	Minimum Building Setbacks from the property line shall be in accordance with Schedule B 3b or if the project is proceeding without Areas 2 and 3, in accordance with Schedule B 3c.			
	ii)	Maximum Height limits shall be permitted in accordance with Schedule B 3b or if the project is proceeding without Areas 2 and 3, in accordance with Schedule B 3c.			
	iii)	Maximum Lot Coverage (including outdoor plaza and public observation terrace) 100%			
	iv)	Minimum Open Space Accessible to the Public <ul style="list-style-type: none"> • 27% of the total site area, including the outdoor plaza and public observation terrace. 			
	v)	Parking			

Special Provision	Zone	Schedule A	Schedule B	Location	By-law
8	C2	7	3a, b, & c	16 Lock St 12 Lakeport Road	
					<p>Minimum required parking for the hotel, 1 space per room and minimum required parking for the residential uses 1.75 spaces per condominium dwelling unit and 1.5 spaces per rental dwelling unit shall be provided underground.</p> <p>Parking for the theatre shall be provided at a rate of one (1) parking space for every four (4) seats of seating capacity.</p> <p>Minimum required parking for the theatre shall be provided by either or both of the following:</p>
					<ul style="list-style-type: none"> • on-site underground parking; • off-site parking in the public parking lot in Lakeside Park, satisfactory to the Municipality;
					No more than 37 of the on-site underground parking spaces required by this Section 1 b) v) may be provided as Tandem Parking Spaces.
		vi)			Setback from the G3 boundary – 0 m; except that one exterior stair case and one ramp to service underground parking and service areas shall be permitted to encroach into the G3 Zone.
		vii)			There shall be no requirement to provide useable recreational space for the <i>exclusive</i> use of each dwelling unit, however, recreational facilities shall be provided as common facilities for the hotel and residential uses.
		viii)			All privately owned open space uses, namely the outdoor plaza and public observation terrace, shall be accessible to the public by easement.
		ix)			Building materials shall be subject to the terms of the site plan agreement, heritage easement agreements and any other agreements deemed necessary to secure building materials sympathetic to the established heritage character.
		x)			For the purposes of this by-law, the following definitions shall apply:
					<ul style="list-style-type: none"> • Grade shall mean the elevation of the finished surface of the ground of Hogan's Court, as identified in Schedules B3b and B3c. • Height shall mean the vertical distance of a building between grade, as defined, and the highest point of the roof. • Tandem Parking Spaces shall mean an area of not less than 30 m² for the temporary parking or storage of up to 2 motor vehicles, or an area of not less than 45 m² for the temporary parking or storage of up to 3 motor vehicles, where access to the parking space for each vehicle may require the moving of another vehicle. Tandem parking shall only be allowed if operated by valet service.
Note: Holding Provisions H4 and H5 also applies to the subject lands.					







Appendix 12

Zoning By-law 2013-283, Proposed Amended Special Provision 8

City of St. Catharines

By-law No. 2017-_____

A By-law to amend Zoning By-law 2013-283, entitled “A By-law to Regulate the use of land, the bulk, height, location and erection and use of buildings and structures, the provision of parking spaces and other associated matters in the City of St Catharines”, and being the Comprehensive Zoning By-law for the City of St Catharines

THE COUNCIL OF THE CORPORATION OF THE CITY OF ST CATHARINES enacts as follows:

That By-law No. 2013-283, as amended, is further amended as follows:

1. Delete existing Schedules 3a to 3c in Section 15.2 – Schedule B – Lot Specific Maps for Special Provisions, being schedules applying to Special Provision 8 as identified on Schedule A7, and replace with attached Schedules 3a to 3k
2. Modify the following existing provisions of Special Provision 8 of Schedule 13 – Special Provisions by deleting the existing provisions so numbered and replacing as follows:
 - i. Areas 1, 2, 3 and 4 identified on Schedule 3a shall be subject to additional provisions as follows:
 - a. Only the following uses are permitted
 - Office
 - Restaurant
 - Retail Store
 - Service Commercial
 - A Hotel to a maximum GFA of 700 m²
 - Cultural Facility
 - Apartment Dwelling Units over and behind any foregoing use
 - A residential building up to a maximum of 220 dwelling units and up to 14 storeys, but not to exceed 50 m in height to the roof of the highest residential floor, and 56 m in height including a mechanical penthouse to a maximum height of 6 m, only in conjunction with the following:
 - A maximum of 2150 m² GLFA of retail, restaurant, office, service commercial or cultural facility space
 - b. The following site specific provisions shall apply to Areas 1, 2, 3, and 4, identified on Schedule B 3a:
 - i. Minimum building setbacks from the property line shall be in accordance with Schedule B 3b to 3k
 - ii. Maximum Height Limits shall be permitted in accordance with Schedule B 3d to 3k. Any building massing that is below the established grade in the central plaza (Hogan’s Court) as shown on Schedules 3d to 3k ((i.e. on P1), which may be visible and accessible from adjacent streets due to

- grade changes, shall not count towards maximum building heights in storeys or meters
- iii. Maximum Lot Coverage (including central Plaza at Hogan's Court) 100%
 - iv. Minimum Open Space Accessible to the Public
 - 24% of the total site area, including the central plaza at Hogan's Court
 - v. Parking
 - Minimum required parking for residential uses shall be at the rate of 1 space per unit, and no parking space is required for the first 4 dwelling units
 - Minimum required residential visitor parking shall be at a rate of 0.17 spaces per unit
 - There is no minimum parking requirement for permitted retail store, office, restaurant, service commercial or cultural facility uses. However, all residential visitor parking spaces located on the same property shall also operate as parking for these uses
 - All required parking spaces shall be provided onsite in structured and underground parking areas
 - vii. There shall be no minimum requirement to provide useable outdoor recreational space for the exclusive use of each dwelling unit
 - viii. All privately owned open spaces at or below grade, namely the central outdoor plaza at Hogan's Court and associated stairs, shall be accessible to the public by easement
 - x. For the purposes of this by-law, the following definitions shall apply:
 - Grade shall mean the elevation of the finished surface of the ground of the central plaza at Hogan's Court, as identified in Schedules 3d to 3k
 - Height shall mean the vertical distance of a building between grade, as defined, to roof of highest residential floor, plus up to 6 m for mechanical penthouse, including parapets, and excluding projections like ladders, smoke stack, mechanical equipment, etc.
 - xi. Encroachments
 - Balconies, canopies, awnings, signage and building projections below, at or above grade may encroach into required yards as provided in Schedules 3b to 3k, to a maximum of 1.7 m
3. Where an existing provision within Special Provision 8 is not proposed to be modified as through the above, it shall remain within Special Provision 8 and be renumbered as appropriate
 4. Delete existing Schedules 1a and 1b in Section 15.3 – Schedule C – Lot Specific Maps for Holding Provisions, being schedules applying to Holding Provisions H4 and H5 respectively, as identified on Schedule A7, and replace with attached Schedules 1a to 1k

5. Modify existing Holding Provision H5 to read as follows:

An additional Holding (H) designation shall apply to Areas 1-4, as identified on Schedules C 1a to 1k, which shall be lifted by City Council once the following additional condition is met:

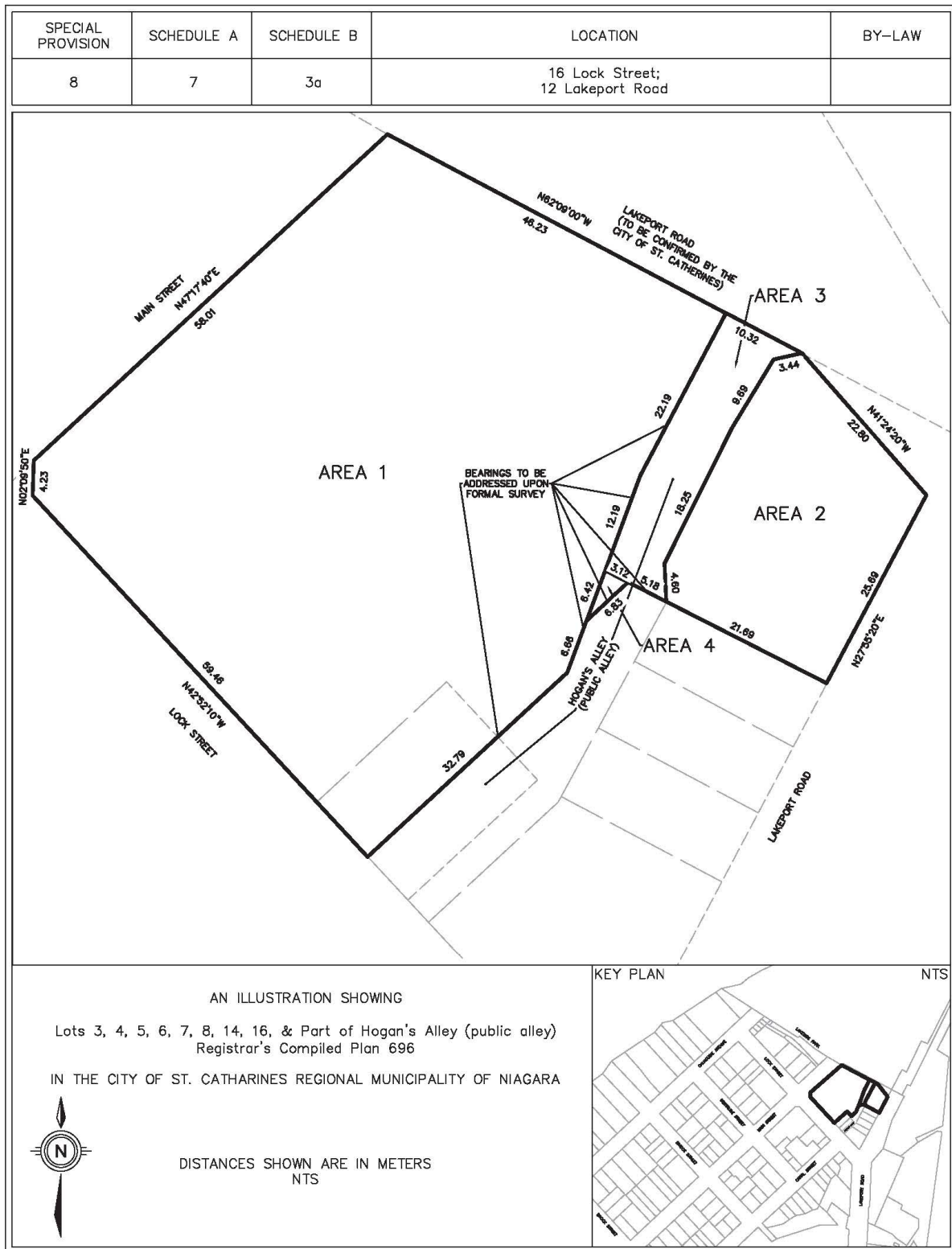
1. Prior to the residential use of the lands, all commercial uses, including a maximum of 2150 m² of retail, office, restaurant, service commercial and cultural facility, and the required structured and underground parking spaces, shall be constructed, and such construction shall include, but not be limited to, complete exterior finishing and interior demising walls of all buildings.
6. Where an existing provision within Holding Provision H5 is not proposed to be modified as through the above, it shall remain within Holding Provision H5 and be renumbered as appropriate

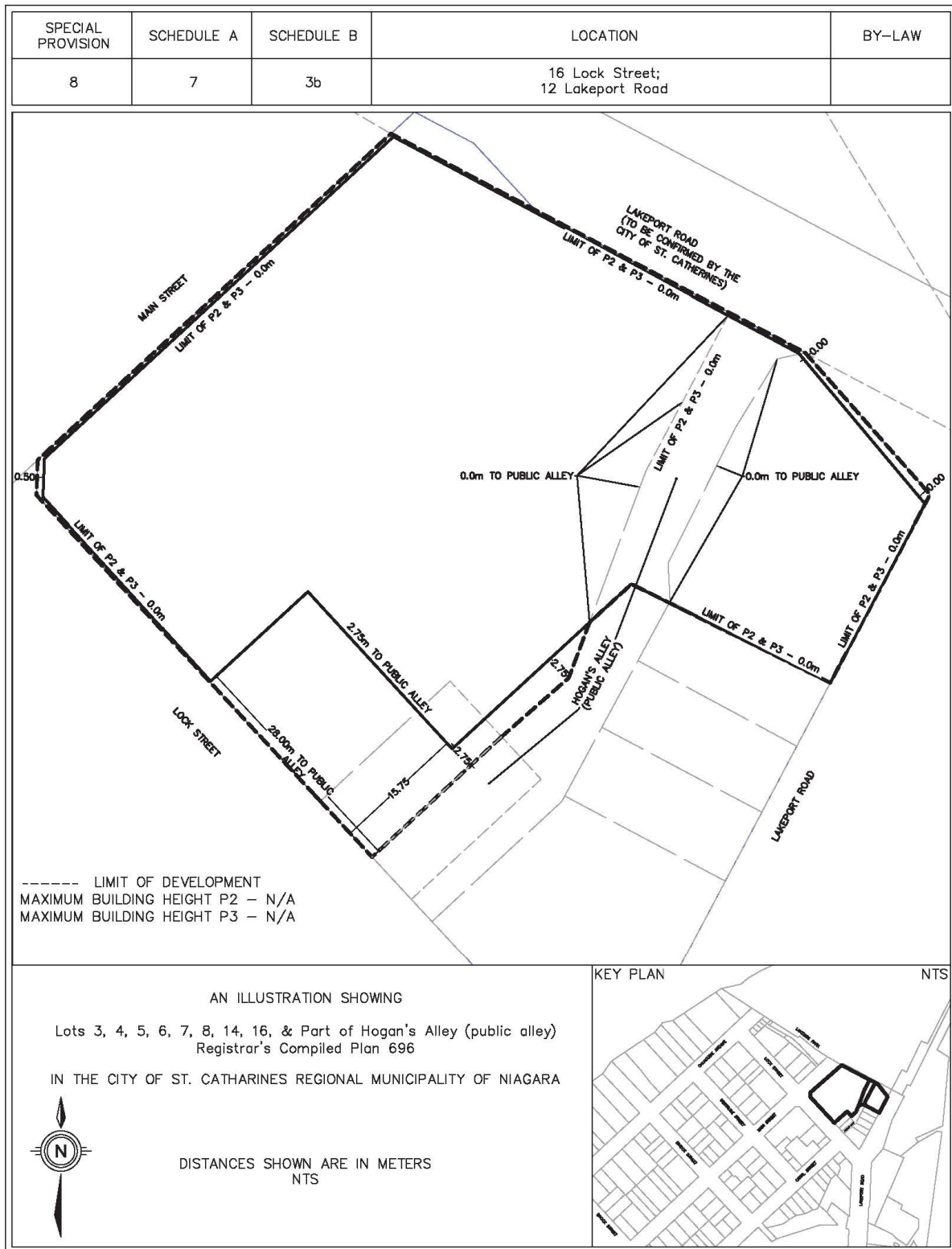
This By-law shall come into force and effect on the date of passing by Council, subject to the provisions of the Planning Act, R.S.O 1990, c.P.13, as amended

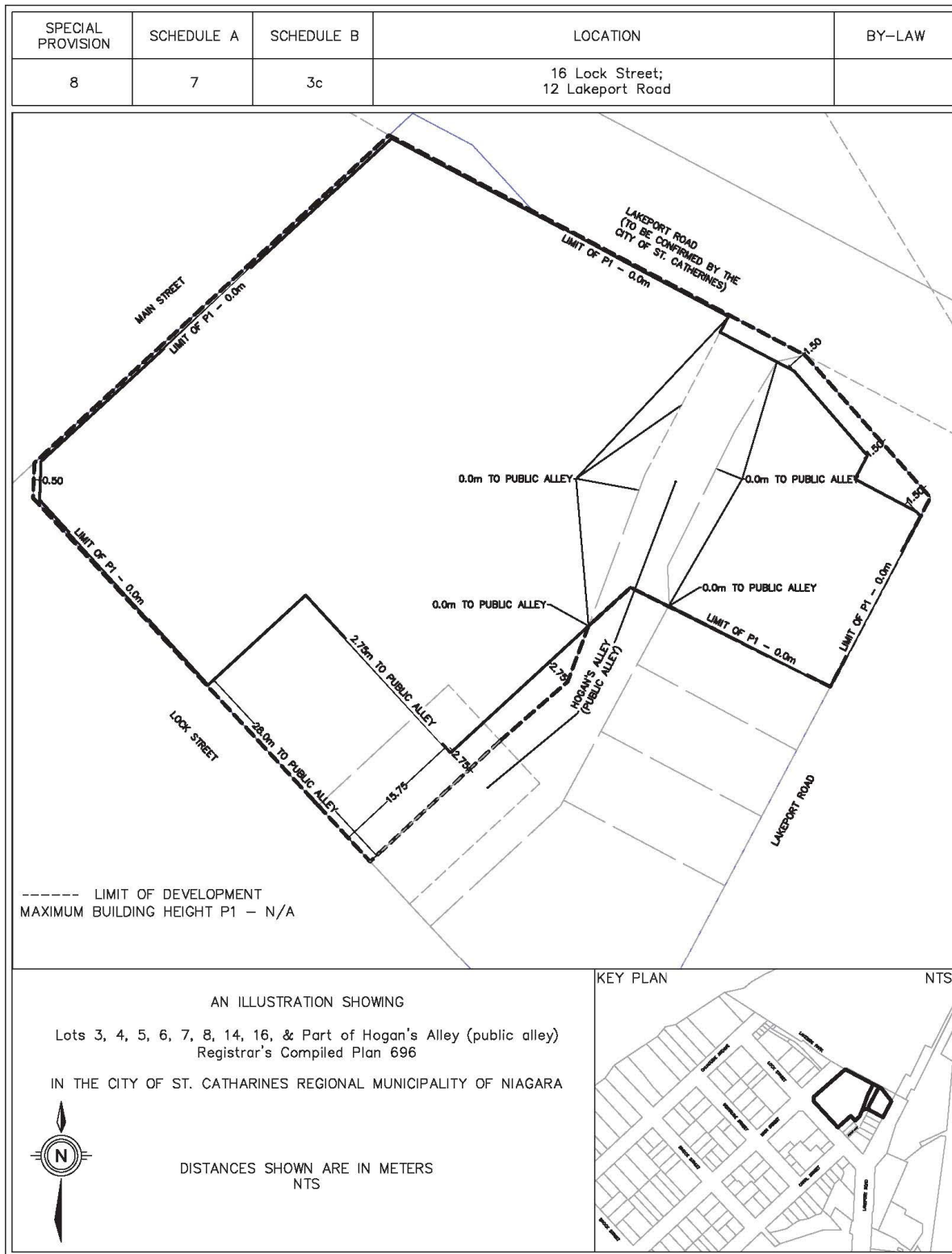
Read and passed this _____ day of _____ 2017

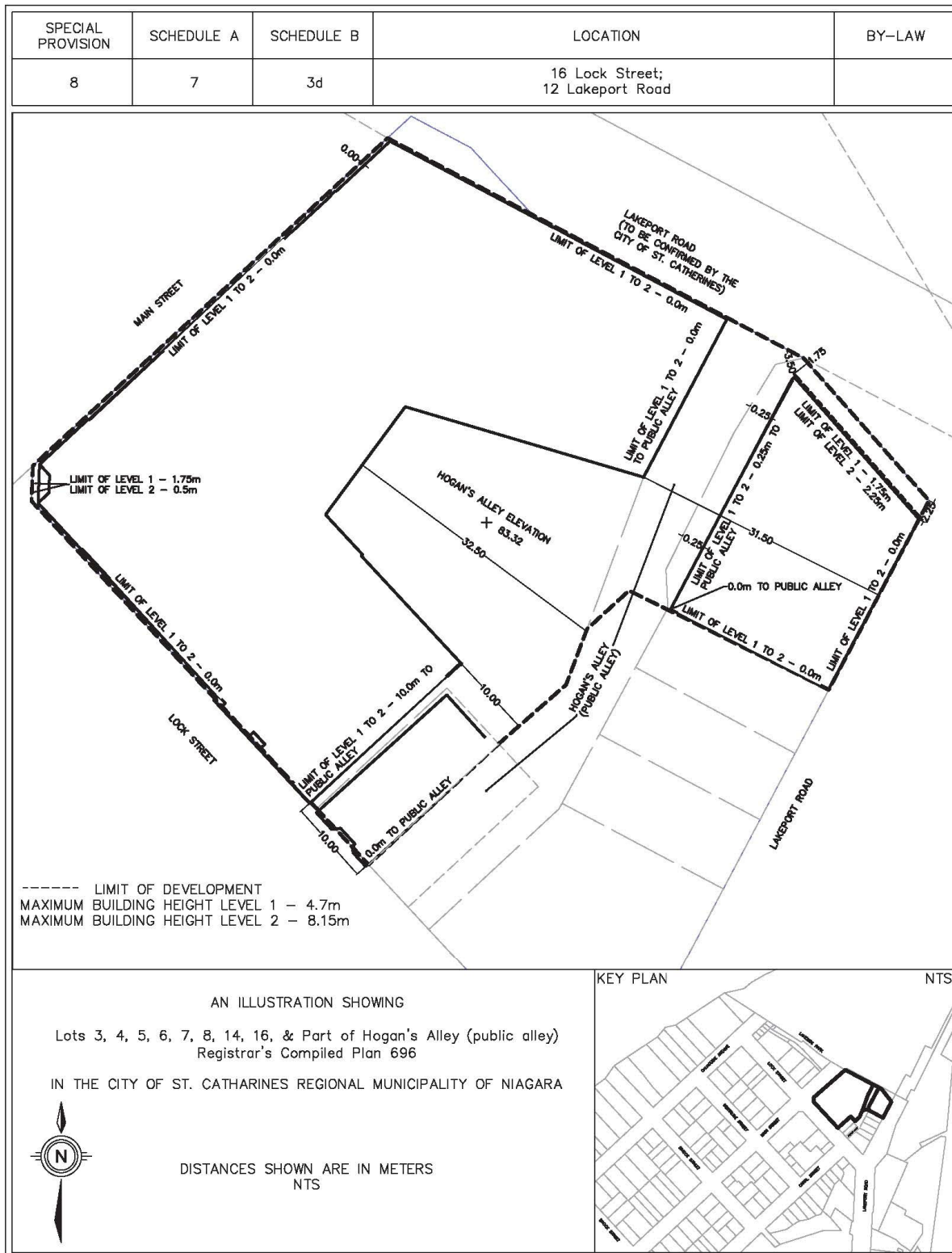
Clerk

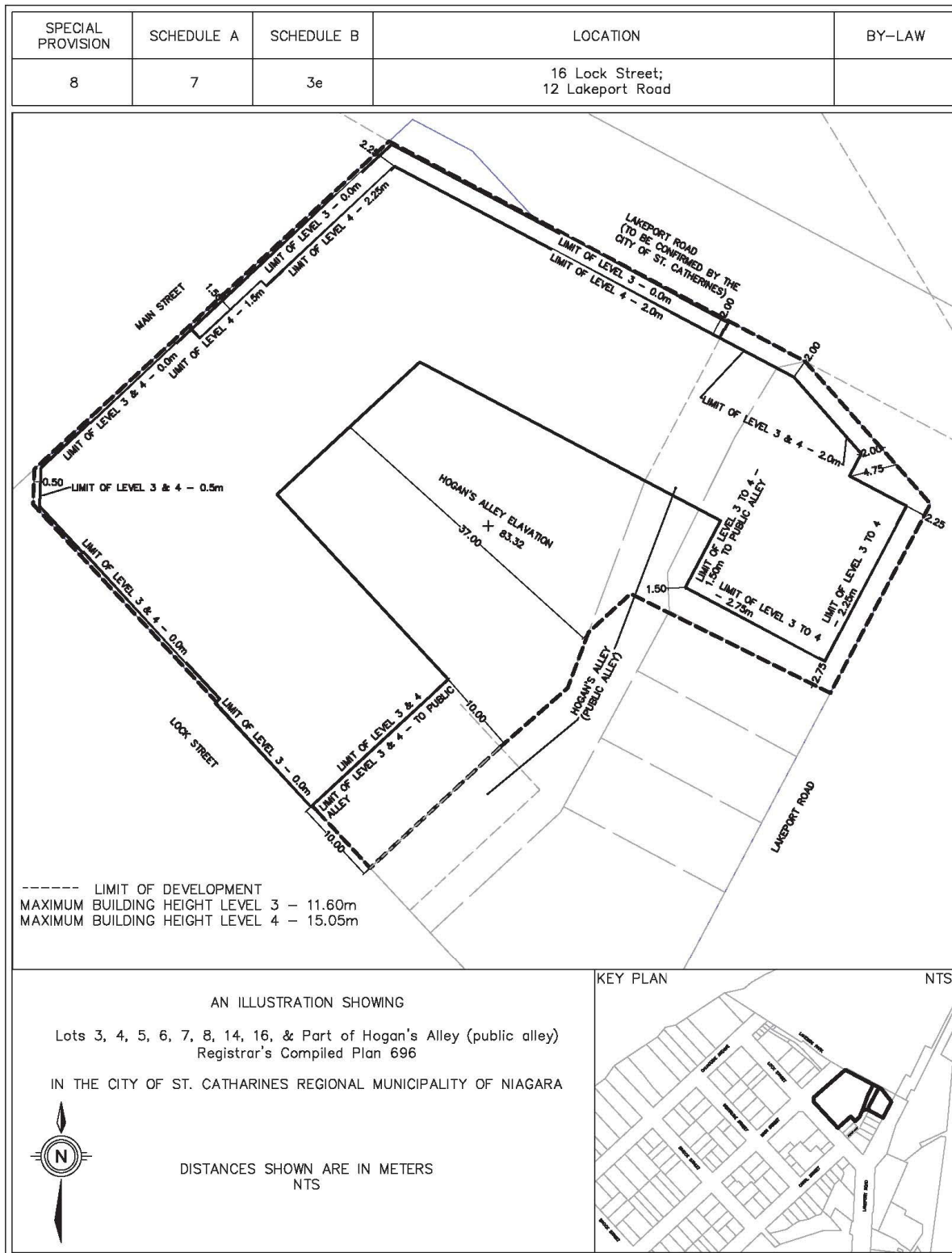
Mayor

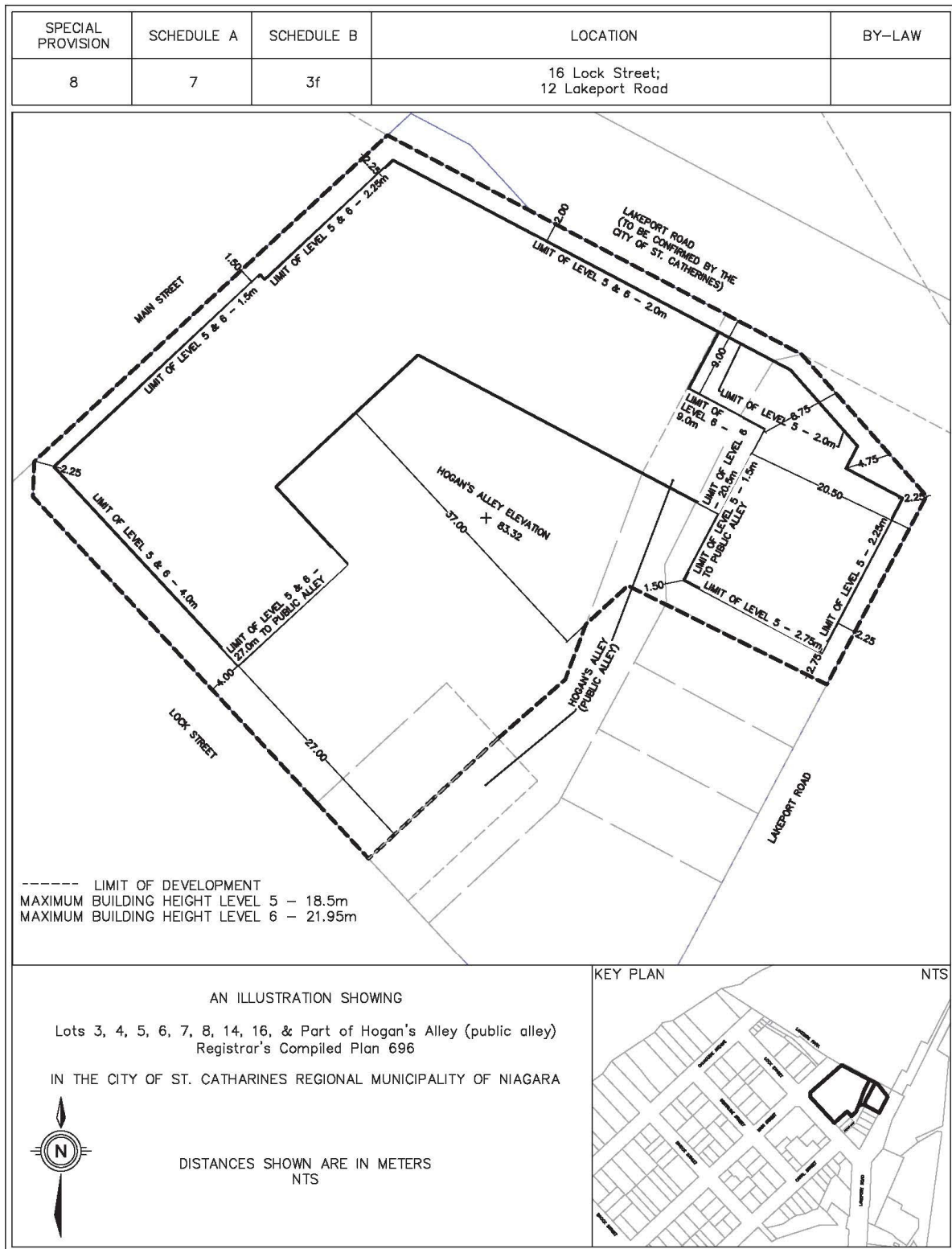


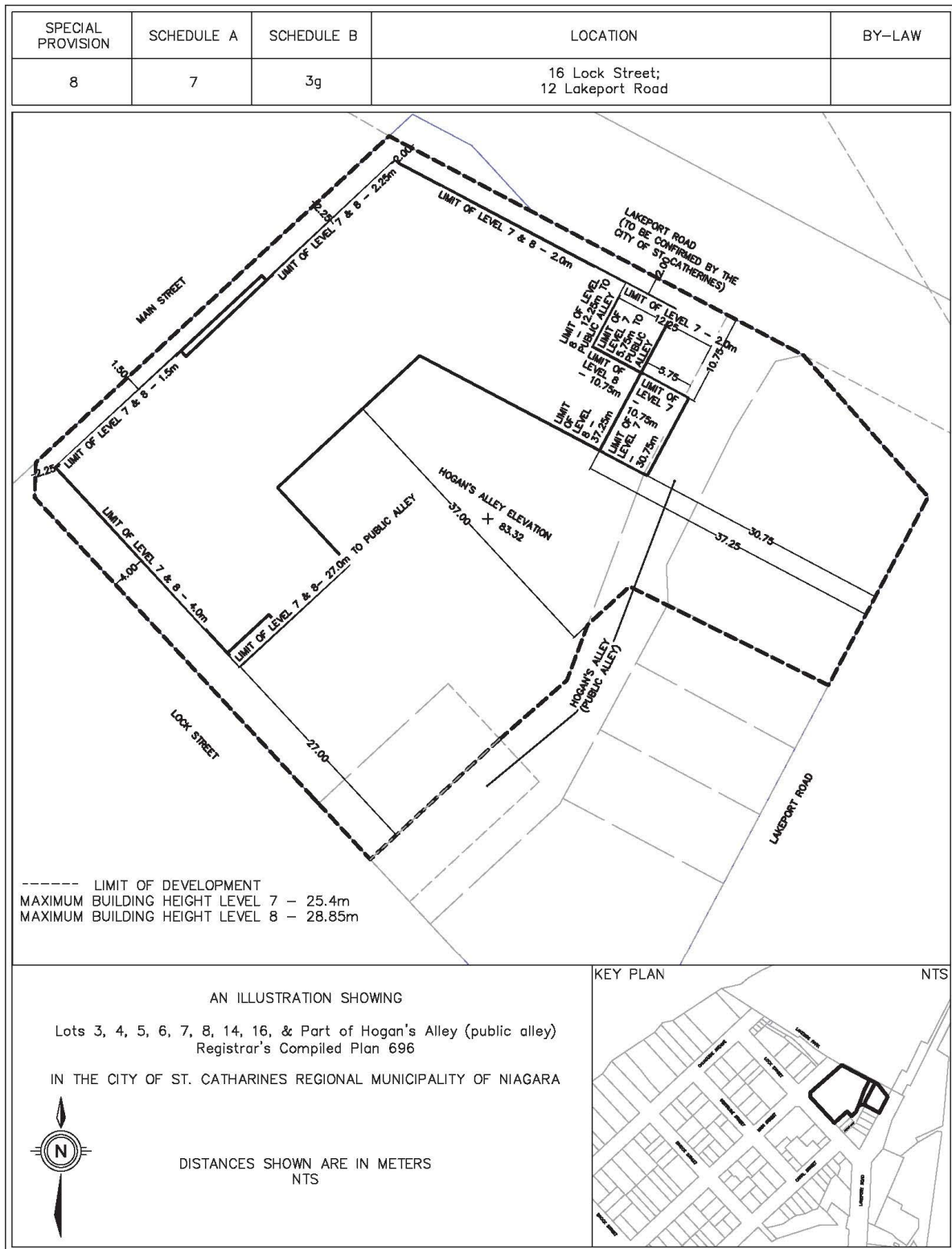


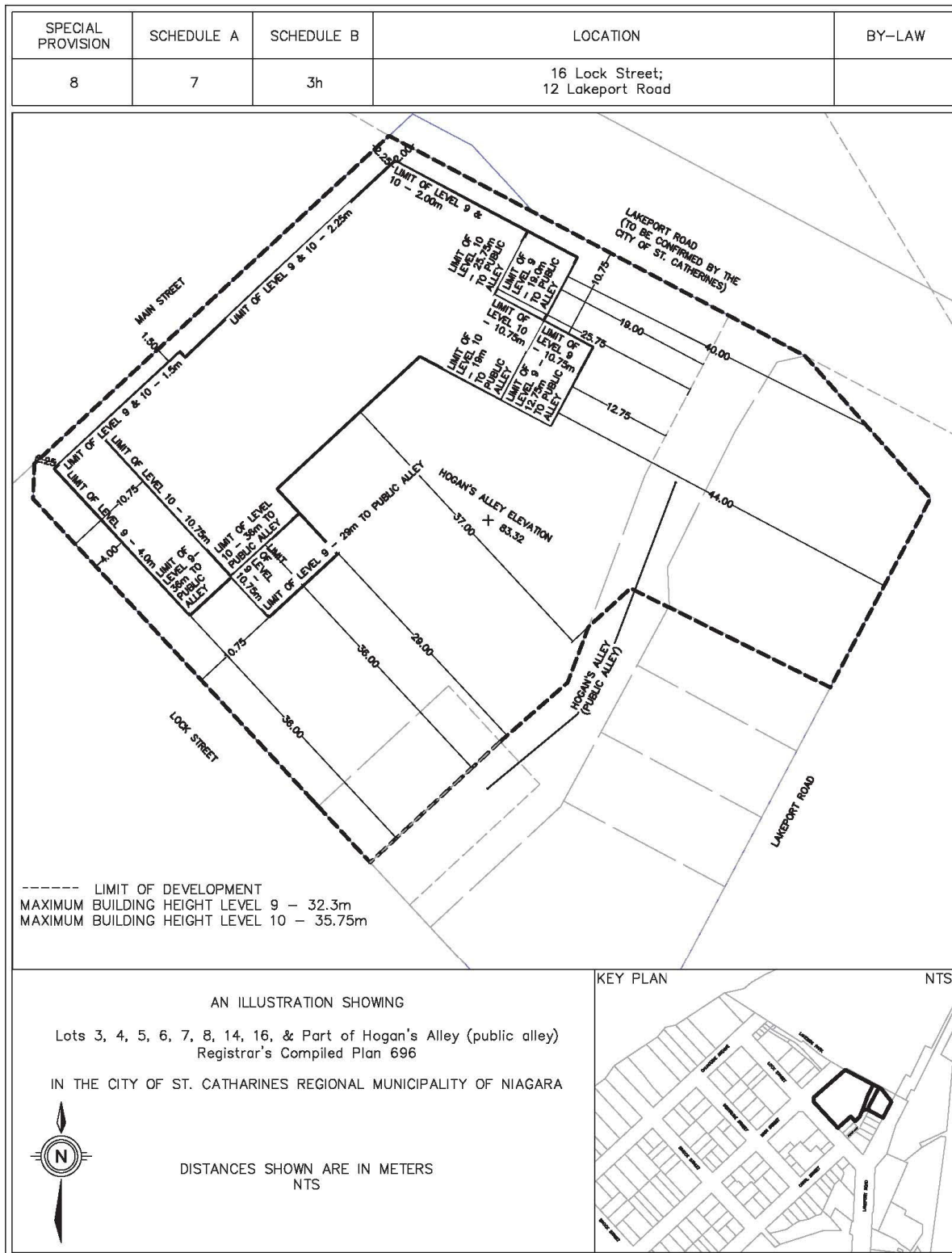


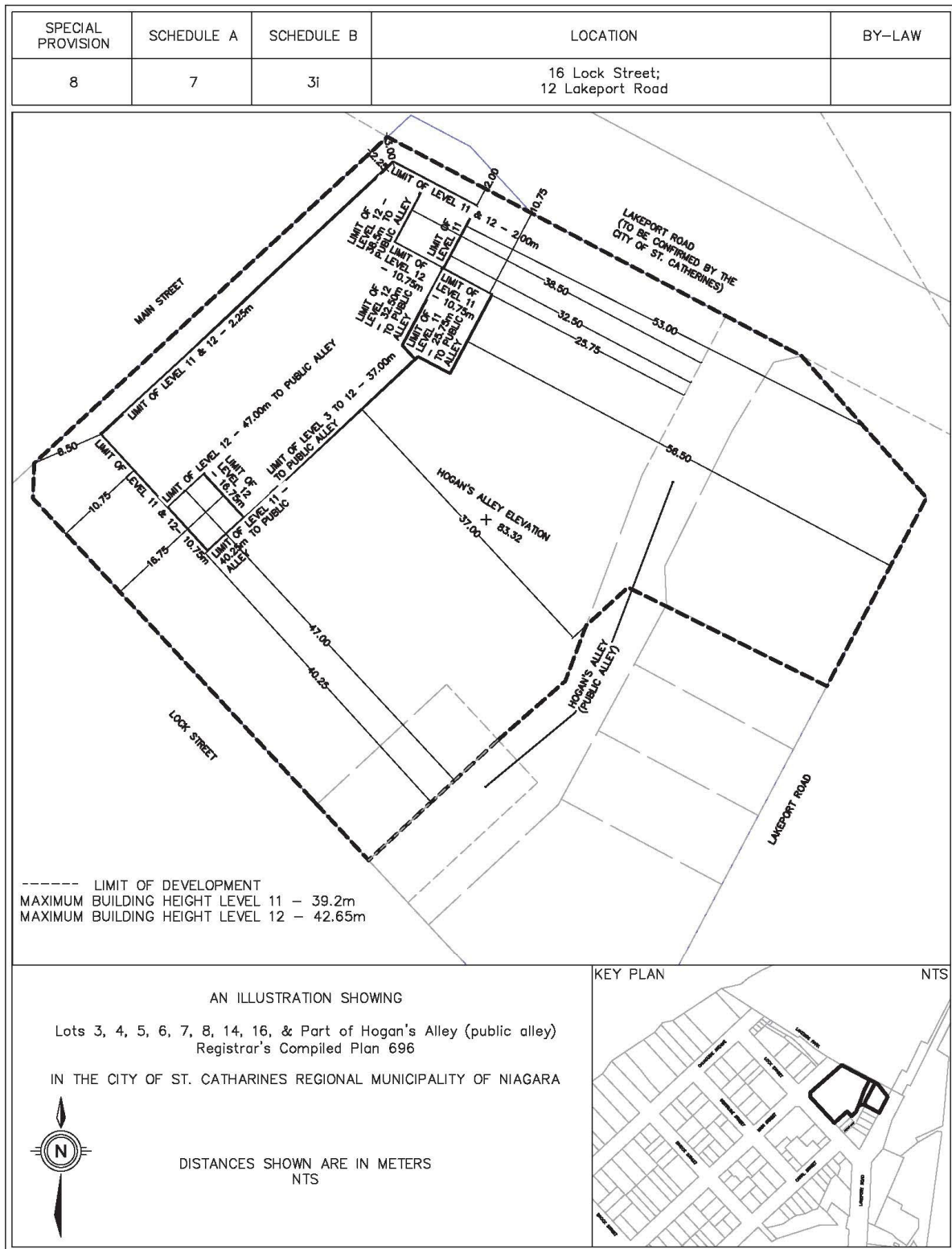


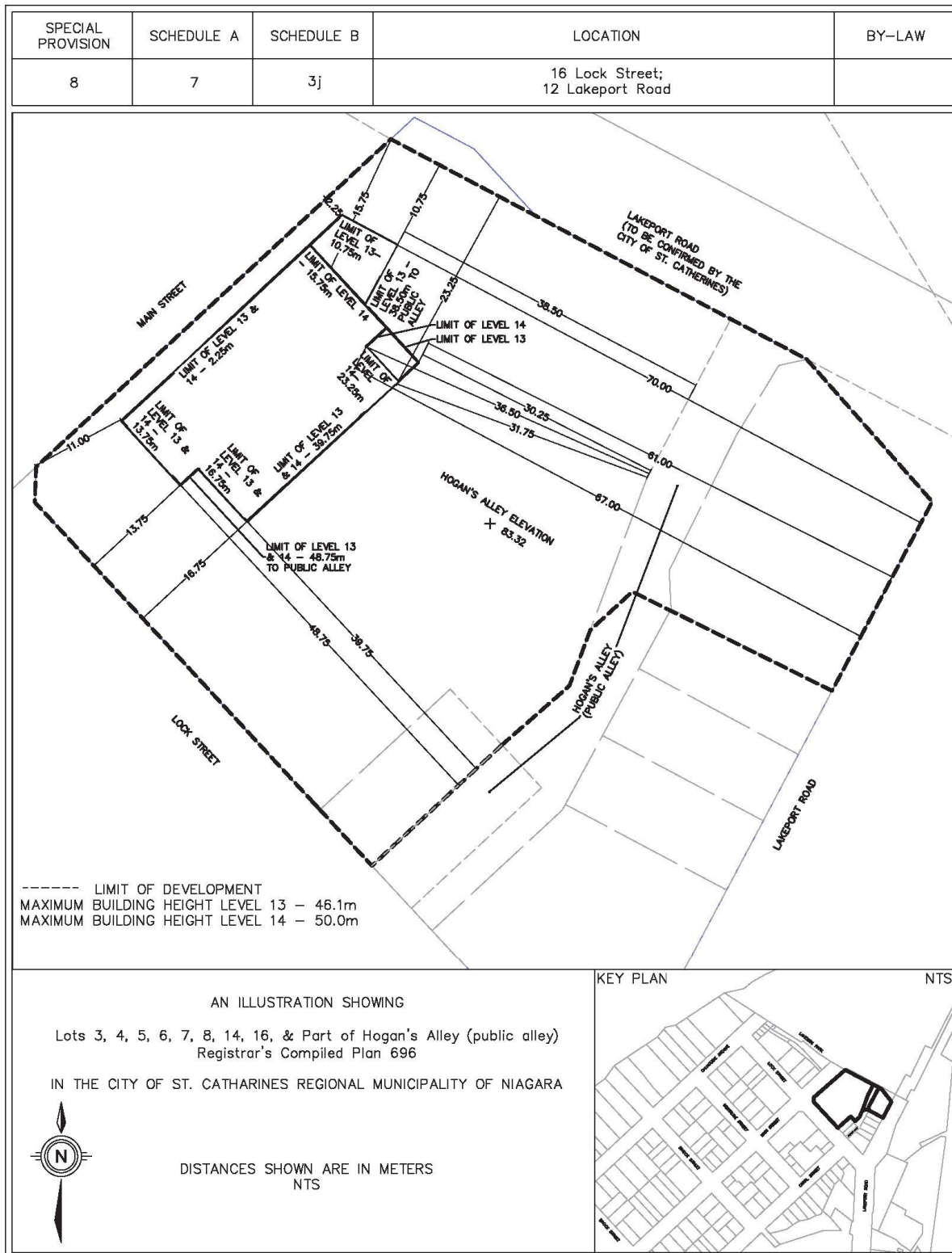


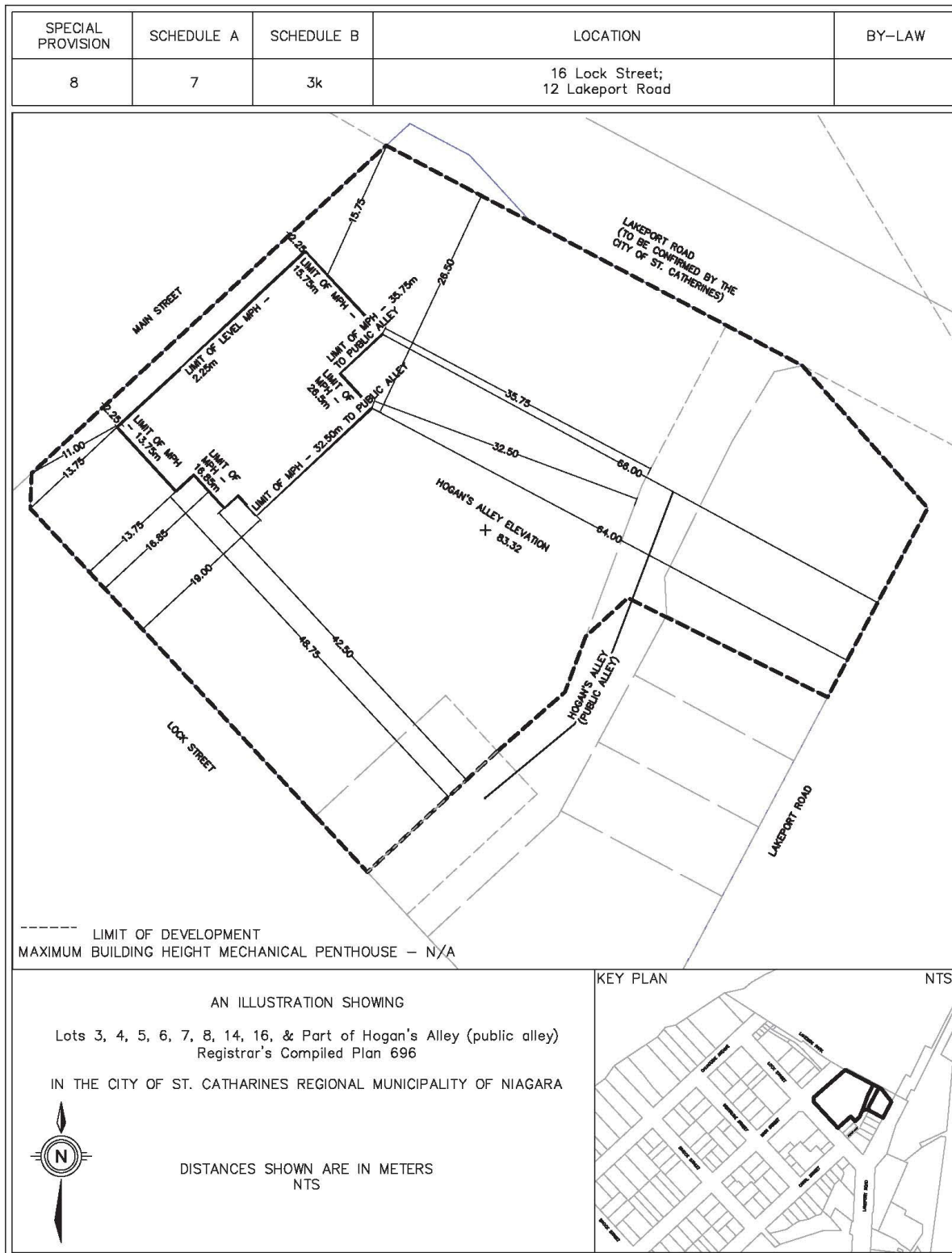












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Letter of Comments from Niagara Region



Planning and Development Services
 1815 Sir Isaac Brock Way, PO Box 1042, Thorold, ON L2V 4T7
 Telephone: 905-980-6000 Toll-free: 1-800-263-7215 Fax: 905-687-8056
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Via E-mail Only

August 1, 2017

Files: D.10.08.OPA-17-018
 D.18.08.ZA-17-078

Scott Ritchie, MCIP, RPP
 Urban Design Planner
 City of St. Catharines
 50 Church Street
 St. Catharines, ON L2R 7C2

Dear Mr. Ritchie:

**Re: Regional and Provincial Review Comments
 Official Plan Amendment & Zoning By-law Amendment
 11 Main Street, 16 Lock Street & 12 Lakeport Road
 City of St. Catharines**

Regional Planning and Development Services staff has reviewed the above noted applications and supporting materials submitted by IBI Group on behalf of Union Waterfront Inc. for an Official Plan Amendment and Zoning By-law Amendment for lands known municipally as 11 Main Street, 16 Lock Street and 12 Lakeport Road in the City of St. Catharines. These applications have been submitted to facilitate the development of a proposed 14 storey mixed-use development containing 220 residential units and 2150 m² of gross leasable floor area. The applications were received by Regional staff on July 13, 2017, with fees received on July 27, 2017.

A pre-consultation meeting was held to discuss the proposed development and application requirements on August 11, 2016 with City and Regional staff, the applicant and their representatives in attendance.

Regional staff notes that applications for Site Plan Approval, Plan of Condominium and Heritage Permits will also be required to facilitate this proposed development. These applications will be submitted by the applicant for review and consideration at a later date.

The following comments are provided from a Regional and Provincial perspective to assist the City in developing a recommendation on these applications.

Previous OMB Decision

Regional staff recognizes that to implement the proposed amendments, the modification and/or deletion of Official Plan policies or Zoning By-law provisions previously established by the OMB will be required. Minor changes to the Official Plan and Site Specific Zoning By-law schedules that pertain to the site are also proposed.

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Regional and Provincial Policies

The subject lands are located within the St. Catharines Urban Area, as defined by the Regional Official Plan (ROP). The St. Catharines Urban Area is considered as a Settlement Area by the 2014 Provincial Policy Statement (PPS). Specifically, the subject lands are within a portion of the City known as Port Dalhousie, and are bounded by Lock Street (W), Main Street (N), Lakeport Road (E, S) and Hogan's Alley (S).

The ROP, 2014 PPS and 2017 Growth Plan for the Greater Golden Horseshoe (Growth Plan) together direct development to take place in urban areas and delineated built-up areas to support intensified development and to build upon existing servicing and infrastructure. Both Regional and Provincial policy place an emphasis on intensification and infill to foster the development of complete communities that have a mix of diverse land uses and housing choices, improved social equity and quality of life, new and expanded access to multiple forms of transportation, the provision of spaces that are vibrant and resilient in their design, and the conservation of built and cultural heritage resources.

The subject lands are located within the Provincially designated Built-up Area. The residential portion of the proposed development is considered as Intensification and will count towards the City's annual residential intensification target of 95%, as set by the Regional Official Plan.

Justification for the proposed amendments has been provided in the submitted Planning Justification Report (PJR), dated June 2, 2017 and prepared by Registered Professional Planners at the IBI Group. The report is cited in the analysis provided below,

Official Plan Amendment (OPA)

The OPA proposes to modify the previous policy direction established through the OMB decision. The applicant is proposing that Official Plan Policy 15.1.1.ii) be modified to permit the following uses on the subject lands:

- i. A 14 storey mixed use residential building with a maximum height of 50m (56m to the top of the mechanical penthouse);
- ii. A multi-use redevelopment concept that includes retail, office, restaurant service commercial, and if deemed feasible, a hotel;

As outlined in the draft OPA, additional policy direction has been proposed that requires that appropriate development standards shall be set out in the regulations and agreements (i.e. zoning by-law, site plan agreement, heritage easement agreement, and any other related agreements).

The provision of mixed-use development within urban environments is encouraged and supported by Regional and Provincial policy. The Lock Street/Main Street/Lakeport Road area of Port Dalhousie serves as the commercial centre for the community, and is the most appropriate area for commercial and mixed use development to be directed, as demonstrated through its historic function as an informal downtown, and the policy and development direction of the City's Plans and By-laws.

The proposed development is designed to be 14 storeys (approx. 56m to mechanical penthouse) and contain 220 residential dwelling units. Regional staff acknowledges that the height is less than the "Port Place" proposal in 2009, which was 17 storeys and 62m in height,

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and the number of dwelling units proposed is greater than that which was previously approved (80 units, 70 hotel rooms).

Regional staff acknowledges that the applicant is proposing to emulate the heritage character of the Austin House throughout the commercial portion of the building to soften the visual impact – namely by using materials to continue the 3 storey commercial façade throughout the Lock Street corridor. This is meant to soften any visual impacts at the pedestrian scale. The PJR also notes that a stepped-back, terraced approach to height is also being employed to abate visual impacts associated with height.

The permitted uses for the site appear to be generally in keeping with what the OMB had approved previously, and other policies of the local Official Plan. Further, based on the supporting materials submitted (Transportation Impact Study, Serving Study, etc.) the surrounding transportation network, and municipal services are not anticipated to be negatively affected by the increase in density in this area. Therefore, and subsequently, the development is considered to meet the intent of Provincial and Regional policies for mixed use developments.

Zoning By-law Amendment

A zoning amendment is required to facilitate the proposed design of the building, additional permitted uses and operational requirements. Several modifications have been proposed to the Site Specific Zoning previously established by the Board, however their intent generally deals with permitted uses, massing, parking and holding provisions. A detailed explanation of each is provided below

Permitted Uses

A range of permitted uses have been requested to be added to the parent Zone through site specific permissions. The primary use being requested is a residential building containing 220 dwelling units and 14 storeys in height, with a maximum gross leasable floor area (GLFA) of 2150 m². The previously approved use on the site was to be a 17 storey building, with 80 dwelling units and 3,600m² in GLFA. These prior development permissions are proposed to be deleted through the amendment.

Within the leasable floor area office, restaurant, retail store, service commercial and cultural facility uses are proposed permitted uses, including a hotel with a maximum floor area of 700m². As outlined in the PJR, the hotel use would only be explored if deemed viable within the community and development. The previous permitted uses of a 400 seat theatre, hotel with a minimum of 70 rooms are proposed to be deleted.

Generally, these uses appear to be in keeping with Regional policy direction regarding the provision of a variety of complimentary land uses in mixed use areas.

Parking

The applicant is requesting a reduction in required parking from 1.75 spaces per condominium unit to 1 space per dwelling unit (which are ultimately intended to be Condominium tenure). A visitor parking ratio of 0.17 spaces per unit is also proposed. The applicant intends to delete all site specific parking requirements in place related to uses/spaces no longer part of or related to the development (i.e. theatre, Lakeside Park).

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The requested reduction in required parking spaces does not affect any Regional or Provincial planning interests.

Modification of Holding Provision

Similar to the Port Place application, a Holding provision is being applied to ensure that the non-residential components of the project are largely completed prior to occupancy of the residential units being permitted. Regional staff supports this approach as it will ensure that the publicly accessible elements and services that will support the residents and surrounding community are included in the development as proposed.

Built Heritage Resources

The subject lands are located within the Port Dalhousie Heritage Conservation District. The subject lands themselves contain designated heritage properties known as the Austin House Hotel and the Port Dalhousie Jailhouse.

Section 2.6 of the PPS and Chapter 10.C of the ROP direct that Built Heritage resources are to be conserved. As a further part of these efforts towards conservation, policy also directs that Planning authorities shall not permit development or site alteration adjacent to protected heritage properties.

Based on a review of the provided design concept, it appears that the developer intends to integrate the Austin House into the redevelopment plan, by maintaining the façade and incorporation its design attributes into the podium of the mixed use building. To ensure that the development can proceed with the Austin House included as a structural elements, Regional staff recommends that the local Heritage Committee be consulted prior to approving the applications (to avoid the need for a redesign/modification, or additional zoning provisions).

As noted, Heritage Permits from the local Heritage Committee will be required, under the Ontario Heritage Act. This application will be filed at a later date.

Archaeology

In support of the previous planning applications submitted for the subject lands (i.e. Port Place), Stage 1-3 Archaeological Assessments were completed. However, based on discussions with Ministry of Tourism, Culture and Sport staff, it was noted that those previous studies do not appear to have been completed in accordance with the current Provincial requirements for Archaeological Assessments. Accordingly, Provincial staff requested that a revised Stage 1 Report (at minimum) be completed and submitted with this application. This request was made of the applicant during pre-consultation.

The applicant has submitted a Stage 1 Archaeological Assessment that has been prepared by Archaeological Services Inc. (ASI) and is dated June 2, 2017. Based on a desktop review of the context of the site, the Consultant is of the opinion that the lands have a low cultural heritage value and has not recommended any further study.

Regional staff request that the applicant submit a copy of the Stage 1 Assessment to the Ministry of Tourism, Culture and Sport for review, and that a copy of their confirmation of registration be provided to Regional staff at their earliest convenience.

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The inclusion of standard warning clauses regarding deeply buried archaeological resources and/or human remains will be required as conditions of Site Plan approval.

Regional Servicing Infrastructure

Regional technical staff have reviewed the Functional Servicing and Stormwater Management Report by TMIG dated June 2017 and the Servicing Plan by TMIG dated June 2, 2017 and offer the following comments.

The Region is the owner and operator of the Lakeside Sewage Pumping Station which is located just north east of the proposed development. The Region has recently completed a Master Servicing Plan for the Regional Water and Sanitary Sewer Infrastructure. Lakeside Sewage Pumping Station was reviewed during this process and has a current capacity of 95 L/sec, future flows were added for the various developments in the sewer shed and the station still has capacity. The development has calculated a peak flow of 5.25 L/sec. Based on the current flows and predicted flow, the Regional Sewage Pumping Station will have sufficient capacity to service the development.

During the site plan stage the Region will request that warning clauses be added acknowledging the potential for regular operating noise and odour from the pumping station.

Transportation Impact Study

Regional staff notes that the subject lands are located along Regional Road 87 (Lock Street). The Region is also the controlling authority on the intersection of Lock Street and Lakeport Road, to the south of the subject lands.

Regional technical staff has reviewed the provided Transportation Impact Study report titled "*Union Waterfront St. Catharines Proposed Mixed Use Development TIS by Paradigm dated June 2017*" and are satisfied with the report.

The report notes that the Regional intersection of Main Street and Lock Street may experience delays during summer peaks and currently there is no suggestion for improvements at this time. In previous reports there was also a warrant for a left turn lane at Lock Street and Lakeport Road but due to the change in existing and proposed traffic counts the recommendation is not included in this report. Since this proposed development has the potential to change the scenario, Regional staff would like to add a condition to the site plan agreement (at a later date) to have the developer confirm the predicted traffic patterns and that the existing intersection configurations can accommodate the proposed traffic once the development has reached substantial occupancy. Regional staff has included an example of the conditions that the Region will include in the future site plan application in the Appendix for information purposes.

Of further note, through the Site Plan process the Region may be requesting an Encroachment Agreement for the underground parking if the structure, or any shoring works are to be located/occur within the Lock Street right-of-way. The applicant has been notified of this requirement via e-mail and has acknowledged this potential requirement.

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Urban Design

Built Form Massing and Layout

The location of the bulk or massing of the building on the site is similar to the Port Place design, and surpasses it by reducing the overall height of the building and shifting the bulk of the height westward shadows to fall on within the property and lessening shadowing of adjacent properties. The balance of the site is comprised by medium and low building forms with units facing the courtyard and Hogan's Alley. This approach allows sunlight to penetrate into the courtyard and alley spaces making these an important public element of the design.

The proposed development is 3 storeys shorter than the previous design. However, the lesser height of the proposed development is offset by a substantial increase to the bulk of the building. Where the previous development comprised a traditional podium and tower configuration, the proposed comprises a podium and longer centre slab block with terracing wings. This increased bulk of the proposed will have a greater visual impact on the surrounding context, in the sense that there is more building mass at heights above 3 storeys than the former design. Staff is not able to determine and compare from the shadow study if shadow impacts of the proposed development are greater. Staff suggests that a better graphical representation is developed to compare shadows between the former design and the proposed design.

Notwithstanding, the overall design of the proposed building is substantially more attractive than the previous design. A more attractive building design composed of appropriate materials, treatments, gradation, and terracing can lessen the visual impact of the proposed building by making it pleasing to the eye and diminishing the contrast between existing and new. The proposed building is larger than anything in its vicinity. However, the proposed building successfully employs a number of architectural design measures that seek to achieve visual harmony within its existing and planned context. For example the proposal uses two different exterior building materials for the upper storeys which can diminish the overall bulk of the building by visually disassociating these massing components making it appear as two buildings.



Materials 1 and 2 are contrasting.

In addition, the proposed design is successful in allowing existing buildings adjoined to the proposed building to be featured and celebrated with an architectural design approach that allows the façade composition of existing buildings to visually stand out and be visually distinct from the new building. This is achieved through an understated architectural design of the new facades that does not mimic the design of the existing buildings. However, the new building

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façade composition carries key vertical and horizontal datum lines of the existing facades across the new facades creating a consistently articulated street wall (see page UDB pg. 27). This is a best practice for achieving design harmony between existing and new facades.

From an urban design perspective the design of the podium level represents a good approach to dealing with the existing features of the site including existing buildings, spaces, and the grading challenges. The proposal provides active uses at grade along almost the entire perimeter of the building which can help enliven the streetscapes that interface with the development. The courtyard space within the development provides an extension of the public realm into the site making this space an important public feature of the design.

Site Design and Streetscape

Along Regional Road 87, the boulevard between the road curb and the proposed building is limited in width. To create the appearance of a wider sidewalk and improve the overall appearance of the proposal and streetscape, staff suggests the following:

a. Burial of the overhead utilities.

Should this development be approved:

- i. The utilities will be impacted by the construction of the development as foundations, floors, and facades are constructed.
- ii. The constrained site area, construction of this development will most likely require redirecting the overhead utility anyway.
- iii. It is a best practice to bury utilities adjacent to tall buildings. Burial of the overheads during construction is the optimal time to do this and is far more cost effective than doing this at a later time.
- iv. Burial of the overhead utilities will substantially enhance the visual quality of the Lock Street streetscape to create a more sympathetic and sensitive approach to the much valued heritage condition being conserved.

b. Widening of the sidewalks along Lock Street.

Should this development be approved:

- i. Widening of the sidewalks will provide opportunities for sidewalk cafes, enhanced streetscaping, and street trees to create an inviting and enlivened visually pleasing sidewalk environment to complement the at grade uses of the proposed building.
- ii. This is a best practice that is realised in many places because it makes better and more valuable use of the public realm. This is usually achieved by extending the sidewalk and road curb so as to create a barrier free space that is flush with the existing sidewalks. On-street parking may be accommodated seasonally or as needed. As demonstrated below, where permitted, vehicles park on a designated space that maybe used for parking or other complementary activities such as patios. Flexibility of this on-street parking design allows the space to be used for parking in colder months of the year and patios for warmer months of the year.
- iii. This is an appropriate condition along development side of Lock Street because it allows the sidewalk to become much wider allowing the public and the

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- development to benefit from the sun exposure during afternoon and early evening hours of the summer.
- iv. It is likely that the existing sidewalk will be replaced after construction anyway due to the close proximity of the sidewalk to the property line.
 - v. Wider sidewalks contribute value by making retail units more attractive to prospective tenants keen on benefiting from the addition room for patios, display of retail merchandise etc.
 - vi. Widening of the sidewalk to accommodate street trees can contribute to visually mitigating building heights.

	
<p>King Street, Kitchener: On-street parking within a flexible space that may be used for parking or patio spaces.</p>	<p>Queen Street, Niagara Falls: On-street parking within a flexible space. Those uses that may benefit from a patio are permitted to utilize this space for patios.</p>

Should this development be approved, Regional staff request a streetscape landscape plan where this proposal abuts Regional Road 87.

Waste Management

Comments pertaining to Regional waste collection service will be provided during Site Plan review.

Conclusion

Regional Planning and Development Services staff offer no objection to the proposed Official Plan Amendment and Zoning By-law Amendment as the proposed amendments are considered to align with the intent and direction Regional and Provincial policy, represent good urban design and, generally, do not appear to have any significant impacts on Regional infrastructure.

Given the site specific nature of the application, the Official Plan Amendment is exempt from Regional approval in accordance with the Memorandum of Understanding with Area Municipalities and Regional Official Plan Policy 14.E.7.

If City staff would like to discuss these comments, please contact me at extension 3442 or Lindsay Earl, MCIP, RPP, Senior Development Planner at extension 3387.

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Please send notice of Council's decision on the Official Plan and Zoning By-law Amendments.

Best regards,



Craig Rohe, M.Pl.
Development Planner
Niagara Region

Cc: Lindsay Earl, MCIP, RPP, Senior Development Planner, Niagara Region
Rino Mostacci, MCIP, RPP, Commissioner, Planning and Development Services, Niagara Region
Susan Dunsmore, P.Eng., Development Engineer, Niagara Region
Phill Lambert, P. Eng., Associate Director, Infrastructure Planning, Niagara Region
Michael Pardo, P. Eng., Manager, Traffic Engineering, Niagara Region
Khaldoon Ahmad, FIUD, MCIP, RPP, Manager of Urban Design
Taya Devlin, Watershed Planner, Niagara Peninsula Conservation Authority
John Ariens, MCIP, RPP, Associate Director, IBI Group

Appendix – Sample Transportation Monitoring Conditions

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Appendix Sample Traffic Monitoring Conditions

Traffic Monitoring Condition:

- Traffic Monitoring is to be carried out by a qualified transportation consultant retained by the Developer. All monitoring plans and protocols shall be approved by the Region prior to commencement. The Region shall be permitted to have any proposed monitoring plan or recommendations reviewed by its own consultant as a peer review, such costs of any peer review shall be the responsibility of the Developer.
- Results and any recommended mitigation measures (including potential intersection changes) shall be forwarded to the Region for review and approval
- All recommended mitigation measures are subject to the approval by Region and shall be obtained in writing.
- All costs associated with monitoring, including the costs of any peer review requested by the Region, and all costs associated with the implementation of any mitigations measures which may be recommended shall be the sole responsibility of the Developer
- The Developer shall ensure that all monitoring conditions will be included in the Site Plan conditions
 - Traffic Monitoring will occur 12 months after substantial (90%) occupancy of site plan development
 - Targeted monitoring of identified operational issues, including but not limited to, failure of the two intersections, Main Street and Lock Street and Lock Street and Lakeport Road.
 - Operational issues can be identified by either the Region, City or Developer.

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Summary of Community Correspondence

Community Concerns	Staff Response
Traffic	
<p>Lock Street and Main Street are congested and the Main and Lock Street intersection cannot handle additional traffic. Increased traffic will cause significant delays and will compromise pedestrian safety. Traffic lights should be installed at Main and Lock.</p>	<p>The Transportation Impact Study evaluated traffic impacts on this intersection and found that it currently operates under poor conditions at times and that this condition is expected to slightly worsen. No intersection capacity improvements are recommended in the TIS as widening or signalizing this intersection is not considered to be feasible. The consultant suggests that the poor operating conditions are only seen to occur on a Saturday peak hour summer condition, which represents a relatively small number of hours during the year and is considered to be acceptable. The report notes that the additional traffic generated by the subject development would not result in significantly different conditions during the summer weekday AM and PM peak hours.</p> <p>This intersection is under the jurisdiction of Niagara Region and Regional staff have requested that a traffic monitoring plan be required to confirm the actual impact on the intersection following construction to confirm the findings of the study. The Region has also requested that the applicant evaluate the impact of converting Main Street to two-way traffic between Lock Street and Lakeside Park in order to divert traffic from Lock Street and to reduce left turn movements at the intersection of Lock and Lakeport that currently create back-ups. This may also improve pedestrian safety at the intersection by reducing turn movements onto Lock Street.</p>
<p>Concern about the methodology of the applicant's traffic study and the study may have been completed while beach was closed due to high water, impacting the findings.</p>	<p>Niagara Region has Traffic Impact Study (TIS) guidelines that are used for all TIS's submitted by developments and these studies are reviewed by Regional staff to ensure that these guidelines are adhered to.</p> <p>The original traffic study was based on counts from November of 2016 and a seasonal adjustment was used to estimate summer levels. Additional traffic counts were taken in August of 2017 to identify actual traffic levels at that time. These new actual August traffic counts generally found less traffic than was previously estimated.</p>
<p>Concern that increased traffic in the areas will worsen the ability to safely cross further up Main</p>	<p>Regional staff advise that they have completed a Pedestrian Crossing warrant study on Main Street</p>

Street and to exit driveways along Main Street.	<p>between Dalemere Crescent and Cole Farm Boulevard and currently the warrants are not met for this location.</p> <p>Regional staff also acknowledge that all Regional arterial roads carry significant traffic and therefore exiting all properties at times can be challenging.</p>
Appropriate traffic calming/pedestrian crossings should be implemented, along with restrictions on loading for commercial uses on Lock Street.	<p>No traffic calming or additional pedestrian crossings are proposed at this time.</p> <p>Regional staff have advised that details of street design along Lock Street and Lakeport Road will be addressed through the preparation of a Streetscape Plan. The Streetscape Plan will seek to coordinate street design and public realm improvements between the various development proposals.</p>
Concern that Port Dalhousie can only be accessed by Main and Lakeport, potentially compromising emergency services and bottlenecking traffic.	<p>Fire Services has not identified any concern regarding their ability to access the subject lands using the existing street network.</p>
Concern regarding the design and function of City's portion of Main Street relative to traffic flow and the ability to maintain sidewalks, parking and loading areas.	<p>Staff note that the proposed site design includes widened sidewalks along the Main Street frontage and the construction of a lay-by adjacent to the proposed residential lobby. The design and function of Main Street is still being evaluated and staff have requested that the applicant prepare additional design plans for this section of street, including a two-way traffic conversion alternative.</p>
Concern about construction traffic, which should be minimized.	<p>No construction traffic or staging information has been provided at this time. These matters will be addressed through the future site plan approval process.</p>
Parking	
Concern that Parking is already an issue in the community and that the project will worsen the situation, discouraging visitors and generating parking spillover into Lakeside Park and onto surrounding residential streets.	<p>It is expected that demand for public parking facilities will increase as a result of this development. Staff note that commercial uses previously approved for this site and in the rest of the commercial core area are exempt from the parking requirements of the zoning by-law.</p> <p>Staff acknowledge that the existing supply of public parking in the area is heavily used at time. The Port Dalhousie Secondary Plan study will include an assessment of the City's parking facilities and is expected to include recommendations for operational changes to public parking in and around the core area such as paid parking and/or time limits to help generate vehicle turnover</p>

	and better ensure availability for visitors. There are no plans to implement any operational changes at this time.
The amount of residential parking and visitor parking proposed is not sufficient.	The existing parking standard for these lands on the subject lands for condominium apartments is 1.75 spaces per unit. While not explicitly stated, that ratio is intended to include a built-in visitor parking compliment. The applicant has proposed a reduced parking rate of 1.17 (one space per unit and 0.17 visitor spaces per unit). The provided visitor parking is proposed to be shared between the condominium units and the commercial uses. Staff are still evaluating this request.
Concern about the provision of parking for future employees of the various commercial uses.	No plans or strategy for the provision of employee parking has been provided. The zoning by-law does not specifically require the provision of employee parking for the uses proposed.
Additional public parking should be constructed in Lakeside Park and/or in Renee Park.	The Port Dalhousie Secondary Plan will include an assessment of the City's parking facilities. The final document will provide specific recommendations regarding the supply and operation of public parking.
Concern about need generate turnover in public parking areas to better ensure that visitors will be able to find parking and to avoid the use public parking facilities for long term residential parking.	The Port Dalhousie Secondary Plan will include an assessment of the City's parking facilities, along with recommended operational changes in and around the core area, which may include paid parking and/or time limits to help generate vehicle turnover. Increased parking enforcement may also be warranted.
Heritage	
Concern that the proposed building scale and design is not consistent with the character, purpose or feel of the heritage district, including appropriateness of terracing design and architectural style.	The Heritage Impact Assessment submitted in support of the application concluded that the proposed building design is appropriate for the heritage district. City staff have initiated a peer review to confirm the findings of the Heritage Impact Assessment. The St. Catharines Heritage Advisory Committee will also be consulted.
Concern that the proposed height and massing would overwhelm its surroundings and adjacent heritage buildings.	The Heritage Impact Assessment submitted in support of the application concluded that the proposed building design would not overshadow the neighbouring buildings and that the terraced building design is appropriate for the heritage district. City staff have initiated a peer review to confirm the findings of the Heritage Impact Assessment.
Concern with design and treatment of old Port Mansion site.	The existing site approval required that a replica façade of the form Union/McGrath hotels be reconstructed in this location, facing Lakeport Road. A new building design for this site is now proposed.

	The HIA suggests that that the current vacancy of the Port Mansion site provides an opportunity to develop a new and sensitive building, rather than a reconstruction replicating a historical appearance. The HIA further states that the new building's impact will be positive as it expresses a contemporary interpretation of its role in articulating the street corner, with a design respectful of the established character of the surroundings. This aspect of the proposal will also be evaluated through the peer review.
Concern about how buildings will be protected during construction.	The applicant will be required to amend the Heritage Easement Agreement with the City in order to better ensure implementation of the approved conservation strategy. This agreement will include securities that will be held by the City.
Concern with proposal to build over Hogan's Alley.	The existing alignment of Hogan's Alley is now proposed to be maintained, but the proposed terraced building mass is shown as extending over a portion of Hogan's Alley, creating a roof over the proposed new staircase. Implementation of this element would require the City to relinquish air rights over this property. This aspect of the proposal will be evaluated through the peer review.
Building Height, Massing and Design	
Concern that the proposed building height, mass and architectural design is inappropriate and does not fit within the otherwise low-rise village character.	<p>The proposed structure is less tall at its highest point than the existing approvals, but is taller in other locations due to the proposed terraced design, bringing portions of the building mass closer to Lock Street, Lakeport Road and Main Street. The reports submitted in support of the application acknowledge that the building will be significantly larger than other surrounding buildings but suggest that the building form is compatible and that the impacts of height are mitigated by the proposed terraced design.</p> <p>With respect to architectural design and style, the proposed structure includes a distinct 2-3 storey base section that reflects a traditional main street character, with a more contemporary building design approach for the terraced upper floors.</p> <p>The proposed massing and design changes will be further addressed in the recommendations report.</p>
Concerns that the building will block sunlight for rest of core and surrounding area.	A shadow impact analysis was submitted as part of the application. The study shows that morning shadows will be cast to the west, across Main Street and up the north side of Lock Street. Afternoon and evening shadows move across the southerly portions of Lakeside Park and the Lakeside Park parking lot. No shadows are shown as

	<p>reaching the Lakeside Park beach, playground, carousel or lawn adjacent to the new bandshell during the spring, summer and fall. Shadow impacts are more extensive during the winter months.</p> <p>Shadow impacts are generally similar between those of the existing height permissions and those of the current proposal.</p>
Concerns that Building extension over Hogan's alley will block open views of lake and park. Tunnel also creates an uncomfortable closed-in corridor.	<p>The proposed structure bridges the north end of Hogan's Alley. The bridged element is placed on top of the proposed new stair feature and would provide an opening two-storeys in height at grade.</p> <p>The proposed massing and design changes are being evaluated by staff.</p>
Concerns that building will be visible from throughout the neighbourhood, impacting skyline and resident's access to sky views.	The submission materials suggest that the largest concentration of building mass has been placed along the Main Street frontage as this location would be least visible from within the community.
Concerns about pedestrian scale and comfort along the Main Street sidewalk.	The proposed structure includes a significant concentration of building mass adjacent to Main Street. A 2m building step back is proposed above the third floor to help mitigate height impacts. The Main Street sidewalks are proposed to be widened through this development and staff will work with the applicant to secure streetscape improvements along this frontage.
Concern that the building height/massing will create wind tunnel effect.	The applicant submitted a pedestrian level wind study. The study identified that the development is not expected to significantly influence pedestrian wind comfort over neighboring areas at grade. The various perimeter streets and Hogan's Alley are predicted to remain comfortable for walking.
Land Use and Density	
Proposal is too dense, introducing an inappropriately high concentration of dwellings into a small village community. Concerns are further compounded by anticipated future growth in harbour area.	<p>The proposal would constitute a high density development, with a residential density of 442 units per hectare. Staff note that while the number of residential dwellings within the building is proposed to increase from 80 units to 220 units, the total floor space of the project is estimated by staff to be 5-10% larger than the previous approval.</p> <p>The proposal would represent a significant infusion of dwelling units into the commercial core. Staff also note that the City's Official Plan identifies that an appropriate range and mix of medium or higher density residential housing is permitted in this area.</p>

	<p>Compatibility and the ability of supporting infrastructure to accommodate the proposed development will be considered in the formation of a staff recommendation. Projected future growth in the harbour area will also be considered in the assessment of impact and capacity.</p>
<p>Concern about need to accommodate high density development in Port Dalhousie when there are many more appropriate sites in the City.</p>	<p>The City's Official Plan provides a framework for future growth and intensification throughout the community in order to accommodate growth projections. The Plan identifies specific intensification corridors, as well as policies related to general intensification beyond these corridors, including in community commercial centres.</p> <p>The existing land use permissions on the site already permit high density residential development, though the application would result in a much higher residential density.</p>
<p>Commercial Core should function principally as an entertainment and tourism destination, rather than that a residential and shopping quarter.</p>	<p>Staff are evaluating the proposed mix of land uses but note that the street frontages are proposed to be lined with shops and restaurants. No ground floor residential dwellings area proposed.</p>
<p>Amount of commercial uses should not be reduced from previous proposal as it is needed to help draw visitors to the area.</p>	<p>The amount of floor space dedicated to shops and restaurants within the new proposal is similar to that of the old proposal. The reduction in total commercial floor area is largely related to the removal of upper floor office uses that were contemplated in the previous design. The proposed future use of the Austin House and Spice of Life buildings has also not yet been identified.</p>
<p>What is the community benefit of the project? The project needs to contribute more social amenities to the community.</p>	<p>The applicant has proposed that a community room space be provided to the City which would serve the local community. The precise nature of the use of this space has not yet been determined but could include cultural facility elements such as a library, gallery, museum or community centre.</p> <p>The applicant is also proposing to improve Hogan's Alley as a pedestrian space that would provide access to a publicly-accessible central courtyard. Streetscape improvement are also proposed around the perimeter of the project.</p>
<p>If the theatre is to be removed, it should be replaced with an alternative use that similarly supports the local cultural community. The small community room is insufficient.</p>	<p>No specific cultural facility uses are contemplated within the proposed development beyond the proposed community room. The proposal to remove the theatre use as a required project element will be further addressed in the recommendation report.</p>

Need to ensure that Hogan's Alley and Courtyard remain public space.	As a condition of approval, the City will have the ability to ensure public access to these lands through the establishment of easements and through the terms of the site plan agreement.
Concerns that public courtyard will be overtaken by restaurant patios, limiting opportunity for community use.	The proposed central courtyard is lined by commercial uses and amenity space, with associated outdoor patios contemplated within the courtyard design. Staff will consider limiting the extent of commercial encumbrances.
Concern that units may be purchased for use as short term rentals (airbnb).	Short term rental units (rentals less than 28 days) are considered hotel rooms in the City's zoning by-law and are therefore not permitted in locations where hotels are not permitted. Hotel uses, however, are permitted on the subject lands. The future condominium corporation and condo board may choose to restrict this use within the building.
Concern that increased height will set a negative precedent for rest of core.	The City is in the process of developing a secondary plan for the Port Dalhousie commercial core and harbor area and this document will identify appropriate building heights in the surrounding area.
Any storefront or lobby entrances along Main Street should be recessed due to the narrow sidewalks.	Staff will be reviewing detailed streetscape plans for Main Street which will include widened sidewalks. The wind study prepared for the application also recommends recessed entrances or the use of vestibules along this frontage.
Servicing	
Concern that the sewage system and treatment plant cannot handle the increased flows from this project and seasonal heavy rains. Concerns were also raised about whether the capacity assumptions take into account other projected growth in Port Dalhousie and up stream.	Regional Staff have identified that their existing infrastructure has sufficient capacity to serve the proposed development. The Region recently undertook a Master Servicing Plan (MSP). The Region has advised that this development falls within the sewer shed for the Lakeside Park Sewage Pumping Stations (SPS). The Lakeside Park sewer shed has been allocated growth out to 2041. The MSP has not identified any required upgrades to the Lakeside Park SPS. The SPS is scheduled for a maintenance project being designed in 2017/2018. To ensure that the station is able to handle the proposed growth in the sewer shed the development's flows have been provided to the Region's Project Manager.
Concern that sewage is already being dumped into the lake on occasion and these additional flows will worsen situation.	The proposed servicing strategy acknowledges that storm events larger than the 2-year storm may exceed the capacity of the pumping station which may result in overflow drainage discharging into the harbour. The

	<p>consultant's report states that this is an existing condition and that the additional sanitary flow is expected to have a minimal impact. The report also notes that there are potentially direct combined service connections discharging storm drainage from the existing site buildings into the combined sewer. These flows to the combined sewer would be removed in post development conditions, potentially providing additional capacity within the sewer and pumping station.</p> <p>The Region has also noted that a wet weather flow reduction program to remove wet weather flows from the sanitary system is included in the MSP and will provide additional capacity for growth.</p>
Concern about where stormwater flows and dewatering will discharge.	<p>Existing stormwater flows from the site discharge overland towards the harbour. The proposed stormwater management approach maintains this approach, controlled to pre-development flow rates. Additional flows will be held in a storage tank and discharges at a controlled rate. No information has yet been provided regarding the proposed dewatering approach.</p>
Concern about soils, water table and ability to construct parking garage.	<p>The submitted preliminary geotechnical report suggests that the proposed construction is feasible from a geotechnical engineering perspective. The study recommends specific mitigating measures and further study.</p>

Appendix 15
Community Correspondence Received to December 1st, 2017

Ritchie, Scott

From: allan yockell [REDACTED]
Sent: Thursday, September 28, 2017 9:04 AM
To: Ritchie, Scott
Subject: Port Dalhousie Union Waterfront Project

I was glad to attend the recent public information session on Sept. 27, but regret that I had to leave at 7:30 for other commitments. I will begin by saying that I am a new resident to Port, but a lifelong resident of Niagara remembering it's better days. I will go on to say that I **support** this development given the consideration and respect for traffic, parking and the area's heritage. To me this project is a no brainer and the residents of Port should be grateful that the Fortress group will make Port a crown jewel in Niagara. Currently, the North side of Lock St looks something like war torn Beirut. It's time this mess finally gets cleaned up...its time to turn the page on its stagnant recent past. With new residents living in the village, this area will have a pulse...why are some afraid of that?? There are a number of examples around North America of smaller cities that have managed to develop their waterfronts...nearby Cleveland, Pittsburgh, Buffalo, Burlington and Kingston all come to mind. Union Waterfront will look fantastic! Think of what Port could become already having...a world class sporting stage, 100 year old/nickel a ride carousel, a great park made famous in a RUSH song, a great lake that offers sunsets, a marina and yacht club. Most cities would love to have those things to build around. The best decisions are never made based on emotional reactions. It's time to allow the new Port to prosper and reach it's potential. Hopefully this will happen sooner rather than later.

Allan Yockell
 [REDACTED]
 [REDACTED]
 [REDACTED]

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September 27, 2017

Private & Confidential

To City of St. Catharines,

As a business owner operating in Port Dalhousie since 2015 we wanted to send this letter of support for the Union Waterfront development proposal.

To start, we believe Balzac's Coffee has been a perfect fit for Port. Our brand works very well in Heritage areas and as result we have nicely meshed with the local community. That being said, opening in an area with the uncertainty of Port was and is considered a gamble. For us, gambles are nothing new, we have had several successful Café openings in neighborhoods early in their redevelopment phases. Two prime examples in this regard for us quickly come to mind:

- Toronto Distillery District – in 2002 Balzac's, along with Mill Street Brewery, were the first two businesses in this redeveloping neighborhood.
- Toronto Liberty Village – in 2006 Balzac's was one of the first businesses to open in the early stages of this neighborhood. We took a corner location in the successful restoration of the old Irwin Toy Factory.

These are but two of our thirteen locations. In each of those location decisions, as a small privately owned company, we had to take gambles in order to secure the right leases in locations we deemed would one day be spectacular. Today any coffee company would jump to have those locations. Certainly Port is not Toronto, and our expectations are thus muted appropriately in that regard, but we see similar potential from what is there today and what can and will be there in the future.

Although our brand fits well into existing communities such as Port, development surrounding us is the only assurance of ongoing success. Port has seen too many businesses close, others simply miss out on their potential. With the amazing Waterfront, Lakeside Park, and the current Heritage buildings, this new Development compliments all very well. We like that it maintains Hogan's Alley, the Jail, and sightlines from street level. We are not concerned about an increase in traffic or parking, though we believe parking enforcement (ie. parking meters) will be critical.

Lastly, we would like to add that there is another improvement development will bring. Balzac's, like many existing businesses in Port, have heritage designation which brings challenges to those many customers with accessibility issues. This makes it difficult to keep all customers happy due to our building limitation realities such as narrow doorways, stairways, washroom size, etc. We cannot change our situation in this regard (it would be cheaper to move and rent another location...and we don't want to) so it is important that other businesses compliment those with Heritage designation. In NOTL we are proudly located beside Town provided accessible washrooms, in Port, our friendly neighbour and competitor in Tim Horton's had a wonderfully accessible business. The new development will be modern, AODA perfection, this helps preserve our ability to thrive with Heritage designation.

Balzac's Coffee Ltd.
675 Tradewind Drive, Unit 4
Ancaster, ON., L9G 4V5

Phone: (905) 662-3500 Fax: (905) 662-3533

www.balzacs.com

In closing, Balzac's is a proud supporter of the Fortress Union Waterfront Project and we wish them every success.

Sincerely,

Steve Janzen

Steve Janzen | [REDACTED]

[REDACTED]

[REDACTED]



Balzac's Coffee Ltd.
675 Tradewind Drive, Unit 4
Ancaster, ON., L9G 4V5

Phone: (905) 662-3500 Fax: (905) 662-3533
www.balzacs.com

Ritchie, Scott

From: Barb Guembel [REDACTED]
Sent: Tuesday, September 26, 2017 3:34 PM
To: Mayor & Council; Ritchie, Scott; Riddell, Jim
Subject: Union Waterfront Project a HUGE GO!

Dear Council, Mayor, City Staff,

I am writing this with an enormous amount of excitement and pride as well as concern and worry. I am a business (Trysport Niagara) and property owner in Port Dalhousie, and I feel the need to voice my opinion on this amazing opportunity for Port Dalhousie.

I am a first generation immigrant, a resident of Niagara my whole Canadian Citizen life and an active individual in the Port Dalhousie area for over 20 years. My family came to Canada for more opportunities, a more progressive society, and a better future for the kids. And this is what I now want for my kids, and the community. The Union Waterfront Project is a key element in future opportunities not just for my kids but for all kids and adults in the Niagara Region and beyond.

When we first arrived to Canada, it was a difficult transition and in many ways it still is and as a first generation immigrant it may be for the rest of my life. One of the thing that made the transition easier for me was the opportunity to be part of the Rowing Community in Port Dalhousie, this was truly the first time I felt at home, right here in Port Dalhousie. Where now I settled down a business and want a future for my family. I rowed for over 10 years, first with the St.Cathariens Rowing club, then Brock University (as I studied Community Health and a Masters in Leisure, Social Capital and Health), winning countless Henley GOLDS and contributing to the community through various volunteering hours and coaching younger athletes. I have seen Port Dalhousie thriving and thus this is so hard to watch.

The proposed development by the Fortress Development group, seems to me like an opportunity of a lifetime for Port Dalhousie. Fortress have carefully crafted a plan that preserves heritage, while allowing Port Dalhousie a chance to thrive and stand out among other waterfront communities such as Niagara-on-the-Lake and Burlington. Providing an amazing community to live, dine, play, and grow in... isn't this what we want for our families and our kids and grandkids.

The current status of the heritage buildings is a complete shame, shame on the community sub groups who lobby against the project and current city councillors who continue to do everything to prevent Port Dalhousie from truly thriving. Without jobs, entertainment and living space, there is no Heritage. Heritage is only viable when people have a place to come to. Right now the heritage of Port Dalhousie is lost because people in the Niagara Region and surrounding communities BY-PASS US. Heritage cannot live without people.

With the Union Waterfront built, I see a thriving Port Dalhousie core, people from far and near enjoying and bringing back to life the heritage and the beauty that Port Dalhousie has to offer, ALL YEAR LONG.

I sincerely hope that this message is not lost but HEARD, and that Port Dalhousie comes back to life because WE SUPPORT FORTRESS AND THE UNION WATERFRONT PROJECT, an outside developer who also wants to see Port Dalhousie THRIVE, now and for future generations.

Thank you

Barb (Szybka) Guembel

[REDACTED]

--

Barb Guembel, M.A., B.A. Co-Owner Trysport Niagara & Aktiv Life

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Ritchie, Scott

From: Brian Bowman [REDACTED]
Sent: Thursday, September 28, 2017 2:16 PM
To: Ritchie, Scott
Subject: 16 Lock Street Union Waterfront

Hello Mr. Ritchie,

I attended the public information session last night at the Port Dalhousie Lions Club.

Although I am all for the new proposal, I agree with many of the residents that there will be problems with the traffic/parking.

I have three suggestions I'd like to make; I think they could alleviate this potential problem drastically.

- 1) Install traffic lights at the corner of Main & Lock Streets. This would control the inevitable increase in pedestrian *and* auto traffic. Two cars would move through the intersection as opposed to one at a time now.
- 2) 2/3 level parking garage to be built at Lakeside Park where the existing parking lot is now. This would be Municipal paid parking for visitors or for events. It could also be used for the overflow from the condos where a condo owner could pay a monthly fee for a permanent spot.
- 3) Gravel parking lot to be completed at Henley Regatta finishline near walkway to Jaycee Park. The City could level this eyesore off and fence it (this has been sitting vacant for 20 years). Again, the City could charge for parking and perhaps include a shuttle service to the centre of Port during peak months.

Thank you.

--

Brian Bowman

[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

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Ritchie, Scott

From: Bunty.hamilton [REDACTED]
Sent: Friday, September 29, 2017 10:59 AM
To: Ritchie, Scott
Subject: Union Waterfront Development

I was unfortunately unable to attend the meeting on Wednesday 27 September at the Lions Club but I have been told by the Port BWC that I can express my opinions through this email.

I have been told that traffic flow was a concern for most citizens at the meeting. I have two concerns. The first is the size of the building. Its far too big! It should not be any higher than the 3 storeys of Austin House. The front section of their plan is lovely. However, from Lock Street, there should be nothing protruding upwards behind. We are not the big city, we do not need elephant sized buildings in our core blocking the light to the existing town.

And secondly, there should be adequate parking spaces within the complex to meet all the needs of the condo owners and tenants as is done in cities. Port Dalhousie does not have space for extra parking. We have people on Dalhousie Street in homes without driveways, they need the street parking. There should be more parking allocated to the larger condo units. Two and three bedroom condos will be occupied by at least two adults and whether you want to believe it or not, most adults drive and own cars. Each unit needs to be allowed 2 spaces as well as requiring spaces for the shoppers in the stores. They can charge a fee for parking to non residents to recoup some income, plus, they will be charging probably \$10,000 upwards extra for each parking space bought. That's not a bad return for a space which is about 100 square feet.

Thank you for allowing me to submit my concerns.
 Bunty and George Hamilton

[REDACTED]
 [REDACTED]
 [REDACTED]

Sent from my iPad

Ritchie, Scott

From: Chris Kim [REDACTED]
Sent: Monday, October 02, 2017 4:58 PM
To: Ritchie, Scott
Subject: 16 Lock -Union waterfront zoning by-law amendment and OP amendment

Hello,

My name is Chris Kim and i wanted to comment on the proposed amendment for the above noted address in Port Dalhousie.

I have been a resident of St. Catharines for about three years now and have wanted to comment on the direction the city has been making in terms of the development of the lands,as presented on September 27th.

First and foremost, i'm going to say you have made great strides in improving the design and direction of the development at the site compared to the previous approval and I personally think it will only be beneficial to the residents of Port Dalhousie and St. Catharines. I really do appreciate that there is a balance between making the site a place for locals to live and potentially purchase a property and enjoy as well as making the area more attractive to non-locals visiting the area. Personally, it's been an eyesore to see the development just sitting empty and it was reassuring that there really has been a lot of thought put into the new development proposal and the direction moving forward.

I've always looked at Port Dalhousie as having all this untapped potential and opportunity and i'm glad to see something is really being done about it..

I'm putting forward my support for the development!

Sincerely,

Chris Kim
St. Catharines resident
[REDACTED]
[REDACTED]

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Ritchie, Scott

From: Chris McLaughlin [REDACTED]
Sent: Thursday, September 28, 2017 12:10 PM
To: Ritchie, Scott
Subject: Comments following Sept. 27
Attachments: scan0054.pdf

Please consider this attachment to be my comments regarding the proposed Union Waterfront development. Would appreciate acknowledgement of receipt.

Chris McLaughlin
[REDACTED]



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PUBLIC INFORMATION SESSION

September 27, 2017-09

16 Lock St. – Union Waterfront

Zoning By-law Amendment and Official Plan

Amendment

COMMENT SHEET

Name: Chris McLaughlin

Email: [REDACTED]@ca

Since PARKING and TRAFFIC in the downtown of Port Dalhousie are two of the VERY SERIOUS concerns of the current residents, I offer three suggestions:

- 1) That City of St Catharines revise parking requirements for the new construction to require the additional construction of another floor, or P4, to consist solely of PUBLIC parking. This will offset condo owners' visitors, and assist the public to continue to have access to Port and Lakeside Park.
- 2) That the REGION change parking rules on Lock and Main Streets to permit commercial vehicles and deliveries (beer, food service, etc) ONLY between the hours of 10PM-10AM, thus keeping Lock and Main Streets open in all lanes during normal business hours.
- 3) That the Business Association, newly enriched by this tremendous influx of visitors, residents and property owners, absorb the cost on a continuing basis of POLICE traffic direction in the Lock and Main area, particularly at the TRAFFIC SIGNAL, to promote better traffic flow and fewer backups on busy weekends.

Thus, all parties concerned (current residents, developer, and business association) will sacrifice something in the spirit of working together to keep a vital Port Dalhousie accessible to all.

Ritchie, Scott

From: Darren DeCaire <[REDACTED]>
Sent: Friday, September 29, 2017 7:51 AM
To: Ritchie, Scott
Subject: Port Dalhousie Development

Hello,

Although I am not a resident of Port Dalhousie, I am a resident of St. Catharines (have been for 48 years), I would like to add a few comments in regards to the development of Port Dalhousie.

I am all for businesses in Niagara thriving and making a profit, creates employment for Niagara residents which puts money back into the local economy. I would mind seeing a greater economic boom in Port Dalhousie, it is a great place to visit, enjoy the surroundings and entertainment, socialize and can be a great draw for tourism. Having said that, I am not apposed to putting up several restaurants, bars, pubs and a hotel. I know the idea of a hotel is now not in the plans, but I see the area more of an entertainment district than a living and shopping district. I know there are current shop owners in Port, how they make a go of it beyond the summer months I'll never know. As I stated above, I have lived in St. Catharines for 48 years and I never think of going to Port to do some shopping. I never have and I never will. I think this is the mindset of most residents in St. Catharines. I have had 4 kids go thru several rowing programs at the Henley and I know how busy it gets in Port. Regardless of what your studys show (anyone wanting to develop land and make a large profit will make the study say whatever they want it to say), there is no way Main and Lock area can handle any type of increase in traffic. The tight narrow turns, stop signs and traffic lights plus the fact the roads are land locked and will not allow for widening.

Having said this. I don't think Port could handle the development of a 220 unit condo and the influx of people and traffic that would bring. I think a better option is to eliminate the shops, replace with a few more restaurants and pubs/bars (have a few plays to accommodate live bands). A hotel would also be a great addition for the entertainment district but even this would be questionable whether the area could handle the increase in traffic.

Thanks for your time, I hope the right decisions are made for Port Dalhousie and not just for someone's pocket book.

Cheers!!
 Darren DeCaire

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Ritchie, Scott

From: Darryl Austin [REDACTED]
Sent: Wednesday, September 27, 2017 7:55 AM
To: Ritchie, Scott
Cc: Riddell, Jim; "[REDACTED]"
Subject: union waterfront approval

I just wanted to take some time to express the importance of the proposed plan to build the union waterfront condo complex. As a multiple business owner and property owner in port Dalhousie (Ausmosis Enterprises Inc. Surf shop) to name one, and lifelong resident i believe it is vital to the survival of small business in the core and the overall province wide view of port Dalhousie. We see the tourist first hand, and it is sad to hear the negative comments on our little" potentially ridden little port town". I believe it is time now not 10 yrs from now, for the city advisors officials to take a stand and allow/guide/befriend, "with expedience" the developers to build this project and bring a close to this drama. We have willing developers to do the job, so lets act on it and get it done... i am not saying this with no voice, I have put my money where my mouth is, and invested heavily into the downtown core with beautiful results, it was hell at times just for the record, but we prevailed and have been awarded many compliments and accolades from the public, funny how the nay Sayers totally love it now!! This is our home our work place and playground lets enhance the experience for our future guests and residents. For the sake of the business owners and true believers in progress enable these guys to build the dream.

Thanks So much for your time Darryl and Sheri Austin

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Ritchie, Scott

From: Debra Chabot [REDACTED]
Sent: Friday, September 29, 2017 6:34 PM
To: Ritchie, Scott
Subject: Union Waterfront Development Proposal

As new residents of St. Catharines in the Port Dalhousie area, we are very concerned about the proposed development, specifically the changes in plans proposed by Fortress. The number of units proposed and the parking at 1.0 spaces per unit is just not realistic . 1.75 parking spaces with additional visitor parking needs to be the minimum. We agree that something needs to happen down in that area but given this proposal in addition to those at the legion and textile factory , this is definitely a disaster just waiting to happen. Traffic and parking problems are probably only the tip of the iceberg. What about water and sewer? We have already seen problems along Main Street due to the many houses that have been infilled into the small openings along this street.

Only living here less than a year , we have seen 3 homes and one to soon be started go up on lots smaller than the city's official plan and zoning bylaws in this block alone, and two further down the street so we have no doubt that the Union Waterfront Development Proposal will do through with only minimal tweaks, but we urge the city to be more forward thinking than that.

The city has done a great job with Lakeside Park but I am sure it will get little use if the people of southern Ontario and American tourists aren't able to find parking when the residents of Union Waterfront are parking in the park lot and all along Main Street. I also know that with only one way in and one way out of our downtown area, we and others are not willing to fight the increased traffic. We'll take our shopping and restaurant dollars out the other way and head down the service road to Jordan and Grimsby.

This proposal is definitely not in the best interest of the residents of Port Dalhousie nor the city of St. Catharines,

Debra and Richard Chabot
 Sent from my IPAD

Ritchie, Scott

From: Diane Hockey [REDACTED]
Sent: Friday, September 29, 2017 1:24 PM
To: Ritchie, Scott
Subject: Union Proposal

Hi

I was at most of the meeting and heard people's concerns with traffic etc. I am in favour of the project though not that excited about the look of the condo.

My only concern is what do the new owners and developers going to different than the previous group that will ensure enough sales so that construction will begin and generate enough sales to complete the project.

Joe Hockey

Ritchie, Scott

From: Don Davidson <[REDACTED]>
Sent: Friday, September 29, 2017 8:07 AM
To: Ritchie, Scott
Subject: 16 LOCK ST- UNION WATERFRONT

Dear Mr. Ritchie:

As one of the many attendees at the information session on Sept. 27, 2017 and a resident of Port Dalhousie, I would like to echo some comments made by many others.

Firstly, both my wife Jill and I are very much in support of getting on with a project along the lines of the one proposed. The large vacant hole and overgrown weed patch which have existed for far too long represent an ongoing visual embarrassment to not only those living in The Port but to all citizens of St. Catharines.

Like many others, our only real concerns have to do with traffic control and to a lesser extent parking. In specific,

1. The "Main St. section leading out of the main parking area for Lakeside Park" appears destined to become a major bottleneck with that being the main in / out for the 220 condo's, as well as Gary Rd being the only exit / entrance for the new condo's at Dalhousie and Lock and this is of course the only exit for anyone visiting Lakeshore Park / the beach / the Carousel / the pier.
2. - The "Lock St. section between Main St. and Lakeport Rd." appears to be the other critical section to examine / effectively deal with.

If reasonable plans are laid out for these two key areas, I and I believe many others, would welcome a speedy "getting on with the project" which is essential to the long overdue revitalization of Port Dalhousie.

While I am referencing only the 220 condo project, my comments are meant to also cover whatever other projects (Lincoln Fabrics building, the Legion, condo development at end of Gary Rd. etc) that the City of St. Catharines approves or contemplates approving in the near term.

Don & Jill Davidson
 [REDACTED]
 [REDACTED]
 [REDACTED]

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Ritchie, Scott

From: DJ Conradi [REDACTED]
Sent: Friday, September 29, 2017 11:22 AM
To: Ritchie, Scott
Subject: Waterfront Port Dalhousie

In my opinion & without hysteria ... the proposed development is too large, sprawling as is does along Lock Street & rising so high behind historic old Front Street [Lakeport Road]. The buildings overpower their neighbours, they block the sky & they obscure the natural beauty of the harbour area. The development does not "fit" in any way with the historic village. Lakeside Park is diminished & disappears, if not in actual fact, then in the feeling of smallness that the towering buildings convey. In fact, everything else in the surrounding village is reduced to a "smallness" that seems wrong. I know that my eye for beauty is not a professional one but I also know that balance matters in all things. This development brings a huge imbalance to Port Dalhousie ... it looks wrong, it feels wrong, it is wrong. Please notice I do not speak about parking & traffic & all the things that might worry many reasonable people. I do not think that the voices of a few local & non-resident business owners should be heard above the voices of people who live here. Port Dalhousie will become one more over-developed space along Lake Ontario's shoreline Grimsby, Burlington, Toronto ...

Please add my voice to those who oppose this proposed development.

Donna Conradi

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Dear Mr. Sendzik,

I have never written to a politician before, but after we spoke at the presentation of the proposed condominium building design in Port Dalhousie a few weeks ago, your question of how I see Port Dalhousie developing has stayed with me, and so I decided to write.

To give you some background, my husband and I have lived in Port since 1985. We raised our children here and are so grateful for the gift of having been able to live here in Port over the years.

Port Dalhousie is a jewel, not just as a part of St. Catharines but for the Niagara Region, this province and beyond. It is a rarity to find such a small village perched on a small peninsula on the shores of one of the great lakes. It is both a quiet community and yet full of life that offers its residents peace and tranquility. It's a refuge from big city life and yet full of life with rowers, wonderful schools, safe streets to walk and bike along, small beaches accessible to everyone and filled year-round with both young and old. I do not feel that I need to tell you all of the beauty of this area — it's well documented in travel and tourism brochures, and goes without saying. Everyone we have met who has moved away from Port says they regret it. We have met people in Lakeside Park who drive several times a week from Burlington to enjoy the quiet waterfront here. When we walk or bike through Port Dalhousie with guests who visit us, they are all so impressed with our community (and we have many foreign visitors from other parts of Canada and even Europe). We once met a couple who took a year to travel across Canada in a camper, and stayed communities across the country to decide where they wanted to live. They chose Port Dalhousie.

We are taking this for granted. The people who live in this community made a deliberate choice to live here for its small village feel and, most importantly, the people who together make this great community that is a part of this beautiful city. It is one of the most sought after areas to live in within this region.

Port has changed over the years, much due to the issues in the core of the town and the park. We have seen the town change when the city allowed the bars to grow to excess, allowing thousands of youth to come and drink, which created many unpleasant situations in the area. Then the developers moved in to further create the embarrassment of what now exists in the Lock and Lakeport area. Change must happen and some new development must now occur — but not a 17-storey tower, that will be taller than the Brock University tower! This building belongs in a city, not a small port town. When one looks at a topical map of the size of Port Dalhousie, this proposal becomes even more incredulous.

We have travelled much of Canada as well as other parts of the world, and we have never seen anything like this. Small villages and towns exist and thrive all across Ontario, Canada and Europe, without anything like this. I cannot imagine the

communities that we have visited even considering such a proposal. This is not Toronto. A friend who sits on the planning board for Ontario Place has told me there will not be condominiums there, as it is a place for the people. So if that is not happening on the hundreds of acres of Ontario Place, why would you consider that for this small community?

At the heart of Port is Lakeside Park, truly a treasure in the Niagara Region. This park belongs to the people — from the city, the region, southern Ontario and anyone else that chooses to visit here. Everything that is developed here should be for the people — for families to enjoy and appreciate. If the tower is built, we are allowing people from outside our community to come in and tell us what we need.

We recognize that Lincoln Fabrics will become a condominium or something similar. We do not yet know what will become of the Legion. We already have a six story building on the edge of the park under construction that even firefighters are wondering about accessing. I envisage the Lakeside Park area remaining an area for the people, with shops designed around perhaps a small 4-5 storey apartment or condominium complex. The park area has improved greatly already, and further ideas can be created to encourage further use in the winter, perhaps including the skating pad that has previously been proposed. The pier has always been a huge attraction for people and once it's repaired — plus with creative development of small shops, restaurants and offices — the area will bring more than enough people. If a small community like Bayfield on Lake Huron can survive with one main street, surely we can build a beautiful progressive area in Lakeside without a condominium complex geared to the wealthy.

It does not take traffic studies to know the huge issues that will occur. This construction is about money, and there is no consideration for the people of the area. There were numerous things said at the presentation that were quite insulting to me as a resident in this area:

- The population density will increase dramatically.
- Vehicular traffic will increase dramatically (it does not take traffic studies to know that this will be an issue; it is common sense). This will significantly affect the safety in the core area of Port.
- The building will cast incredible shadows.
- Parking in Port will be dramatically reduced, and there was talk at the meeting of bringing in pay parking. Again, this is one of the beauties of the area: all people and families can come and enjoy the area without paying for parking. Certainly the people in the tower will be able to afford it and take up the spaces.
- The infrastructure to handle the gas, water, electricity, sewers, etc. does not exist in this small peninsula.
- In light of the recent changes in weather patterns including earthquakes (and yes we do live on a fault line), hurricanes and the flooding in Port, there has

been much discussion about the Federal, Provincial and ultimately Municipal governments not allowing construction of homes and other buildings on low lying areas so close to water.

- The sandy loam soil of this peninsula has slowly been eroding in past years. How will the construction deal with this and all the water below the development, regardless of how well they try to insulate it?

Mr. Sendzik, we feel strongly that you have done a lot of wonderful things for the City of St. Catharines and you have represented the city beautifully. This cannot be a development about money and gaining taxes, which is what it appears to be at the moment. It is not just about what is morally and ethically right to do — it is about simple common sense. We hope that common sense will prevail in your and councils decisions about this development.

I encourage you to call a meeting of the residents of Port Dalhousie and St. Catharines, and ask the people for ideas of how they see Port Dalhousie developing. I'm sure I speak for many of us when I say we hope to keep our historic community intact, without a proposal that's better suited for the GTA with 6.5 million people — not 650 residences!

Ritchie, Scott

From: Dorota Parafianowicz [REDACTED]
Sent: Thursday, October 12, 2017 6:49 PM
To: Ritchie, Scott
Subject: Port Dalhousie Development Proposal
Attachments: letter to Mayor Sendzik #2.docx

Follow Up Flag: Follow up
Flag Status: Flagged

Dear Mr. Ritchie - First I would like to thank you for speaking with me and my husband at the last meeting about the Port development. You had asked that I send you a copy of the letter that I dropped off at the mayor's office and I have attached that below (for which I have not received any reply). My apologies for the delay in forwarding it to you.

Pursuant to that letter and attending the meeting, I do have a few further comments and questions. I would like to say first that I am not opposed to development. Everyone agrees that that is required in Port Dalhousie. However, I feel strongly that this development is too large from multiple perspectives for this small Port village. My comments/questions are as follows:

1. How many of the individuals from The Fortress Group, Union Waterfront, the City, those wanting "more traffic", and anyone else involved in the decision making process of this development actually live in Port Dalhousie? We are allowing a group from a city of 6.5 million people try to tell people in Port what is best for them. The slogan of the Fortress Group, "the greatest thing we create are communities", certainly does not apply here as Port Dalhousie is already a community and their failure to recognize that speaks volumes.
2. When the question was asked at the meeting about the reputation of The Fortress Group and the McCleans article about them, Mr. Ridell quickly shut down that discussion saying that we were not there to discuss the merits of Fortress but those of the development. A very simple Google search clearly indicates significant and multiple concerns about this group. To not respond to the question was completely inappropriate. The development is the developer and the developer is the development.
3. It is said that the tower is 14 stories. Does that include the levels on the top that house the heating/cooling/mechanical systems?
4. In response to many questions, Mr. Travis Nolan and others kept referring to and commenting on the previous proposal. That is completely irrelevant now. We are looking at the new proposal and not the old. An attempt is being made to try to sell this proposal by saying how much better it is than the last one. I see this again as demeaning to the residents.
5. Just because the OMB (which I understand is being disbanded or restructured because of its constant issues and siding with developers) stated that a tower of 17 stories can be built in Port does not mean that that is the right thing to do. Your hands are not tied as

was stated that evening. The simple response is no. This building is too big and does not fit into the character and life of Port Dalhousie.

6. Perhaps most importantly, I would like to reiterate the environmental concerns that I stated in the letter to the mayor. We recently attended a presentation here in St. Catharines by Mr. Dave Phillips, Meteorologist with Environment Canada who again stated the need to stop building in low lying flood possible areas. Who will be responsible should flooding occur not just from rising water levels as we experienced this past summer but also from the water that lies in the sand bar area of Lakeside Park.

6. It is unfortunate that the the mayor and councillors who were present at the beginning of the meeting left prior to hearing all of the final comments and questions of people at the meeting. It took some time for people to be brave enough to stand up and express their deeper concerns.

Thank you for taking the time to read my letter.

Sincerely, Dorota

cc. Carlos Garcia
Mayor Sendzik

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Ritchie, Scott

From: Ed Smith <[REDACTED]>
Sent: Friday, September 29, 2017 4:51 PM
To: Ritchie, Scott
Subject: Re: Union Waterfront

One more question for you Scott, it was mentioned that all the documents are available to the public, I would like to get a copy of the traffic and parking studies if you don't mind?

Thanks Scott

Ed

From: Ed Smith [REDACTED]
To: Scott Ritchie <sritchie@stcatharines.ca>
Sent: Friday, September 29, 2017 4:42 PM
Subject: Union Waterfront

Hi Scott,

I am wondering why the citizens are forced to rely on the report as presented by the proponents engineers as far as traffic studies go? That can not be considered independent and therefore is subject to all kinds of challenge...is there a process whereby the city would fund such a study from an external source?

Thanks,

Ed

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Ritchie, Scott

From: Ritchie, Scott
Sent: Monday, October 02, 2017 11:33 AM
To: 'Ed Smith'
Subject: RE: Union Waterfront

Hi Ed,

In some cases, where the City has concerns but may not have expertise in house, we can require a peer review. We are still reviewing the submission materials but may require a peer review of some aspects of this development application. In these cases, the developer would be required to pay for the cost of a peer review, but would not be involved in selecting the reviewer. We'll be working closely with our colleagues at Niagara Region to evaluate the projected traffic impacts.

From: Ed Smith [REDACTED]
Sent: Friday, September 29, 2017 4:42 PM
To: Ritchie, Scott <sritchie@stcatharines.ca>
Subject: Union Waterfront

Hi Scott,

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Ritchie, Scott

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Sent: Friday, September 29, 2017 4:42 PM
To: Ritchie, Scott
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Thanks,

Ed

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Ritchie, Scott

From: Ritchie, Scott
Sent: Monday, October 02, 2017 2:56 PM
To: 'Ed Smith'
Subject: RE: Union Waterfront

Hi Ed,

A traffic impact study was not required for the project on Dalhousie Avenue. In understand that a traffic study was not required due to that project's limited scope (maximum 16 units).

From: Ed Smith [REDACTED]
Sent: Monday, October 02, 2017 12:36 PM
To: Ritchie, Scott <sritchie@stcatharines.ca>
Subject: Re: Union Waterfront

Hey Scott...would I be able to get the same report that was done for the condo that is being built right now on Lakeside Park...The Beaches?

Thanks,

Ed

From: Ed Smith [REDACTED]
Sent: Monday, October 2, 2017 12:23 PM
Subject: Re: Union Waterfront

Thanks for that Scott

From: "Ritchie, Scott" <sritchie@stcatharines.ca>
To: Ed Smith [REDACTED]
Sent: Monday, October 2, 2017 11:35 AM
Subject: RE: Union Waterfront

Hi Ed,

I've attached a copy of the transportation study that was submitted with their application.

Scott Ritchie M.C.I.P., R.P.P.
 Urban Design Planner
Email: sritchie@stcatharines.ca **Visit:** www.stcatharines.ca
Phone: 905.688.5601 x1722 **TTY:** 905.688.4TTY (4889)
Mail: PO Box 3012, 50 Church Street, St. Catharines, ON L2R 7C2



From: Ed Smith [REDACTED]
Sent: Friday, September 29, 2017 4:51 PM
To: Ritchie, Scott <sritchie@stcatharines.ca>
Subject: Re: Union Waterfront

One more question for you Scott, it was mentioned that all the documents are available to the public, I would like to get a copy of the traffic and parking studies if you don't mind?

Thanks Scott

Ed

From: Ed Smith [REDACTED]
To: Scott Ritchie <sritchie@stcatharines.ca>
Sent: Friday, September 29, 2017 4:42 PM
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Ed

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Ritchie, Scott

From: [REDACTED]
Sent: Tuesday, October 17, 2017 6:35 PM
To: Ritchie, Scott
Subject: Union Waterfront Plan

What I would like to say about the Union Waterfront Development - how inappropriate it is for a heritage canal village.

My most urgent concern is for the traffic in and out of Port Dalhousie. Any special events in Port Dalhousie, accidents on those two lane roads in and out, or on the QEW (where traffic is diverted through Port Dalhousie) have already demonstrated a problem for access and movement including emergency vehicles into the area. The increase of 220 residential units plus retail traffic will do nothing but compound this problem if not make it a daily occurrence at peak periods.

This congestion will create an inconvenience at the least and most probably a dangerous scenario if emergency vehicles cannot make their way easily into the community.

For this reason I am opposed to the Union Waterfront Plan.

Eleanor Lancaster

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Ritchie, Scott

From: Elspeth Cameron [REDACTED]
Sent: Friday, October 13, 2017 12:50 PM
To: Ritchie, Scott
Cc: N. Carlos Garcia
Subject: Fwd: The proposed Fortress condo building in Port Dalhousie

Elspeth

Begin forwarded message:

From: Elspeth Cameron [REDACTED]
Date: October 13, 2017 at 12:47:07 PM EDT
To: sritchie@stcargarines.com
Subject: The proposed Fortress condo building in Port Dalhousie

I am strongly opposed to this project.

It is far too big for its location. It looks like a brutal pile of cubes, even though the architectural drawings are carefully positioned to minimize the huge ugly building.

I am concerned about traffic. I attended the meeting at the Lions Club on Main Street and saw in particular plans for the part of Main St that would run one way past the northwest side of the project. It was explained that deliveries, entrance by 250 plus tenants to and from the underground parking. That section of Main St is not wide enough to handle the traffic this would involve. It is a Lane and a half wide, effectively one lane. Furthermore the traffic tie-ups at the four way stop intersection would inevitably become clogged with traffic. This is especially likely since there will be heavy traffic from the condo on the corner of Dalhousie St that is approximately half finished. I think great pressure will be put on the City to widen roads and the bridge across Martindale Pond. That is a great expense the city should consider carefully before approving this current proposal. The traffic on Main Street is also likely to become clogged with so many more drivers in the community.

I also believe that at least some, and possibly many, units will be used as Airbnb (or other business) short-term rentals. This would destabilize a community that is currently stable and safe.

The previous proscers of a project called Port Place discovered that it would be essential to have special walls to keep water from the underground parking levels. I believe that there would be overflow parking in the current parking lot at Lakeside Park. That lot is full on some weekends and during special events. The City would definitely need to patrol this parking lot and possibly install formal parking such as meters or Pay per Hour machines. This, too, would be costly for the city.

Please, please do not approve this project.

Elspeth Cameron -

[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

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Ritchie, Scott

From: Eve Silver [REDACTED]
Sent: Friday, September 29, 2017 5:17 PM
To: Ritchie, Scott
Subject: 16 Lock Street - Union Waterfront

Dear Mr. Ritchie,

I write in regard to 16 Lock Street - Union Waterfront, Zoning By-law Amendment, and Official Plan Amendment. I attended the meeting Wednesday night. My comments here are a follow-up, a hope that my voice will be heard.

I am in favour of a development on the lands in question. I believe a development will be beneficial to the Port Dalhousie business community and, possibly, to the community as a whole. That said, I have numerous concerns regarding the proposed development plan.

Size: The proposed building is visually large and dense, not in keeping with the current community or even St. Catharines as a whole which boasts few tall buildings, let alone one in a heritage district. The size of the building is not only visually unappealing, reminiscent of the downtown core of a large city, but the density is problematic. Port Dalhousie is a small neighbourhood with several hundred residences. This project proposes to add an additional 220 residences to this neighbourhood. And it is not alone. There is current construction of a condominium that will add 17 residences, and future plans for two additional buildings at the Legion site and the former factory site. There is potential here to more than double the number of residences in Port Dalhousie in a small period of time.

Appearance: The design of the building is quite modern. Though the proposal includes renovation and refurbishment of existing streetscape, the condominium building itself is of modern design with no element in keeping with the heritage feel of Port Dalhousie. The building will loom 14 storeys—50 metres, 164 feet—over small one and two storey homes. It will be a tower, a mountain, a presence that will shadow the entire area. Where those walking through Port or through the park or even across the bridge now enjoy endless sky, they will be confronted by a mass of grey blocking the sun.

Shadow: The topic of shadow was raised at the meeting and the response was that there would be some shadow to the property at the corner of Lock and Main in the mornings. I cannot help but feel that the reply underplayed the impact. In winter, the shadow will stretch, affecting pedestrians. The shadow will throw trees that currently enjoy and flourish in the sun into shade. How will that increased shade affect the trees and the old growth canopy? I have concern that the effect will be adverse.

Community: Port Dalhousie is a small and welcoming community. Neighbours know each other by face if not by name and there is a feeling of caring. I have concern that not all those purchasing units in this massive building will be doing so for personal residences. I worry that many who purchase will plan to rent out their units as short-term rentals (airbnb) and the turnover will not only affect the sense of community this area enjoys, but have an unpleasant impact on this community.

Infrastructure: In spring 2017, Niagara's sewage treatment plants spilled one billion litres of diluted sewage into area waterways because the region's infrastructure could not handle the strain of the higher than normal rainfall (<http://www.stcatharinesstandard.ca/2017/06/01/sewage-floods-niagara-waterways>). Of the 302.44 megalitres of bypassed sewage, 129.98 megalitres came from Port Dalhousie. What is to say that we will not

have increased rainfall next year and the year after that and five years hence? In such a situation, the system cannot handle the sewage from existing residences. Suddenly adding an additional 220 residences to the existing infrastructure is of concern to me.

Traffic: There will be, without a doubt, significant disruption during construction of this building. There will be traffic delays and inconveniences for the community. Regardless of the assurances of the representatives of Union Waterfront at the meeting, the assurances of a construction plan, etc. —no plan is foolproof and no build is without problems. That said, it is the traffic that will follow the build that causes me grave concern. There was much discussion at the meeting. Port Dalhousie had one way in and out. The four-way stop at Lock and Main is already crowded and adding 220 residences, 220+ vehicles to the mix will create significant delays. Perhaps a stoplight installed at that intersection would help the situation...or a building with fewer residences and therefore fewer vehicles.

Parking: In the drawings presented at the meeting, it appeared that existing street parking spaces had been removed and replaced by two driving lanes rather than one. I don't know if that was merely an artist's rendering or part of the plan. Either way, the proposal of one parking space per unit causes me concern. Will people in the condominium use parking spaces in the Lakeside Park lot for their additional vehicles? The representative for Union Waterfront at the meeting said he wouldn't want to park outside and walk in the cold to get to his car, but that is not a solid assurance or answer. If those living in a unit have one parking space and two cars, where would the second car go but into the free adjacent lot, at the expense of visitors to the park and beach. How will the lot be patrolled/ how will it be ensured that those with one spot and two vehicles do not park in the Lakeside Park lot?

My family and I moved to Port Dalhousie 2-1/2 years ago. We moved here for the old growth trees, the big sky unmarred by tall building, the community, the heritage look and feel of the area. My adult sons and daughter-in-law were equally enamoured with Port and bought a house three doors down from mine. Please think about the impact of this monstrous building on those already living here. Think about me walking my dogs every day to the beach, walking past the site where this proposed building will go. Now, as I come down the hill on Lock Street, I tip my face to the sun. I see the tops of the masts of the boats in the marina. I walk in sunshine, not shadow. The building that is proposed will see me walking down the hill to a mass of grey, no sky or sun to behold. That building will be the first thing people see driving across the bridge toward port. It will be that last thing they see as they leave. It will sit as a dark sentinel, having stolen the heritage flavour that made Port special and unique.

Please consider decreasing the number of allowed storeys and keeping the maximum number of 80 units. Please consider requesting a revised design that incorporates the positive elements such as the public courtyard but tailors the design to more appropriately reflect the feel of Port Dalhousie. The choices the city makes today will define Port Dalhousie's tomorrow. The entire neighbourhood is counting on you to keep Port beautiful while at the same time balancing the best interest of the business community, the city, the developers.

Thank you for your time.

Eve Silver


--

Eve Silver

National bestselling, award-winning author

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Ritchie, Scott

From: Fred Vanderlip [REDACTED]
Sent: Thursday, October 05, 2017 2:03 PM
To: Ritchie, Scott
Cc: Garcia, Carlos
Subject: Re: 16 Lock Street - Union Waterfront

Follow Up Flag: Follow up
Flag Status: Flagged

Follow-up from Public Information Session – Sept. 27, 2017

First let me thank you for the opportunity to be informed in greater detail of the project and thank you for your (and the city's) receptiveness of listening to the concerns of the residents.

Let me also acknowledge the strategic direction of the OMB, being increased densification vs. urban sprawl. I fully support that strategic direction.

Let me start by saying that I generally support development on the vacant lands and the preservation of the historic facades along Lock Street. I do have concerns over the height and number of units this proposal presents. I feel any project of this size needs to be a win for not only the developer, but the local community and the city as a whole. At the Public Information Session, it was clear that the local community have concerns, as witnessed by the numbers that turned out.

Concerns I have with regards to the project as proposed, especially when coupled with the other possible projects involving the lands at the Legion and Lincoln Fabrics are:

1 – Port Dalhousie is a small community. If you consider the number of residential units being added to the existing 'Port' community is significant from a percentage basis. St. Catharines is growing slowly, is there really a need for 220 additional residential units in Port? Does it need the additional units that will also come from the development of the Legion and Lincoln Fabrics projects? While I recognize that Port is a desirable location, St. Catharines has vacant or underutilized space in its core (former Hotel Diu, the General and soon GM Plant 1, just to name a few). A 14 story high building 'fits' better on one of those properties, whereas it will dwarf existing buildings in Port.

2 – The developer for Union Waterfont was vague in answering the question as to their target market, and marketing plan. This is important for a number of reasons. First their marketing plan needs to compliment the city's plan with regards to attracting both residents and business enterprises to fill the proposed building. Port Dalhousie already has vacant and underutilized retail space. There does not appear to be any short-term requirement for this space. St. Catharines as a whole appears to have an abundance of space currently. Are all of the 220 proposed units going to be acquired by business people wanting to set up new businesses???

3- What does this development do for Port Dalhousie and its historical district? I appreciate that the proposal as presented cleans up an eyesight, and preserves a few buildings, but what other local needs does it resolve? The developer is not contributing additional parkland, or community space. The additional public parking spaces (37) being provided, depending on the occupancy mix of the building may not be sufficient, if most of the residents are 2 car families. Many of Port's residents currently have more than 1 car.

4 – What does this development do for St. Catharines? If a developer was developing a 220-residential subdivision and a small strip plaza, would they not have to include a parkette and/or school lot donation to the city? What does the increase in property and business taxes collected do for the wider city?

5 – The proposed height. While I appreciate the fact that the proposed 14 stories high is lower than the previously approved plan, does this set a precedent that other development may also go to similar heights? I look at how the Town of Niagara-on-the-lake has been able to keep a relatively low profile in its historical district, and that is one of the (many) reasons NOTL remains attractive to residents and tourists alike. Setting a precedent of 14 stories high, will our waterfront look like Toronto's in a few decades?

I do not share all of the concerns expressed Sept. 27th. I do not believe 200 to 400 additional cars will clog our streets, under normal conditions. I am more concerned if the new residents all head to the QEW in the morning to commute towards Hamilton or beyond for employment. While I realize that is not a City of St. Catharines problem, when there is a problem on the QEW, it has created a high-risk situation in Port, if emergency response units needed to access the area. Again, we need to understand the target market and marketing plan for this development.

I hope my feedback and questions help your deliberations.

Fred Vanderlip
[REDACTED]

[REDACTED]
[REDACTED]
[REDACTED]

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Ritchie, Scott

From: George Darte [REDACTED]
Sent: Friday, September 29, 2017 4:28 PM
To: Ritchie, Scott
Cc: Al Visser; Travis Nolan
Subject: Condo

Hi Scott,

I attended the meeting the other night at the Port Lions Club and actually just stood back and observed. Travis and I have had several discussions about the proposal. My biggest concern is parking, parking, parking. I also am a logistics guy so it needs to be confirmed that the infrastructure can take 400-450+ more toilets being flushed to add to the current 650 homes in Port. We need to have sufficient infrastructure for not only this project, but the 3 other already proposed ones as well as any new ones coming on stream.

I am copying Travis and Al so they can hear me out. We live on Canal St. and parking is often a challenge here on our street, not just on the weekends or special times of year. Since I have become a part owner with the brewery, I have heard ideas about having metered parking as there are boaters who park in the public lot for long hours or days. Maybe 1 thing will be to ban overnight parking in the current lot or I might suggest a time limit for 'free' parking etc.

I had suggested to Travis that Fortress look at building a parking garage on top of the existing parking lot. Cost share it with the city and charge for parking. Even better, now cost share it with the other 3 condo companies and or the city. It could be a 2-4 story, pretty building and not necessarily an eyesore. Make it solar and or wind capable (now or in the future) so it will pay for it's own electrical source. There still is not the answer to where the retail shoppers, and workers will park as well as the people who work for the condos. Travis suggested in our discussions that they could ride bikes or take the bus. This isn't the GTA. Unfortunately our locals just aren't wired that way.

As Al said the other night let's worry about traffic-let's have lots of traffic. I concur with that idea. I am indeed a partner at Lock Street Brewery and I want nothing more than lots of people coming into our building. Keep in mind, THEY CAN'T COME IN IF THEY CAN'T FIND A PARKING SPOT.

I definitely look forward to new residents in Port but we must ensure that the current residents needs are met and we are able to take in new ones.

I'm OK with the funky design your team have done Travis and offer to sit down with others to discuss further if you wish Scott.

I am in Michigan on the 25th and am hoping to be back in time for the 6 pm meeting.

I also want to suggest that the Lions hall may not be large enough just like last time. It may be too late to change the venue but Bethany Community church might be a whole lot better or some such larger place.

Regards,
George

George Darte

[REDACTED]

[REDACTED]

[REDACTED]

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Ritchie, Scott

From: Port Reporter [REDACTED]
To: [REDACTED] September 30, 2017 1:42 PM
Cc: Kortekaas, Tricia
Subject: Ritchie, Scott; Garcia, Carlos; Williamson, Bruce; [REDACTED]
 Re: Port Dalhousie Union Waterfront: Let's move forward!

Thanks for sharing your personal views. I agree that we should move forward and I can appreciate where you are coming from and can respect that. I am not sure where you are getting your information since I am unaware of any community resistance in the last five years. I have heard significant frustration by business owners with city delays and process's but the community has been working very hard to make things look better. Community residents simply want appropriate development in Port that respects our laws.

Hank
 Sent from my iPad

On Sep 28, 2017, at 12:13 PM, Kortekaas, Tricia [REDACTED] > wrote:

Port Dalhousie Union Waterfront: Let's move forward!

The 2016 Census Profile of the city of St. Catharines indicates that residents aged 50+ account for 43.5% of the population of St. Catharines, with 30 to 50 year olds making up 23% of a total population of just over 133,000 residents. I belong to the minority 23% of the population and am a fairly new transplant to Port Dalhousie. In the five years I've been living here I've watched my neighbourhood slowly change from older, retired residents to younger families which I am sure is also happening elsewhere. The reasons my husband and I moved to Port were a) not only is it a beautiful place to live but b) because of the potential real estate investment as we knew the downtown core was set for re-development.

For too long I've been listening to a large majority of Port's long-time residents complain that any of the proposed developments would destroy their community, be an eyesore, and cause major disruptions due to lack of parking and increased traffic. It's my belief that should this development NOT come to fruition it will destroy what's left of this beautiful community. No disrespect the Port Dalhousie beautification committee, but the downtown core looks like a slum and putting lipstick on a pig will not attract people and new businesses. In fact, the people of Port Dalhousie have made things so difficult for new development that if this development is stopped eventually nobody will want to do business with us as we'll have proven that we can't be reasoned with.

I attended the public meeting at the Lion's Club on September 27th and one thing I heard repeated a number of times, besides the traffic and parking concerns, was "this is definitely a step in the right direction, however, I'm not sure it's the best thing". The world won't stop because we can't figure out what to do with that piece of land, it's been sitting vacant for too long. Many small businesses have come and gone because they just haven't been able to bring people to the downtown core and as Mr. Visser explained, those currently in operation are hanging on for dear life as the downtown core is currently a wasteland where nobody wants to visit and stay.

This development will bring long-term residents and patronage to that area **all year round** as opposed to 3 months out of the year **if** the weather is nice enough, and the lake water is safe to swim in. These are not the young "hooligans" experienced when the downtown core consisted of a number of bars; however I believe this is what the older residents are recalling when they think of this new

development. This will be a combination of young families, professionals who want close access to the QEW and retirees looking for a nice quiet community to settle in. The businesses will not only attract tourists looking for shopping, but residents as well. Currently, everyone has to drive out of the area to do groceries, banking, shopping, etc. If we had these in our locale we're more than likely to visit the local shops than go elsewhere, with younger residents and families walking or biking rather than driving.

The "good ole days" are well and done, the world has moved on, and it's time Port Dalhousie moves forward before it becomes a permanent graveyard where none dare to tread. We need to look and move forward instead of backwards. This development **must** move forward and I commend the forward-thinking designers for providing electric vehicle charge stations, bike lanes, and community-use indoor and outdoor spaces.

In 50 to 100 years the current majority of 50+ year old residents will no longer be here, but perhaps their grandchildren and the current young families and residents will be. I hope this is kept in mind when the city is looking to decide whether or not to move forward with this design. We have nothing left to lose, but everything to gain.

[REDACTED]

[REDACTED] Catharines

[REDACTED]

[REDACTED]

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Ritchie, Scott

From: Riddell, Jim
Sent: Thursday, September 28, 2017 2:34 PM
To: Beekhuis, Hank
Cc: Ritchie, Scott; Pihach, Judy
Subject: RE: Morning Jim

Follow Up Flag: Follow up
Flag Status: Flagged

Good afternoon: Yes certainly we will make the presentation available. I have copied Scott so that he can make it happen.
 Regards Jim

Jim Riddell M.P.L., M.C.I.P., R.P.P.
 Director of Planning & Building Services
Email: jriddell@stcatharines.ca
Tel: 905.688.5601 x1713

From: Beekhuis, Hank [REDACTED]
Sent: Thursday, September 28, 2017 9:00 AM
To: Riddell, Jim <jriddell@stcatharines.ca>
Subject: Morning Jim

There was a good turnout last night. I was wondering if you could forward the power point from last night. We would like to get together some time in October to go over all these developments. We are quite concerned that we can't really judge anything until we know what is all on the table. We also have to be very careful that we don't use the measurements of a bad year when everything was closed to justify the capacity that will be needed when everything including the Pier, Park and the development is fully functioning.

HANK BEEKHUIS
 Port Dalhousie Conservancy

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Ritchie, Scott

From: Sendzik, Walter
Sent: Monday, October 16, 2017 2:19 PM
To: Hans Braul
Cc: Ritchie, Scott
Subject: RE: Fortress Capital Proposed Condo Development

Good Afternoon Mr. Braul,

Thanks for your email and feedback. The community has been waiting for progress at the development site for years now. As the development proposal is now going through the formal planning process there will be more opportunities for public input. Comments received by the Planning Department will be considered in preparation of a staff report for Council to consider.

I am sharing your comments with the City's project planner, Scott Ritchie.

Thanks again for your feedback. I look forward to reviewing the report and considering the public comments and presentations when the development comes to City Council.

Sincerely,
Walter

Walter Sendzik
Mayor
Email: wsendzik@stcatharines.ca
Tel: 905.688.5601 x1540

From: Hans Braul [REDACTED]
Sent: Monday, October 16, 2017 9:53 AM
To: Mayor & Council
Subject: Fortress Capital Proposed Condo Development

Dear Mayor and Members of Council,

Since the meeting at the Lions Club, I have found myself thinking a lot about what I fear is a fait accompli. Yet I hope that clear thinking and civic duty will prevail.

I find that the more I think about the proposal, the more concerned I get. My concerns are in the following areas:

1. It's just too big. It simply does not fit. To my knowledge there are no buildings in downtown St. Catharines that are 14 stories (plus several floors for mechanical penthouse?). This is taller than the Brock tower. Why on earth should this little village have the tallest building in St. Catharines? While I appreciate the skills of the graphic artists that created the appealing views from Lock Street, where are the artists' renditions from the Carousel? They don't exist because that view shows a massive edifice that dwarfs all surrounding structures. Consider Paris. Such a charming city - full of humanity. I realize Port Dalhousie is not exactly comparable to the City of Lights, but my point is that by limiting the height of buildings in that city, they have created a very human, welcoming place that attracts tourists by the millions. Don't we want to make Port a destination for people who are attracted by this charming village?
2. Fortress' need to make money is not our concern. Their business plan seems predicated on a minimum number of units in order to make the bottom line work for them. We can no allow such logic to drive us into accepting something that is bad for the area. I can not believe that a smaller building with fewer residents is a financial non-starter. (And by the way, who exactly is Fortress Capital?? They seem to have a VERY shady past that does not inspire trust!)
3. I don't see the benefits. Just because there are more people living in this small area doesn't mean the surrounding businesses will automatically succeed. Their success depends upon drawing people from all over the Niagara region, just as Niagara on the Lake does. Imagine if there were only one two-lane entry into NOTL! People will not see Port as a destination unless they can get here without traffic snarls, and they can park here.
4. Which brings me to parking. I simply do not believe the projections of the developer on the number of parking spots required. Lakeside park will be a parking mess, and people will simply stop coming to the park to enjoy its natural beauty, in part because parking will be next to impossible, and partly because they'll be in the shadow of a massive tower.

Thank you for your consideration. I know it's a tough decision and I trust you will place the interests of the citizens above those who seek to profit at the expense of quality of life in our beautiful village.

Hans Braul
[REDACTED]
[REDACTED]

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Ritchie, Scott

From: Imast-Bayzar Bulgan [REDACTED]
Sent: Wednesday, September 27, 2017 12:15 PM
To: Ritchie, Scott; Riddell, Jim; Mayor & Council
Subject: Supporting Union Waterfront Development

This e-mail is informing that as a small business owner in Port Dalhousie, I am ALL SUPPORT of the Union Waterfront Project..

ps. I had my business here for 7 years starting with 2 units and unfortunately couple years later had to "downsize" to one unit due to lack of business .. Still struggling to keep the door open.. Meantime I am doing ; my best -with networking and marketing- to make my business succsesful, because this is my passion, and believe that I will make it work;however, I know that this project will support my business and all the other small business in Port..

I am hoping this project will go through and we can bring some live to Port and family friendly visiting sweet town..

Kind regards

Imast-Bayzar

[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

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Ritchie, Scott

From: James McWilliam [REDACTED]
Sent: Saturday, September 30, 2017 9:08 AM
To: Ritchie, Scott
Subject: RE: Port Dalhousie

Scott

Port Dalhousie Comments

- .. I am looking forward to getting something built in Port. The design looks interesting and an improvement on the last concept
- .. I like the idea of the jail being relocated to a more sensible location (the last proposal had it under the building which didn't seem to work, especially from an aesthetic perspective)
- .. I am not too concerned about extra traffic. I have lived in Port for the last 17 years and I can count on one hand the number of times I have had to wait more than a minute or two at the Main/Lock Street intersection
- .. The development of this project would provide an excellent opportunity to update the streetscape environment providing a coordinated 'vision' for the downtown of our community

James

From: Ritchie, Scott [mailto:sritchie@stcatharines.ca]
Sent: September 29, 2017 10:27 AM
To: James McWilliam [REDACTED]
Subject: RE: Port Dalhousie

Hi James.

I've attached a copy of the comment form. You can also just provide an email if preferred.

Scott Ritchie M.C.I.P., R.P.P.

Urban Design Planner

Email: sritchie@stcatharines.ca **Visit:** www.stcatharines.ca

Phone: 905.688.5601 x1722 **TTY:** 905.688.4TTY (4889)

Mail: PO Box 3012, 50 Church Street, St. Catharines, ON L2R 7C2



From: James McWilliam [REDACTED]
Sent: Thursday, September 28, 2017 6:46 AM
To: Ritchie, Scott <sritchie@stcatharines.ca>
Subject: Port Dalhousie

Hi Scott

Can you send me a comment sheet. I didn't get one last night.

Thanks

James



James McWilliam, BES, BLA, OALA, CSLA

Phone: [REDACTED]

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Ritchie, Scott

From: JANIE GREGORY [REDACTED]
To: Ritchie, Scott
Subject: Port Project

We attended the meeting last night at the Lions Club regarding the Union Waterfront Development. While I do have concerns that this proposed monstrosity is not going to fit into the "small town ambience" of the present Port Dalhousie, my main concerns are with the parking and traffic problems that are most certainly going to occur.

We have lived on Main Street for over 30 years and have dealt with the traffic issues it entails, especially in the summer months. During that time, with the various events at the Henley and Lakeside Park, we have a very difficult time just getting in and out of our driveway. This becomes even worse when there is any kind of accident on the QEW and the traffic is re-routed through Port. I agree with a gentleman who said, last night, if the traffic study done by the developer was done this past summer, it wouldn't give an accurate estimate of the cars and trucks going in and out of Port. The beach was closed, the pier inaccessible, no fireworks at Lakeside park and Lakeside Park itself was under water most of the time. Now that most of it is up and running we can expect the same traffic problems, or worse next summer. If there are going to be two or three hundred more cars going in and out of Port daily, all year round, you can only imagine the problems that will occur.

Parking is another issue that worries me. As it is, it's very difficult to get a parking space anywhere in downtown Port during the summer or at any time there is something going on. The developer was talking about all the new shops and restaurants and the courtyard that would be available to the public. Where do they think all the people coming to Port for any of these, are going to park? Not only are there not enough existing parking spaces for all these anticipated visitors, where are all the people in these units who need 2 parking spaces and spaces for their visitors going to park? I would guess, in the spaces that are supposedly there for all these new visitors.

I'm wondering, also, how much interest people have already shown in acquiring one of these units, considering the potential parking issues. I certainly wouldn't want to spend a few hundred thousand dollars on a condo, only to find myself walking a couple of blocks in the middle of winter or the sweltering heat of summer to get to my second required parking place. I don't think it is going to be as easy as the developer is probably thinking to fill these 220 units. I really don't think it's going to be a case of "build it and they will come".

I am hoping that our representatives at City Hall will give this potential traffic and parking problem some serious consideration.

Janie Gregory

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Ritchie, Scott

From: Ritchie, Scott
Sent: Tuesday, October 10, 2017 8:57 AM
To: 'Joanne Azzopardi'
Subject: RE: Union development

Thank you very much for your comments. I'll do my best to answer your questions with the information that we have available at this time. Your feedback will be considered as we continue to evaluate the application.

1. Based on the proposal by Union Waterfront, ownership of Hogan's Alley would be split. Union Waterfront would own and maintain the central courtyard and the north half of Hogan's Alley (stairway portion extending towards Lakeside Park). The City would have an easement over these lands to ensure public access. The City would retain ownership of the south half of Hogan's Alley (portion towards Lock Street).
2. The Region has reviewed the applicant's servicing report and has not identified any sewage capacity concerns at this time. We are still reviewing the stormwater management design and will discuss the dewatering issue with our engineers.
3. The applicant is proposing to make some modifications to the Main Street sidewalk. Pedestrian safety and comfort along this corridor will be very important and we'll be looking at these issues closely. I do not believe that any building encroachments are proposed along Main Street at this time.
4. We understand that traffic is a concern for many people in the community and we're working with our colleagues at Niagara Region to assess the projected impacts and any improvements that might be warranted.
5. Your concerns about social amenities have been noted.

From: Joanne Azzopardi [REDACTED]
Sent: Thursday, October 05, 2017 1:51 PM
To: Ritchie, Scott <sritchie@stcatharines.ca>; Garcia, Carlos <cgarcia@stcatharines.ca>; Williamson, Bruce <bwilliamson@stcatharines.ca>
Subject: Union development

Hello Mr. Ritchie,

As a concerned resident of Port Dalhousie I would like to ask a few questions of the City and to state some concerns. I did not speak up at the public meeting as I have a diverse commentary. I apologize for the lengthy notes, but I have attempted to flesh-out my concern to the questions.

- 1) Can you please confirm who will retain ownership of the Hogan's Alley courtyard and passage?
 My concern is that if under City control, this is another area that will demand service such as cleaning, garbage pick-up and greening. While the City does a fine job of this at local parks, this will be a different scenario where the City may be cleaning for area restaurants/cafes. Is this an attempt by the developer to pass scrutiny and management of patios to the City?
 Of particular interest is the proposed stairway on the north side. Who will be responsible for snow clearing?

2) You mentioned that the Region has already said the sewage requirement of the development can adequately be handled.

My concern is regarding storm flow and where that discharges. In particular I would like to know where the huge amount of dewatering flow will be directed. While this may only be a concern during the construction phase, I wonder if existing infrastructure is adequate or could be damaged from a multitude high flow events.

3) Is the existing sidewalk from the intersection down to Lakeside Park going to be altered? I understood the underground access for the complex will be along that route. I did not see any protections for the large pedestrian crowd that traverse that sidewalk. It is often unsatisfactory now when people with strollers or wheelchairs are on the sidewalk - people generally have to step off into the roadway. This will be more dangerous with increased traffic, especially when not just park visitors. From the renderings I could not see what the plans for that side of the structure were but adding retail entrances or any other obstruction to the sidewalk is not feasible without alteration such as widening. This will also require the City to maintain it's setback requirement, as I've heard an encroachment of nearly 2m was requested.

4) the groan heard during the discussion that no upgrade to traffic control was well warranted. Even the current 4-way stop is inadequate during high traffic days. Will the City thoroughly review the concluded report and mandate additional work/study as required? As a minimum I cannot see how even a pedestrian cross-walk would be required, as it currently is also not adequate at the intersection. Add more people and retail there will be increased person vs automobile encounters.

5) Social amenities are lacking in this proposal. The City must begin to require social amenities as part of developments in prime areas. I was discouraged to hear the "Beaches" development is not required to invest in upgrade or replacement of the public stairway that they have commandeered for a year, cutting off a link to Lakeside Park. Providing a room for City use does not seem adequate, especially since this is carved from the retail area, that some say is the literal requirement to salvage the other businesses in the core. Residents and visitors will surly utilize the Lakeside Park parking lot and public amenities. This development needs to provide more for the community that it will be forever altering.

In closing I would also like to note that this application must be viewed with respect to the other developments already being planned. The development at the foot of the Humber River south of the QEW is classic folly of the regulator not having a community view. Specifically in this scenario a heritage community is being altered, so I hope but also trust that the City will be cognizant of that during review.

I understand if you are not able to respond to each email you receive on this project. At the meeting Mr. Nolan offered to address any questions to him, however his email address has been elusive. I do hope that if not answered directly, these concerns are considered during the City's review and are addressed.

Sincerely,
Eric Azzopardi

[REDACTED]
[REDACTED]

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Ritchie, Scott

From: John Spanton [REDACTED]
Sent: Friday, September 29, 2017 9:31 AM
To: Ritchie, Scott
Subject: Port Dalhousie community meeting

My wife and I attended the meeting at the Lions Club this week. We have just retired to Port from Toronto, where we have gone through the very same growth that is being proposed here. We moved out of the Beach area in Toronto because of the traffic and parking issues that are inherent with dense development in a small area. Trust me when I say there will be problems, they are unavoidable. I am interested in the next meeting which will have more information about roads and parking. I have many questions that were really not answered the other night. Will the lack of parking bring enforced street permit parking to the area? This was not even touched on the other night. I live a mile from the Port beach area and on weekends when the beach is up and running or the Martindale Pond is being used I have cars parked in front of my house because there is no parking available any closer. I do not mind the parking, the point is how far will these visiting cars have to go once all the condos are in and these people and their guests are using up the closest spots. A point that should be considered when discussing traffic flow is the current traffic on Bayview Rd. Currently Bayview is an un-posted 50km/hr zone with no stop signs, lights or calming devices. It is being used on a daily basis as a quick way to get from the newest development area to downtown Port avoiding Main St. When the QEW is close anyone that knows the short cut is using it to avoid Main St. We have seen vehicles traveling way over the speed limit because they are in a hurry. What is going to happen to all side streets when Port and Main Street are busier and there is traffic delays and even more people use the short cut?

I absolutely agree that our town needs development, it needs a hug. However, some serious thoughts have to be given to what a development of this magnitude will bring with it. And what are the options for fixing the traffic and parking if it does not work as projected?

A second quick comment is about the building being stepped back from the street to make it look better from the street according to the developer. They have done this in Toronto and it really makes no difference. The light and the view are still blocked, you cannot hide a 14 storey building just by setting it back.

On a personal note, I just retired from the Toronto Police Service where I spent several years in charge of community issues. I can tell you first hand nothing gets people more upset than traffic and parking issues. If these projects cause havoc you will have an irate community to deal with.

Thank you for the opportunity to submit my comments.

John Spanton
 [REDACTED]
 [REDACTED]

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Ritchie, Scott

From: europeanplanters@[REDACTED]
Sent: Thursday, September 28, 2017 12:19 PM
To: Ritchie, Scott
Subject: traffic study report of Port Dalhousie in regards to the proposed condo buildings.

Hello Mr. Scott Ritchie,

I was at the meeting last night at the Port Dalhousie Lions Club re: the condo unit at Lakeside Park area.

I would like you to send me the website that I can read your traffic report with all the proposed buildings that are or maybe going up.

I am pro-building something in this destroyed area. The condo is beautiful. By reading your traffic reports considering the other projects that may come to your table, I can give an answer if the condo will be too big and we will have a traffic problem.

At what stage (size) does the condo need to be, to be viable for the builder.(Fortress)

Please send this info to this sending email address.

Sincerely, John van der Zalm

[REDACTED]

Thank you.

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Ritchie, Scott

From: JOHNNY VANDERBURGH [REDACTED]
Sent: Friday, October 06, 2017 6:43 PM
To: Ritchie, Scott
Subject: Fishy deal 16 Lock St. Union Waterfront Zoning By-law Amendment

Follow Up Flag: Follow up
Flag Status: Flagged

Oct.6 2017

Written By
 John Vanderburgh

To Whom it may Concern:

Regarding the proposed fortress project in Port Dalhousie. This Project Stinks Both Literally and Absolutely. The addition of another 200 condos on an already over taxed sewage system on lighthouse road makes absolutely no sense as a Resident and Boater I almost vomit every time I pass the sewage treatment plant on lighthouse road that is already over flowing with stinky stench by the result of city infill. How can a bunch of Carpetbaggers from Fortress whom have been barred from trading for 15 years (yes fifteen years) from the Securities commission be taken seriously by the City of St. Catharines, it seems like very Bad Judgement on behalf of the city to even deal with fortress as they are already involved in a class action suit with investors regarding a similar project in the city of Barrie? I think the city needs to take a long hard look at the Characters they are Dealing with from out of town whom primary focus seems to be to make a quick buck on the back of unsuspecting investors and current tax payers who will ultimately pay. The original Port Theater project that added a lot of value to the area ie: Hotel, Theater, Shops, and other mixed uses. This project has now been hacked up to the point to what is now just another Condo that can be built anywhere. the old general Hospital would be a great site for this condo and it has lots of ample parking as well.

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Ritchie, Scott

From: JOHNNY VANDERBURGH [REDACTED]
Sent: Friday, October 06, 2017 6:42 PM
To: Ritchie, Scott
Subject: Re: Stinky Port Condo Project

Follow Up Flag: Follow up
Flag Status: Flagged

From: John Vanderburgh **Sent:** October 6, 2017 10:36 PM
To: sritchie@stcatharines
Subject: Tower project port Dalhousie

My name is John Vanderburgh. My family has always lived in the Niagara Region and surrounding area. In June of 2017, I brought to Jenny Stevens' attention that RVers could no longer use the Lighthouse Road Water Treatment Facility pump out station as it had been torn out and removed completely.

I spoke to Ms Stevens because I recall her, years ago now, being vehement about "never wanting to hear from RVers ever again". Unfortunately for us, the issue of RVers using our pump out station at the Lighthouse Road Water Treatment Facility was never actually put forth in any kind of St. Catharines City Council Meeting. The singular reason for my not pursuing the matter was because I live only blocks away from the plant and that means that I can often smell why there is no pump out station anymore. While this may all seem foolish or petty to some, the concern remains that the construction of just one more tower structure in the Port Dalhousie and Lakeshore Area will over-tax the city's established water treatment facilities.

If it is the city's intention to proceed blindly, then I implore in the interest of fairness, before accommodating several hundred new residences, that we at least re-establish the pump out station for St. Catharines' citizens who have, in many cases, lived here our entire lives. About this matter we are adamant. It is unconscionable to stuff as many new people as will possibly fit in the Port Dalhousie and Lakeshore Area despite inadequate infrastructure.

If you can not accommodate the RVers, then, it is my contention that we are too lacking in basic infrastructure to accommodate any new tower structure in the Port Dalhousie and Lakeshore Area

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Ritchie, Scott

From: James Vanderburgh [REDACTED]
Sent: Tuesday, October 10, 2017 11:59 AM
To: Ritchie, Scott
Subject: Fwd: CCF10102017.pdf
Attachments: CCF10102017.pdf

Dear Sir: Please confirm that ou have received my comment sheet. Thank you! j.

----- Original Message -----

From: James Vanderburgh [REDACTED]
[REDACTED] "sitchie@stcathaines.cs"
<sitchie@stcathaines.cs>
Date: October 10, 2017 at 11:55 AM
Subject: CCF10102017.pdf

Sent from [Mail](#) for Windows 10

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PUBLIC INFORMATION SESSION

September 27, 2017

16 Lock St. - Union Waterfront Zoning By-law Amendment and Official Plan Amendment

COMMENT SHEET

Comments may also be directed to sritchie@stcatharines.ca, and should be submitted no later than October 16, 2017.

Name James & Lisa Vanderburgh Email [REDACTED]

We oppose this proposal because: too massive inappropriate for the neighborhood (low rise) too much density, insufficient parking, will cause more traffic congestion. This is a compact area that does not need more housing crammed in. We do not understand why the City would want us (residents) to partner with a company whose owner's have demonstrated that they do not act in the public's best interests. They are banned from trading by the Ontario Securities Commission. Checkout on Google Fortress Bannockburn Ontario. Does not belong in a Heritage District. Lots of lawsuits in Toronto by owners of these types of structures after 20 years. This type of building is not well suited for residential use. Public Green space & private property will be negatively impacted by shadows. The City historically has taken a very short sighted approach to heritage i.e. Where is our Carnegie Library etc.

Ritchie, Scott

From: John <[REDACTED]>
Sent: Saturday, September 30, 2017 4:58 PM
To: Ritchie, Scott
Subject: Port D. development.

Sorry I missed the public meeting, but I would like to comment. My wife and I have lived in Port Dalhousie ward for 38 years and the last 7 years on Dalhousie Ave. at Brock St. We have a very active life and still work, so I can attest to the fact that traffic is not a problem in this village. Even during rowing events and fireworks nights etc. there is very little inconvenience coming and going from our house on Dalhousie Ave. Even if all these developments go through the extra traffic will have minimal impact on the residents. Parking may be a different beast as I agree with the people who think the developers may have underestimated the number of spaces needed. Even so, it can't be worse than the days when a 1000 young people flooded into the area on a nightly basis and we still managed. Bottom line from us is this proposal seems to have addressed most of the fears the locals have and in my opinion is quite spectacular. If you're counting up votes put John and Laurie Williams in favour of this proposal and please approve it as it stands. You're never going to please everyone and the urban decay that has crept into the area is really discouraging.

John Williams
[REDACTED]

Sent from my iPad

Ritchie, Scott

From: Kelly Lewis [REDACTED]
Sent: Monday, October 16, 2017 3:19 PM
To: Ritchie, Scott
Cc: Garcia, Carlos
Subject: Comments Sheet - 16 Lock St - Union Waterfront Zoning By-Law Form September 27th Meeting

Scott,

Just a few comments regarding the proposed changes.

Traffic down Main St is going to be unbearable with that amount of vehicles going to the new development. As mentioned by Jim Riddel, being "traffic being minimal" for the changes is ridiculous. I had reviewed traffic prior to Main St being redone two years(maybe 3 now) ago with Ralph Sholtz from the Region, and numbers then, were 8000 vehicles per day. Maybe next meeting, the numbers being communicated by Mr Riddel vs what he anticipates they will be, would be of some value to the public.

He also indicated that no updates were required to update water or sewage facilities are required. The next day in the paper, Region was asking for more tax dollars to bring up these facilities thru increased to water bills. Maybe a clarification is in order.

Parking is going to be horrendous. Although the developer indicated they have more parking that other facilities, the overflow from this place is going to spill out on the streets, and the parking for the beach and boaters. Has anyone looked at that ? I'm sure road access is going to take some beach parking, has this been brought forward to the community? Being from the area, people looking for parking down towards Lock St, slow way down looking for a spot, which impedes traffic. More vehicles looking, slower traffic.

I have concerns with the additional 140 Units, and now the area being planned at Lincoln Fabrics, and at the Legion site, is going to be overwhelming traffic wise. I don't want to see traffic lights all the way down Main St. Maybe some traffic calming bumps can be installed.

Do the residents of Port Dalhousie know that when the Region does a rehab to the rest of Main St, on street parking will be eliminated? It's in their plan to do this. This will also impact parking in the area.

Who will be maintaining the public area of Hogan's Alley and the Common area of this complex. I don't want to see the taxpayer on the hook for their property maintenance. There is a lot of legacy costs here on this alone.

Also, Mr Riddel indicated that all anyone has to do for complaints is call By Law Enforcement, I have two requests in to the City regarding clothing racks, coffee tables, and Ice cream tables impeding handicap traffic on the west side of Lock St. Nothing has ever been done. It's sad when individuals on walkers, electric chairs struggle thru this area.

Do these comments get recorded for the public.

Kelly Lewis

[REDACTED]

[REDACTED]

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Ritchie, Scott

From: Kevin Kortekaas [REDACTED]
Sent: Thursday, September 28, 2017 9:59 AM
To: Ritchie, Scott
Subject: Re: Port Development

Hey Ritchie,

Just wanted to chime in as a resident of Port Dalhousie and one that is 100% behind the proposed development. My family moved to Port 5 years ago in the "first wave" of cycling out the older residents in the area.

The proposed development is likely the last shot we've got at something positive happening, and to be honest, it's only good. More bodies in our downtown core (which by itself is a laughable statement, we've got 2 restaurants and a couple small retail shops), will allow those existing business to bridge the rough times in the winter months, and the additional proposed light retail could bring even more business downtown.

I'm tired of the old scared people bitching about the change and how they want everything "back to how it was", I'm sorry, that ship has sailed and will NEVER be coming back. The reality is; if this doesn't happen, no other developer is going to touch the property with a 10 ft pole and the existing historical buildings will continue to crumble into ruin.

Just my 2 cents

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Ritchie, Scott

From: Ritchie, Scott
Sent: Friday, September 29, 2017 9:07 AM
To: 'Lauren S'
Subject: RE: Information re: Lock Street Development
Attachments: Planning Justification Report.pdf; Traffic Impact Study.pdf

Hi Lauren,
 For you information, I've attached a copy of the applicant's Justification Report (includes a summary of all studies) and a copy of their Traffic Impact Study.

From: Lauren S [REDACTED]
Sent: Tuesday, September 26, 2017 12:07 PM
To: Ritchie, Scott <sritchie@stcatharines.ca>
Subject: Information re: Lock Street Development

Hello Scott,

Great to speak to you this morning. Thanks for helping me out with finding the best place to register my concerns.

Before I do so, would you kindly be able to point me in the direction of some information on the traffic and sanitation feasibility studies (an executive summary)?

In particular, I'm interested in hearing about their thoughts/plans for the additional traffic (their building combined with Lincoln Fabrics and additional 12 unit building up on Lock Street as well as the number of incoming businesses), sanitation issues, the removal of the theatre (replaced with more units?), the reduction of parking spots and the building set back requirements.

Anything you could provide would be a great help. I'm sorry I cannot attend this important meeting tomorrow, but with the commute back from Toronto it would make it very challenging to get to a 6pm meeting :(

Thanks,
 Lauren

[REDACTED]
 [REDACTED]

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Ritchie, Scott

From: Lauren S [REDACTED]
Sent: Wednesday, September 27, 2017 5:30 PM
To: Ritchie, Scott; Jean Brooker
Subject: Statement: Official Plan Amendment and Zoning By-law Amendment for Lock Street, Port Dalhousie Union development

Hello Scott,

I would like to provide a written statement in lieu of my verbal statement at tonight's meeting at the Port Dalhousie Lion's Club which I am unable to attend.

I do not support the Official Plan Amendment and Zoning By-law Amendment submitted by the Union Waterfront organization. I have been a long time resident of Port Dalhousie. I own a property at 178 Main Street and two other family members own other properties on Main Street, one being my mother whose house I grew up in.

Sticking to the facts, removing the emotion around what I would actually like to see in the place of this proposal, here are the things I take issue with the most:

1. An increase the number of permitted apartment dwelling units from 80 large units to 220 smaller units. I do not believe that Port Dalhousie can physically handle that huge number of additional residents. I believe there will be a huge impact on the traffic in the area (which is already bad), the sewer system and sanitation, as well as the parking. As a resident who sees traffic even in the "off times" I don't know how the additional people beyond the Lincoln Fabrics and the Lock Street developments will not be a burden on our already packed streets and our resources. We also do not have the public transportation in place to support this population increase. I would like to see improvements made to public transportation in the local area which is not within the remit of this private company. Therefore, before I could support this plan I would need to see what the city is proposing to do to support a structure of this magnitude in the area.
2. A decrease in the required amount of commercial uses. We need to draw people down to Port Dalhousie as a destination, not simply a place to live in order to support our local business. We have the beach, the pier (kind of), the Marina and the residents... why have they decreased the commercial use when we need to find ways to create jobs, support our existing residents and keep visitors coming back to support our local business? This comes across as an attempt to make as much money as possible instead of investing in the long term development of the area.
3. Removal of a theatre – this original plan ensured that we were investing in the local culture and local community. They have replaced this proposal with a bookable community room which is grossly insignificant. I would like to see something in its place that adds to St.Catharines' recent cultural renaissance.
4. A reduction in the required parking – one parking spot per person is not sufficient. The overspill will go into the parking lot or up the streets. For over 20 years we see parking go all the way up the street to the Lions

Club on weekends and during events. That's a 15 minute walk. What happens when we add hundreds of new people via the three new developments? I'm only assuming there would be an overwhelming increase in the volume of cars, traffic and overspill parking on side streets. I would like to see a plan for metered parking to go in to ensure there is a time limit on cars parked in the public parking so we can cater to visitors. I would like to see the money made going back into Port Dalhousie improvements. We have a duty to provide public access to the beach and other Port Dalhousie amenities... we can't have everything overtaken by the residents of this one building.

5. Setback requirements – having not seen the specifics on this I don't feel prepared to say whether I agree or disagree, however, I am in favour in general of a set back from the sidewalk. The building is not attractive by any stretch of the imagination. I would like to keep the historic buildings as the feature of Port Dalhousie and set the apartment structure back as far as possible.

6. It's ugly. Plain and simple. It is not an appealing design. It doesn't fit with the heritage status of the area and does not compliment the already existing buildings.

My more emotional appeal now is this... I love Port Dalhousie. I want to see something in the hole in the ground that has been an eye sore to for the residents for over a decade. It pains be to see what this sweet little Lakeside town has become. I want nothing more than to have something built there, but we need to take a pause. Many residents want to see something, anything, built and it's impairing their ability to ensure that the RIGHT development goes in there. It needs to be something that brings back the charm, supports our residents via infrastructure, invests in our area's culture, protects and cherishes our heritage status and buildings and enhances the look of the area. Sadly I don't feel this is the development that meets any of these requirements. I'm prepared to wait for the right development even if it means my own lifetime is spent looking at a hole in the ground because I think Port Dalhousie can be the gem of St.Catharines. It could draw of people to the area which is not just in Port Dalhousie's interest, it's St.Catharines as a whole. We have one chance to get it right and I'm not willing to take a bet on this project to live up to that task.

Culture over function

Beauty over utility

Feasibility over haste

Thoughtfulness over greed

Community over corporate interest

I'm looking forward to the outcome of this meeting. Thanks for hearing out my thoughts on the subject.

Thanks,
Lauren

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Ritchie, Scott

From: Leo Versluis <[REDACTED]>
Sent: Friday, September 29, 2017 2:42 PM
To: Ritchie, Scott
Subject: union waterfront project

Hi Scott

I was at the meeting this week at the Port Lions club for the public input on the port project. Thank you for the presentations and a well run meeting. I did not take advantage of the mike time offered but do want to put in my two cents worth.

While no one seems to argue that it is high time something finally happens in Port and even though some of the design elements are in keeping with the intent to protect and showcase Port as a relevant heritage area, I feel the scope and style is inappropriate.

The sheer mass of the new building overpowers the remaining structures, that is obvious even on the renditions that the developer presented. The developers willingness to retain the Austin house and some of the other structures are commendable and appreciated but the balance is so skewed that it pretty much makes the heritage elements irrelevant. This could be alleviated by knocking off about 4 storeys of this proposed building which would go a long way to making the blend work and still give significant room for development.

Beyond using Port on all the tourist and promotional literature for St. Catharines I am not convinced the City and staff care much about Port and fail to see it as a viable heritage area that has community appeal as well as tourist draw. I have seen little that shows much vision to enhancing that appeal, observing much more that seems to, at the least, lean toward compromise and quite often looks like total capitulation to development at all cost. I hope I am wrong on that and that we will see a difference in this case. Having been a Port resident my entire life I know for sure this project is way too big to sustain the community feel that Port has cultivated over the last number of years.

Keeping in mind all the other proposed developments, this all creates a critical mass that is likely over ambitious from a marketing standpoint, making me wonder, how many units can actually be sold here? It is up to City staff to study that as part of this overall process to ensure we don't have an oversupply and lots of empty buildings. As mentioned several times the ensuing population explosion and resulting traffic and parking issues are part of the concern that will also need to be dealt with, by due process.

Thank you

Leo Versluis
 [REDACTED]

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Ritchie, Scott

From: lesley newton [REDACTED]
Sent: Friday, September 29, 2017 1:44 PM
To: Ritchie, Scott
Subject: Union Waterfront Development

Hi Mr. Ritchie.

The following points are in response to your request for comments on the proposed development at 16 Lock Street in St. Catharines.

I am not against the development of the area as long as it reflects the heritage flavour of the area. In no way, shape or form can I understand that a 14 storey building is in keeping with the neighbourhood's history and heritage designation (I understand the commercial core is not subject to the heritage designation, but it is bound by areas that are.)

I will list a few points about which I am concerned.

-Density, density, density and density again. The proposal puts 220 units in an area that is for all intense purposes a cul-de-sac. Being in a cul-de-sac creates huge problems for traffic flow. I cannot comprehend how that small area of Lock Street and Lakeside parking lot will not be adversely affected by an additional 220 units and their accompanying inhabitants, family and friends.

-at the Public Information Session on September 27, 2017, it seemed that some of the people who spoke in favour of the plan were retailers who would obviously gain from the project. I'm unsure why their needs should be more important than the residents of Port Dalhousie and those who travel to Port Dalhousie to enjoy Lakeside Park. It would seem that the larger project, the better it will be for any retailer in the commercial core. I hope you take this into consideration.

-I am concerned that the Planning Department might be influenced by City Councillors who want development at any cost. I understand that we need to pay taxes and I have never complained when my taxes increase. I realize that without taxes, a city cannot function or grow. I would rather my taxes increase than have this development as a venue to obtain property taxes.

I drove down Martindale Road the other day and saw the Grenadier Apartments which are not even 14 storeys high. I cannot imagine a building that tall in the middle of such a small area. I was at the Secondary Plan meeting previously. It seemed to me, that the consensus was for no more than 4-5 storeys. This is a far cry from those proposals.

Sincerely,

Lesley Newton

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Ritchie, Scott

From: Linda Crabtree [REDACTED]
Sent: Monday, October 02, 2017 2:50 PM
To: Ritchie, Scott
Subject: Fortress

Hi Scott: If the people at Fortress have their architects use the concept of Universal Design the tenants will be able to age in place and make it their last home. I'm not sure if the OBC mandates Universal Design but it would be a selling point for Fortress as seniors will likely be in the majority of buyers. Access, Universal Design and aging in place are rarely, if ever, mentioned in a sales pitch for condos but they are a very real factor in the mind of the buyer.

I lived on Port for 15 years, renovated two houses, but left because the amenities I needed weren't there. I'd love to go back but, using a mobility device, I need accessible housing and it doesn't exist there. It could if Fortress does it right.

Take care,
 L.
 Linda Crabtree C.M., O.Ont.

[REDACTED]
 [REDACTED]
 [REDACTED]
 [REDACTED]
 [REDACTED]
 @LindaDCrabtree

Ritchie, Scott

From: Lorne Stobbs [REDACTED]
[REDACTED]
To: Ritchie, Scott
Subject: Port Development Information Session
Attachments: Port Development.docx; ATT00001.txt; ATT00002.txt

September 27, 2017

Scott Ritchie
Urban Design Planner,
St. Catharines, ON

Dear Mr. Ritchie,

I attended the public information meeting for the Union Waterfront Zoning ByLaw Amendment and Official Plan Amendment. I offer the following comments.

In principal I welcome the development and the positive impact it will have on the City of St. Catharines. By increasing the residential and retail footprint, it should have a positive impact on the economic growth of the city. The 220 units however, may be too big to fit the site. Let me explain.

1. **Traffic** – Port Dalhousie has one main access. Lock Street services the core and Lakeport and Main Street provide the only ingress and egress from town. When there are major events in Port, traffic flow is congested. If these streets had 2 lanes of traffic in each direction and a traffic light at Lock and Main, it might accommodate the additional traffic created by the 3 condos being considered for Port. The fact that the new development is accessed from the north end of Main street which also services Lakeside Park and bottlenecks at the four way stop at Lock Street creates a big problem. If there was another access to Port, this would not be as much of an issue. I think this is the major obstacle with this development.
2. **Sewage**- The existing sewage treatment plant is inadequate and with heavy rainfall and storm sewer overload, raw or partially processed sewage has in various instances been dumped directly into Lake Ontario. By adding the new developments in Port to the system, the sewage treatment situation will worsen. We have seen the impact of climate change across the continent, and more frequent, more intense storms are the predicted norm. Will there be plans to upgrade the sewage treatment facility and will the taxpayers be picking up the tab.
3. **Design**- I understand the Port design is a modification of a similar design used in north Toronto. Several people at the meeting commented that it did not fit into Port, and that a design integrating with the character of the existing buildings on Main Street would be more pleasing. The scale of the building may be too grandiose for Port and perhaps something on a smaller scale would be a better fit if economically feasible to the developer. Again, I feel that the traffic flow is the big issue. Preselling 65% of the units may be unachievable and result in the property remaining undeveloped for an extended time frame, something we are all too familiar with.

The need for a theatre and hotel was unrealistic and was dropped by the current developers in favor of more residential space. Although retail space was reduced from the previous proposal, it is still adequate. Overall, I feel that a development of this type would have a positive impact for the city, but the traffic flow is a major limiting factor. Given the geography of Port, I do not see any simple solutions given the limited road access.

I submit these comments for your consideration.

Lorne Stobbs



Ritchie, Scott

From: Marion Markarian [REDACTED]
Sent: Wednesday, October 04, 2017 10:45 AM
To: Ritchie, Scott
Subject: Union Waterfront

This is regard to the Union Waterfront proposal in Port. Given the information that this where to send comments so hopefully you are the right one.

My husband and I have been residents of Port for 40 years now and are active in our community (St. Catharines rowing club, Port Beautification) Our daughters and their families both live in Port so I feel we have a vested interest in what happens in our community.

I will start out by saying that I would love for Port to maintain its small beach community feel that it currently has. I love living in this community. But having said that we cannot live in the past forever. The commercial area is not viable as stands. Small family owned businesses cannot survive with only summer patronage, which was sorely hurt by this year's high water in addition to the closure of the peers. If we do not do something to breathe life into the area it will surely continue to die it's slow death and Port will go to sleep again. Those buildings that some want to maintain for their heritage value will continue to decline.

We have a chance here to make new beginnings and integrate the developments into our community. Far better that we work in concert with those who wish to develop the area to try to mitigate issues that could arise such as the dilemma about traffic flow and parking. The city and region should be working with the developer to come up with a plan mutually beneficial to all. Indeed we should be working with all the potential new developers together in creation of this plan. I believe this can happen.

There are some who continue to challenge the need to go forward. They are right that this development will change the face of Port forever. I respect their opinions in terms of heritage buildings and ambiance of the town. I too agree that this should be forefront in discussions with the developers. But this does not mean that we do not develop. It means that we work together to develop. Another lengthy and costly OMB hearing is counterproductive to that end.

Let's be a community that understands and appreciates it's past but embraces the future. Let's show that we are open for business..... and a side note... that we are worthy of the federal government restoring our piers because we have many people here who will appreciate and utilize them.

My two cents worth... but I would not want only the voice of the naysayers to be heard. I would hope that you share this with Bruce and Carlos as I cannot seem to find their emails at this moment.

Respectfully,
 Marion Markarian
 [REDACTED]

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Ritchie, Scott

From: Mark Kudreikis <[REDACTED]>
Sent: Tuesday, October 10, 2017 11:42 PM
To: Ritchie, Scott
Subject: Union Waterfront Zoning & Official Plan comments

Follow Up Flag: Follow up
Flag Status: Flagged

Is this the perfect development for this area in Port? Probably not but what would the perfect design be. It will have such a massive impact both good and bad that it is nearly impossible to determine the perfect solution. Something needs to be done but must be done smartly for the overall good.

Is this proposal better than what had been approved? In general scope yes. It provides denser residential component, eliminates poorly conceived theater requirement, preserves more heritage structures and more emphasis on streetscape not so internally focused.

Specifics in regards to OP & Zoning amendment requests:

1. Residential - increase is reasonable as additional units still net less people than what theatre proposed bringing into Port on regular basis
 - were studio and/or loft units considered - these would attract greater diversity and create uniqueness of a mixed residential makeup
 - is the tiered design something people actually want to live in? Seems very intrusive to be looking down on others below
2. Commercial - what resulted in the 15,000 sq. ft. reduction from approved? Seems like very similar lot coverage. The community use space seems to be wasted, how much use will it get. Would be better to designate it as commercial/cultural and encourage a possible Port Dalhousie museum with a meeting room available for community use when required
3. Theatre/Hotel - agree with removal of theatre component as poorly conceived in first place
 - would like to see hotel component remain and the Austin House be reverted back to this use with restaurant/tavern on street level. Preserve some of the past into this contemporary development
4. Parking - like idea of reducing parking requirement but is it feasible. Port Dalhousie and St. Catharines does not currently have the public transit infrastructure that larger metro areas have. Maybe in 10 years time that may be true and should be the goal in mind now which i commend the developer for trying to go that direction. If the parking is maxed out where will the additional spots be built that Travis Nolan mentioned he would build if residents wanted to buy them?
5. Building Height - why not go taller and maintain single tower design with lower massing around property perimeter? This tiered design feels too massive for this space and creates an overbearing feel. The single tower creates focal point from afar and provides those million dollar views. Not convinced that with this design there won't be significant shadow and wind effects on surrounding properties. Has there been modelling done to show this impact as well as visual impact from various points around Port and north St. Catharines?

That is all, can over analyze this thing forever. I really want to see Port return to an active commercial core as it really was something just a mere 12-15 years ago.

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Ritchie, Scott

From: Kelly Lewis <[REDACTED]>
Sent: Monday, October 16, 2017 3:19 PM
To: Ritchie, Scott
Cc: Garcia, Carlos
Subject: Comments Sheet - 16 Lock St - Union Waterfront Zoning By-Law Form September 27th Meeting

Scott,

Just a few comments regarding the proposed changes.

Traffic down Main St is going to be unbearable with that amount of vehicles going to the new development. As mentioned by Jim Riddel, being "traffic being minimal" for the changes is ridiculous. I had reviewed traffic prior to Main St being redone two years(maybe 3 now) ago with Ralph Sholtz from the Region, and numbers then, were 8000 vehicles per day. Maybe next meeting, the numbers being communicated by Mr Riddel vs what he anticipates they will be, would be of some value to the public.

He also indicated that no updates were required to update water or sewage facilities are required. The next day in the paper, Region was asking for more tax dollars to bring up these facilities thru increased to water bills. Maybe a clarification is in order.

Parking is going to be horrendous. Although the developer indicated they have more parking that other facilities, the overflow from this place is going to spill out on the streets, and the parking for the beach and boaters. Has anyone looked at that ? I'm sure road access is going to take some beach parking, has this been brought forward to the community? Being from the area, people looking for parking down towards Lock St, slow way down looking for a spot, which impedes traffic. More vehicles looking, slower traffic.

I have concerns with the additional 140 Units, and now the area being planned at Lincoln Fabrics, and at the Legion site, is going to be overwhelming traffic wise. I don't want to see traffic lights all the way down Main St. Maybe some traffic calming bumps can be installed.

Do the residents of Port Dalhousie know that when the Region does a rehab to the rest of Main St, on street parking will be eliminated? It's in their plan to do this. This will also impact parking in the area.

Who will be maintaining the public area of Hogan's Alley and the Common area of this complex. I don't want to see the taxpayer on the hook for their property maintenance. There is a lot of legacy costs here on this alone.

Also, Mr Riddel indicated that all anyone has to do for complaints is call By Law Enforcement, I have two requests in to the City regarding clothing racks, coffee tables, and Ice cream tables impeding handicap traffic on the west side of Lock St. Nothing has ever been done. It's sad when individuals on walkers, electric chairs struggle thru this area.

Do these comments get recorded for the public.

Kelly Lewis

[REDACTED]

[REDACTED]

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Ritchie, Scott

From: Pat Waters [REDACTED]
Sent: Thursday, September 28, 2017 11:41 AM
To: Ritchie, Scott
Subject: Fwd: Port Dalhousie Development`
Attachments: Port Development.docx

To: J Riddell
 Mayor Walter Sendzik and St. Catharines City Councillors

As I am unable to attend the meeting on Wednesday evening and since I have been very involved in all that has had to do with Port since the first development came on the scene I would like to take this opportunity to express in writing my thoughts and main concerns re the latest proposal.

I have attached for your information the letter I sent out following the developer's presentation at the Legion earlier this summer.

My three main concerns remain the same: density, traffic and parking, and lack of cohesion with surroundings.

The proposal is too dense, and too massive for the area in question. 220 units is almost 3 times more than the previous proposal and together with the Lincoln Fabric development, the Locke and Dalhousie Sts condo building and the proposed development at the Legion the pressure on the infrastructure, traffic and parking of this small and limited access area must be seriously considered.

Since the first proposal, the developers have all tried to impress everyone with their knowledge and how their plan for parking and traffic will work but nobody really understands what is required and they still don't get it. This time with 220 condos and 1 parking space per unit the current developers definitely don't get it. Of course traffic has to be considered along with parking and we all have seen the nightmares that can and often do occur with a one roadway in/out configuration.

Lastly this development remains as have all its predecessors totally out of character with Port Dalhousie.

Why do we need all these condos, apartments in the waterfront area of Port? Are there statistics to prove they are needed or is this another example of greed over need?

Thank you for taking the time to consider my concerns. I will watch with interest the outcome of this proposal at City Council.

Patricia Waters

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Patricia Waters

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Patricia Waters

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Ritchie, Scott

From: pkbar pkbar [REDACTED]
Sent: Wednesday, September 27, 2017 11:31 AM
To: Ritchie, Scott; Mayor's Administration; Williamson, Bruce; Garcia, Carlos; Bellows, Sandie; Phillips, Bill; Harris, Matthew; Kushner, Joe; Britton, Mike; Sorrento, Sal; Siscoe, Mathew; Elliott, Mark; Port Dalhousie Conservancy
Subject: Fortress Condo Tower proposal - Port Dalhousie

September 27, 2017

To: Scott Ritchie, Urban Design Planner

and St.Catharines City Councillors

THE DOWN FALL OF ST.CATHARINES - SUMMER FUN PLACE

Without Prejudice

Re: Fortress Condo Tower proposal in regards to the Official Plan Amendment and Zoning By-law Amendment for Port Dalhousie at 12 & 16 Lock Street, 12 Lakeport Road – Files: 60, 36, 311 & 60, 35, 1029

Scott Ritchie and City Councillors

I personally would like to thank Travis Nolan, Senior Project Manager for Fortress Real Developments for getting the community involved with their project with an open house on August 23, 2017. However I am presenting my personal opinions as a result of my education training, international travelling and personal work experience in regards to this new Fortress Condo Tower proposal for Port Dalhousie. I do realize that there may be some who will disagree with my comments and opinions... but from experience there is no right or wrong answers. There could always be compromises. It is my opinion that unfortunately a lot of decisions are made because of money, power and greed. One of my degrees is in Business Administration and Urban & Environmental Studies where we studied urban planning. We studied that in the long term successful financial projects depend on proper common sense urban sustainable planning goals. Some of my concerns are addressed below:

LOCATION: It is my opinion that this oversized unexciting residential complex is way too large to be built in this small core village foot print. It is my opinion that Port Dalhousie is a SUMMER FUN PLACE ...not a Mecca for a concentrated 14 storey building with 220 luxury residential units ... that does not meet with today's sustainable urban over-crowding common sense planning for a confined limited land space. There are other larger land masses in St.Catharines and surrounding areas to build this large complex with attractive ravine water views (examples: old Hotel Dieu and General Hospital sites, old General Motors site and to the west at Prudommes). Port Dalhousie is not Toronto, Montreal or Vancouver. Please note...in all of my domestic and international travels I observed that small harbour villages in Canada (like Newfoundland and east coast villages and northern Ontario) and also countries around the world.... are summer fun places or tourist areas that do not have concentrated condo towers in their village cores. **Why are we now trying to destroy our unique Port Dalhousie village for our grandchildren with an oversized condo tower?**

DESIGN: I personally enjoy new distinctive construction buildings. It is my opinion that this Fortress Union Waterfront tower design is not very stimulating. It reminds me of the Habitat structure built in Montreal for

Expo. It is my opinion that the original project design by the famous architect Jack Diamond with his unique lighthouse narrow glass tower and with the Port Dalhousie façade base was very attractive and exciting and it at least trended in the village design. Unfortunately, Jack was not hired by the original developer. It is my opinion that this new Fortress building design will create wind tunnel effects also interfering with the natural lake convection winds and create continuous neighbourhood shadowing from the highest north side of this step-up-design building on the Main Street hill entrance/exit. Also with increasing climate changes, new global warming, unpredictable violent storms and increasing high water floodingbuilding deep underground parking in an area where the soil is questionable being on former land fill close to Lake Ontario and the tributary of the Twelve Mile Creek this type of construction in this area should come under scrutiny as discussed at the OMB Hearings. I recommend residents pack life jackets in their vehicle especially if the power is out for long periods of time and the generator fuel for the water pumps runs out...lol!!!

PARKING & TRAFFIC: Port Dalhousie has been a SUMMER FUN PLACE since I was a kid....seventy years ago. I did a research urban history paper on this village of Port Dalhousie since 1826 with all its ups and downs. It is my opinion that this village is not designed to be a shopping Mecca. It is an unique small water front recreation village community with a great beach and two attractive marinas for all the tax paying citizens of St.Catharines to come out to relax, use their boats, swim, picnic, play in the kids play ground, teens playing beach volley ball, ride the carousel and socialize in some of the local pubs and restaurants with family and friends, etc. It is not an extended shopping Mecca for downtown St.Catharines. I sat on the St.Catharines Parking Advisory Committee for several terms. I had to testify at the OMB Hearings where the current parking lot in Port Dalhousie was expanded to assist with the parking to handle the current businesses in the village core. Under the provisions of expanding this parking lot it was noted that if any new structures that were built in the Port Dalhousie core additional parking would have to be built and supplied by the new contractor to meet the parking site-plan by-laws which basically have to be put underground.

There is only one road into and out of the village of Port Dalhousie. The streets are narrow and the traffic especially on the weekends and summer months can be heavy. Parking now is a premium at times. It is my opinion that this complex would saturate the existing parking spaces more with condo visitors and condo owners not using the underground parking garage for additional vehicles or having oversize vehicles or being frustrated getting in and out of their limited access entrance and exit. It is my opinion that the condo residents entering and exiting the complex could become very frustrated when there is traffic congestion around the complex from Lakeport Road and especially on the Main Street hill and with the Stop a the corner of Lock Street and the traffic light at the bottom of the Lock Street hill.

OTHER CONDOMINIUM COMPLEXES: There are three other smaller residential condo complexes in the building and planning stages that in my opinion are in better locations and provide land usage needs. It is my opinion that these three condo complexes provide great sustainable residential urban planning. They are located on the fringe of this small village commercial core. The small complex at the top of Lock Street currently under Silver Line Group & Mountain View Construction does not interfere with the village core and traffic flow. The two complexes on Lakeport Road under the planning stages are also on the village fringe , away from the commercial core and do not interfere with the traffic flow and will enhance the residential aspect of the village. It is my opinion that Tom Rankin will make his new condo complex on the Legion property an attractive gateway to the Port Dalhousie core. I expect the same for the proposed Lincoln Fabric building condos. It is my opinion in studying urban planning that Port Dalhousie does not need any more residential units in the small commercial core area. Urban residential planning should be located on the fringe of this small village core.

MONEY, POWER & GREED: It is my opinion that there are usually three hindrances in a well planned out sustainable project to become financial successful. Money could be a problem if the primary focus of the developer is to make a huge profit without the environmental effect and sustainability consideration infringements on the neighbours and surrounding community. Power can also play a key factor of some zealous City staff and/or certain councillors in trying to change new zoning by-laws that are not in compliance of the

sustainability needs and the wants of the neighbours and the community just to increase the tax base. It is my opinion that massive condo projects like this result in greed when huge profits are made by the developer and the City has their new property tax base greatly increased. The big losers are the neighbours losing their privacy, shadowing and wind tunneling, lack of natural lake convection breezes, increased traffic and parking congestions... and the tax paying residence of St.Catharines coming to enjoy the beach and park, facilities with their families and friends... picnics at the new pavilion, play ground, the carousal and band pavilion concerts, etc....losing their SUMMER FUN PLACE with the lack of parking and open air privacy with a huge tower having condo residents staring down at them in the back ground. It is my opinion that this could create new divisions within the community and a new lack of trust in the political leadership.

SUGGESTIONS: It is a difficult task with the responsibility in planning sustainable planning complexes that will please the developer and most of the citizens. It is my opinion this project should be on a smaller scale which means less profit for the developer and reduces the tax base.

1. Lakefield is located on the tourist Trent Canal Water System and has a great Cape Cod style three plus storey complex called the 'Village Inn' that has inn rooms, banquet hall and business rooms, restaurant and commercial shops, etc. It is my opinion that this type of complex on a slightly larger scale would really enhance the Port Dalhousie core.
2. In Burlington on Fairview Avenue north of Applebee Line there is a business live-in owner's condo complex. Basically these are small businesses on the street where the proprietors live in the condos above their businesses with two additional levels with balconies and parking at the rear of the units. This could be implemented in the Port Dalhousie commercial core using the existing heritage buildings and additional new construction with a previous Hogan's Alley look and atmosphere. It would only require a much smaller underground parking facility than the proposed for 220 condos.
3. A third suggestion is that in Port Dalhousie there is no entertainment place for weddings, conventions, banquets, community and group meetings, dinner theatre, etc....and also for overnight accommodation for visitors. A small hotel complex would enhance the Port Dalhousie core in the off summer months. There could also be surrounding commercial stores on the site...especially a needed convenience store. Also parking requirements would be much less.

I want to thank you for listening and allowing me to express my concerns and provide a few suggestions.

As I see it

Sincerely

Phil Baranoski

[REDACTED]

[REDACTED]

[REDACTED]

[REDACTED]

[REDACTED]

c.c Travis Nolan, Fortress Real Development

Port Dalhousie Conservancy

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Ritchie, Scott

From: Rachel Wrighton [REDACTED]
Sent: Tuesday, September 26, 2017 10:45 PM
To: Ritchie, Scott; Riddell, Jim
Cc: Mayor & Council
Subject: Union Waterfront-Port Dalhousie

Dear Mr Mayor, Council, Mr Riddle and Mr Ritchie,

I am writing this letter to voice my opinion about the Union Waterfront development project.

I am the Manager of both the Balzac's in Port Dalhousie and Niagara on the Lake as well as a board member of the PDBA.

We opened the Port Dalhousie location 2.5yrs ago and have had an amazing support from our local community. Unfortunately, I do not think this will be enough to have a thriving business long term. As it stands now we only have a handful of shops and restaurants in the core area and this is not enough to be successful year round. We need to have a vibrant commercial and residential core 365 days per year.

There is a strong negative perception that Port Dalhousie is 'closed' and this needs to end. Myself and my fellow business owners are the ones that are working here day in and day out and see the potential of this amazing area but also know the repercussions if something does not change soon.

Please I am asking you to have an open mind when considering this project, this has gone on for far too long. This project has so many wonderful features that will benefit the community and local businesses. I do believe that all the good points strongly outweigh the bad.

Thank you for taking the time to read this.

Sincerely,

Rachel Wrighton
 [REDACTED]
 [REDACTED]
 [REDACTED]
 [REDACTED]
 [REDACTED]

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Ritchie, Scott

From: Ritchie, Scott
Sent: Monday, October 02, 2017 9:19 AM
To: 'Brad Stoll'
Subject: RE: Questions Regarding The Union Waterfront Development

Hi Brad and thank you for your comments. I'll do my best to answer your questions with the information I have available at this time. As we're only dealing with the issue of land use permissions at this time, we don't yet have all the details of building construction.

1. Details of the proposed construction method, including construction traffic routes and storage areas for materials and equipment have not yet been provided. The applicant did provide a traffic impact study that looks at projected traffic growth and intersection operations after project completion. This study is being reviewed by City and Regional transportation staff and I'll share your concerns with those reviewers.
2. As part of their application, Union Waterfront was required to prepare a conservation strategy for the heritage buildings. This plan details the condition of the buildings and how they should be protected during construction. If approved, the City would also require a heritage easement and securities to further guard against damage. If there was some incident that resulted in the Austin House coming down, the owner would likely be required to rebuild the structure, but would depend on the conditions of any approval. I also understand that the owner has been working with the City's building department to stabilize the buildings.
3. The applicant did prepare a traffic impact study and this document also addresses parking. This document is still under review but I do not believe that they have yet identified a strategy for employee parking. This is something we'll need to consider. With respect to visitor parking, the first level of their parking garage is proposed to be mostly visitor parking (currently 33 visitor spaces).

From: Brad Stoll [REDACTED]
Sent: Friday, September 29, 2017 11:28 AM
To: Ritchie, Scott <sritchie@stcatharines.ca>
Subject: Questions Regarding The Union Waterfront Development

Good Morning Scott,

I hope this email finds you well!

Thank you for bringing to our attention the information regarding the Union Waterfront project at the Open House on Wednesday night. It was very informative and I feel more confident that the city and Fortress are trying to work together to come to a common goal to a revitalized Port Dalhousie. My wife and I, for the most part, support what Mr. Nolan and Fortress are trying to build and appreciate their efforts to hear the residences of Port Dalhousie about what they want to see from the Union Waterfront. I do have a couple of questions that I thought would be better to email to you instead of trying to find time to present at the open house.

1. What steps is the city and/or region taking to minimize the amount of construction traffic utilizing Main St. and Lakeport Rd. during the construction period? I think it's silly to think that construction traffic will not be a factor during the construction period for a project this size. We live in the Cole Farm subdivision in Port Dalhousie where a lot of children cross Main St. to access the Avondale, friends houses and parks and currently don't have a safe area to cross between Martindale Rd. and Corbett St. Currently, after school is out, Main St. is quite a busy thoroughfare and with people coming off the QEW from Seventh St., traffic is becoming a concern already. Adding construction vehicles back and forth plus additional traffic after construction accessing the condos, what measures in the city/region doing to make Main St. a safe and slower street? I couple of years ago, I spoke with Councillor Williamson about adding a traffic controlled crosswalk (similar to the crosswalk in front of Murphy's in Port Dalhousie or similar to the new crosswalk in front of the PAC on St. Paul St.) ideally at either Cole Farm Blvd. or Dalemere Cres. at Main St. and he agreed that something needs to be considered. He presented it to council and it was then directed to the Region (since Main St. is a regional road) and, at the time, the region turned it down. With potential increasing traffic coming, is this something the city and/or region can consider to help keep our community safe?

2. My second question is what measures is Fortress taking to preserve some of the older buildings while construction is commencing? What happens if there is an "accident" to the Austin House (for example) and now it's deemed unsafe and has to come down. Some of these heritage buildings have been sitting vacant for years now and I'm sure they haven't been overly maintained. What are the repercussions to Fortress and what is the city doing currently to make sure that something like that doesn't happen?

3. My third question is has the city or Fortress considered parking for employees of the stores, offices and condo? Are those spaces included in the building spaces or are they going to have to find somewhere else to park? What about guests of the condo residences? If a residence (or two) are throwing a party or get together, will there be enough parking to accommodate all guests without it impacting Lakeside Park parking and side street parking?

Thank you, Scott for taking the time to take in my questions. I appreciate it and I look forward to your reply.

Regards,
Brad Stoll

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Ritchie, Scott

From: Sas <[REDACTED]>
Sent: Thursday, September 28, 2017 9:43 AM
To: Ritchie, Scott
Subject: amendment proposals for the port tower.

I find this all so sad.

My observation and opinion even though I don't live 'in' Port. I lived close by for many years. It's normally my daily walk area.

Pre-Fortress buying property and ripping buildings down it was a quaint little 'port'. I saw nothing wrong with it with the exception of "My Cottage" and drunks late at night. This was allowed to happen so that the residents would be so happy to have 'any change', in my opinion and many I talk with.

OMB should have been given a deadline, as in most transactions/contracts, and if there was no action the contract becomes null and void. I would prefer to see OMB become null and void also. Basically '**one**' woman made a decision for a community of 300 plus.

Fortress is not 'hearing', listening but not hearing, the people of Port and St Catharines.

We, or most of us, don't want another place to shop, there's tons of those around. We had the quaint little shops already pre-greed.

Who is going to buy \$300,000 condos which are all squashed together and look terrible. The brick ones are okay but the rest look a sanitarium or jail.

How do you make underground parking in soil that is probably silt?

The only thing that would make most happy is cancelling all of it and starting from scratch. Again a deadline should have happened and the land sold to someone who listened to the people of Port.

No more than three stories high by-law should have been honoured!
 Shame on City Hall. Shame on OMB

We are now where they want us. You're either "with us or against" attitude. Greed and a dead village for another 10 years.

Elite Town Fortress we'll call it as we are slowing losing a 'regular people' place.

(I couldn't make the meeting unfortunately)

S McD

St Catharines ON

On a personal note. Is your mother Josephine?

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Ritchie, Scott

From: stephen bedford <[REDACTED]>
Sent: Friday, September 29, 2017 10:42 AM
To: Ritchie, Scott
Cc: Colin Johnston; Hank Beekhuis; David Bergen
Subject: Preliminary Comments Fortress

Hi Scott,

Sorry i could be at the meeting the other night as I had another meeting. However the newspaper seemed to describe the meeting we all expected. Was there anything new from your perspective that came out?

It's pretty difficult to speak to the development proposal without the secondary plan but I do have a couple of preliminary comments to offer.

I do believe however that the developer has made some good efforts in trying to create a compatible relationship with the existing buildings and the community.

Overall I think the proposal is still too big. It is a very urban development in a canal Village. The examples Fortress presents in support of the redevelopment beside a canal are all in European cities, not villages. Even given those examples, in my experience in travelling there are many renovated warehouses along canals in cities that are not as high as Fortress is proposing. The warehouses reflect a building technology that didn't support the height and their renovation did not include making them higher.

The overall intensity needs to be measured against the infrastructure carrying capacity of the harbour area. That's hard to measure without the complete study that a secondary plan will provide.

Given the increase in grade from the east and north to Main Street I would suggest there is still too much height. A 10 story building might work better and be less overpowering and would relate better to the other buildings in Port.

Could I suggest the role of Main St be reconsidered. It seems to act as a back door to the waterfront and Lakeside park. Main Street is the main central corridor / road in Port and consideration should be given to its function extending all the way down to Lakeside Park. If two way traffic isn't possible and Main St remains one way out of Lakeside park perhaps the pedestrian treatment could be given more emphasis. Wide sidewalk(s?) into the Park and have the residential building step back after 3 or 4 floors as it does along Lock St. A 10 story building along Main would still be the highest building for a village by some margin.

The developer is proposing to maintain the view of Lakeside park, the parking lot and the Lake down Hogan's Alley. If that's the case the bridge across Hogan's alley at the north end is counterproductive. The bridge cuts off the view, creates a very closed in perspective and I would suggest it be removed.

Some preliminary thoughts. Look forward to continuing the conversation and to see the construction of a great and "compatible" development next year.

Best Wishes

Stephen

Stephen Bedford MCIP, RPP, PLE

[REDACTED]

[REDACTED]

[REDACTED]

[REDACTED]

[REDACTED]

[REDACTED]

[REDACTED]

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Ritchie, Scott

From: Steve Janzen [REDACTED]
Sent: Wednesday, September 27, 2017 3:53 PM
To: Ritchie, Scott; Riddell, Jim
Cc: Mayor & Council
Subject: Union Waterfront Letter of Support
Attachments: Balzac's Coffee Letter of Support 092717.pdf

Greetings Scott and Jim,

Balzac's Coffee is proudly located in Port Dalhousie and we look forward to attending tonight's open house. Between our two Niagara Café locations and those who commute to our Head Office in Ancaster, we employ more than 20 locally (including myself). In advance of the meeting I wanted to share our view on the proposed development. Our letter of support is attached. If you have any questions please get back to me at your convenience. Thanks,

Steve Janzen [REDACTED]
 [REDACTED]
 [REDACTED]



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Ritchie, Scott

From: Sumeth Tanyaovalaksna [REDACTED]
Sent: Sunday, October 01, 2017 8:55 AM
To: Ritchie, Scott
Subject: Re: Union Waterfront

Sir,

I am Port Dalhousie resident, and would like to express my opinion regarding the zoning of the new condo. According to the Union Waterfront, the corporation only assigns one parking per unit, plus .17 unit for visitor space. I am afraid that these parking assignments are inadequate for the current needs. Most of the residents in the Port Dalhousie area has 2 cars/household, which I believe that it will be true for the residents in the condo of the Union Waterfront. This will result in the overflow of the excess cars to the parking lot of the Lakeside Park, discouraging other St. Catharines to come and enjoy the park. Moreover, they also pay tax as much as residents of the Union Waterfront. Why do they have to give up their rights to the new condo residents.

Another issue is the traffic flow which I believe it's detrimental to Port Dalhousie residents. Opposite the planned exit/entrance of the new condo is a small road(Gary Road) for the cars of another condo which is under-construction. This will create a three way traffic just about 20-30 feet away from the four-way traffic (Lock & Main). In addition, the Lincoln Fabrics is being converted into a new condo units, which will bring more traffic into the area.

I can see the coming chaotic traffic and conflict between the new condo owners and the current residents of Port Dalhousie.

Thank you,

Sumeth Tanyaovalaksna

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Ritchie, Scott

From: Kortekaas, Tricia [REDACTED]
Sent: Thursday, September 28, 2017 12:13 PM
To: Ritchie, Scott; Garcia, Carlos; Williamson, Bruce
Cc: [REDACTED]
Subject: Port Dalhousie Union Waterfront: Let's move forward!

Port Dalhousie Union Waterfront: Let's move forward!

The 2016 Census Profile of the city of St. Catharines indicates that residents aged 50+ account for 43.5% of the population of St. Catharines, with 30 to 50 year olds making up 23% of a total population of just over 133,000 residents. I belong to the minority 23% of the population and am a fairly new transplant to Port Dalhousie. In the five years I've been living here I've watched my neighbourhood slowly change from older, retired residents to younger families which I am sure is also happening elsewhere. The reasons my husband and I moved to Port were a) not only is it a beautiful place to live but b) because of the potential real estate investment as we knew the downtown core was set for re-development.

For too long I've been listening to a large majority of Port's long-time residents complain that any of the proposed developments would destroy their community, be an eyesore, and cause major disruptions due to lack of parking and increased traffic. It's my belief that should this development NOT come to fruition it will destroy what's left of this beautiful community. No disrespect the Port Dalhousie beautification committee, but the downtown core looks like a slum and putting lipstick on a pig will not attract people and new businesses. In fact, the people of Port Dalhousie have made things so difficult for new development that if this development is stopped eventually nobody will want to do business with us as we'll have proven that we can't be reasoned with.

I attended the public meeting at the Lion's Club on September 27th and one thing I heard repeated a number of times, besides the traffic and parking concerns, was "this is definitely a step in the right direction, however, I'm not sure it's the best thing". The world won't stop because we can't figure out what to do with that piece of land, it's been sitting vacant for too long. Many small businesses have come and gone because they just haven't been able to bring people to the downtown core and as Mr. Visser explained, those currently in operation are hanging on for dear life as the downtown core is currently a wasteland where nobody wants to visit and stay.

This development will bring long-term residents and patronage to that area **all year round** as opposed to 3 months out of the year **if** the weather is nice enough, and the lake water is safe to swim in. These are not the young "hooligans" experienced when the downtown core consisted of a number of bars; however I believe this is what the older residents are recalling when they think of this new development. This will be a combination of young families, professionals who want close access to the QEW and retirees looking for a nice quiet community to settle in. The businesses will not only attract tourists looking for shopping, but residents as well. Currently, everyone has to drive out of the area to do groceries, banking, shopping, etc. If we had these in our locale we're more than likely to visit the local shops than go elsewhere, with younger residents and families walking or biking rather than driving.

The "good ole days" are well and done, the world has moved on, and it's time Port Dalhousie moves forward before it becomes a permanent graveyard where none dare to tread. We need to look and move forward instead of backwards. This development **must** move forward and I commend the forward-thinking designers for providing electric vehicle charge stations, bike lanes, and community-use indoor and outdoor spaces.

In 50 to 100 years the current majority of 50+ year old residents will no longer be here, but perhaps their grandchildren and the current young families and residents will be. I hope this is kept in mind when the city is looking to decide whether or not to move forward with this design. We have nothing left to lose, but everything to gain.

Tricia Kortekaas

[REDACTED]
[REDACTED]
[REDACTED]

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Ritchie, Scott

From: Wolfgang Guembel [REDACTED]
Sent: Tuesday, September 26, 2017 3:25 PM
To: Ritchie, Scott; Riddell, Jim; Mayor & Council
Cc: Gregory Darte; George Darte; Sarah Casorso
Subject: Support of Union Waterfront

Hello Mayor, Council, Mr. Ritchie, and Mr. Riddell,

I'd like to indicate for your records that as managing partner of Lock Street Brewing Company and President of Four Black Sheep Inc., both business we run in Port Dalhousie proper, I and we support the development being proposed by Fortress Real Developments and the Union Waterfront project.

This project fits in our community and it provides an opportunity for the future of Port Dalhousie as well as contribution to the quality of life for those residents in the entire north end of our City. The preservation of heritage buildings, the additional public parking spots being created, the provision of public use space, the preservation of Hogan's Alley, and the provision of new retail and restaurant spaces, are all positive elements for our future in Port Dalhousie.

I encourage each of you to enable this project in all means you have available to you. It is in the better interest of the broader public here.

Thank you for your time,
 Wolf

--

Wolfgang Guembel [REDACTED]
 [REDACTED]
 [REDACTED]
 [REDACTED]

L2N 5B6

PUBLIC INFORMATION SESSION

September 27, 2017

16 Lock St. - Union Waterfront Zoning By-law Amendment and Official Plan Amendment

COMMENT SHEET

Comments may also be directed to sritchie@stcatharines.ca, and should be submitted no later than October 16, 2017.

Name Marilyn Weber Email [REDACTED]

I am concerned about density - 220 units are too many
I am concerned about height - a mountain is not better than
I am concerned about design compatibility - does not look
like a historic village
I am concerned about sustainability - if it doesn't sell
we will be left with a white elephant
I am concerned about traffic flow - in + out of Lakeside
+ in and out of Port Dalhousie
I am concerned about parking in Lakeside Park if condo
owners use it for parking

Thanks for letting me express my concerns
Marilyn Weber
[REDACTED]

PUBLIC INFORMATION SESSION

September 27, 2017

16 Lock St. - Union Waterfront Zoning By-law Amendment and Official Plan Amendment

COMMENT SHEET

Comments may also be directed to sritchie@stcatharines.ca, and should be submitted no later than October 16, 2017.

Name

Email

Irene McEvoy

[REDACTED]

I liked hearing the #'s for parking spaces.
you should make that more explicit.

i.e. previous spots = 80 units + 70 Hold + ? Theatre >

proposed spots = 220 units + 37 public

I appreciate your efforts to reduce # spots/unit
Those who buy your units will have to rethink
cars / household

I appreciate your concerns to work with the community.
Also, the previous model (I think) proposed a public
skating rink for the community. I hope the new model
can include one as well.

PUBLIC INFORMATION SESSION
September 27, 2017

16 Lock St. - Union Waterfront
Zoning By-law Amendment and Official Plan
Amendment

COMMENT SHEET

Comments may also be directed to sritchie@stcatharines.ca, and should be submitted no later than October 16, 2017.

Name JOE GALABRERA Email _____

GET IT DONE

ASAP

ST. CATHARINES

NEEDS

IT !!

PUBLIC INFORMATION SESSION

September 27, 2017

16 Lock St. - Union Waterfront Zoning By-law Amendment and Official Plan Amendment

COMMENT SHEET

Comments may also be directed to sritchie@stcatharines.ca, and should be submitted no later than October 16, 2017.

Name

Email

G. Ashworth

Parking is totally inadequate.
It's not right that the rest of the city
should have to suffer to find space to park
in order to visit the beach.

This is a public beach for all the
citizens ~~not~~ a private beach/park
for owners of the condos.

Shame on the council if this
is approved.

~~Not~~
~~for sale~~
Insure adequate on-site parking
at least 1350 or more
the number of units

PUBLIC INFORMATION SESSION
September 27, 2017

16 Lock St. - Union Waterfront
Zoning By-law Amendment and Official Plan
Amendment

COMMENT SHEET

Comments may also be directed to sritchie@stcatharines.ca, and should be submitted no later than October 16, 2017.

Name VICTOR CASSISI Email [REDACTED]

HOW WE ENDUP THIS MASTER OF
 BUILDING WHEN WE REJECTED THE
 TOWER WITH 80 UNIT NOW 220
 UNIT APPARTMENT?

WE DO NOT WANT TO LOOSE A
 SINGLE PARKING, AT LAKE SIDE
 PARK PARKING LOT.

THIS MASTER OF BUILDING DOES NOT
 BELONG IN PORT DALHOUSIE, BUT
 TORONTO!

WE ASK CITY COUNCIL TO
 REJECT AMENDMENT AND ZONING BY-LAW

MR. CASSISI
 A RESIDENT
 OF PORT DALHOUSIE FOR
 37 YEARS.

PUBLIC INFORMATION SESSION
September 27, 2017

16 Lock St. - Union Waterfront
Zoning By-law Amendment and Official Plan
Amendment

COMMENT SHEET

Comments may also be directed to sritchie@stcatharines.ca, and should be submitted no later than October 16, 2017.

Name

Shelley Pagula

Email

[REDACTED]

Parking for retail - where is that going to come from
we have two ways in & out of Port Authority & if cars
are parked on both sides of road it will be quite congested
& there won't be room for the buses
fire trucks - what happens if we need them & they get
boxed in by traffic

PUBLIC INFORMATION SESSION

September 27, 2017

16 Lock St. - Union Waterfront Zoning By-law Amendment and Official Plan Amendment

COMMENT SHEET

Comments may also be directed to sritchie@stcatharines.ca, and should be submitted no later than October 16, 2017.

Name J. HILDEBRAND ^{Nº} 105 1177
 Email [REDACTED]

As someone who has visited
 Lakeside Park since the 1930's
 in the days of streetcars and
 Lake Steamers I would be most
 interested in being assured that
 the parking for Lakeside Park will
 not be diminished by this project.

PUBLIC INFORMATION SESSION
September 27, 2017

16 Lock St. - Union Waterfront
Zoning By-law Amendment and Official Plan
Amendment

COMMENT SHEET

Comments may also be directed to sritchie@stcatharines.ca, and should be submitted no later than October 16, 2017.

Name

Email

LARRY KROEGER

St. Cath L2K SEC

2 am in favour of this
development!!

To: City of St. Catharines Councillors and City Planning Department

SUPPORT THE UNION WATERFRONT DEVELOPMENT

The business community of Port Dalhousie and St Catharines **supports Fortress Real Developments with their development plan and vision for Port Dalhousie.** We welcome the new businesses, jobs, and residents that the project will bring to St. Catharines and **we would like to see positive action to move it forward as soon as possible.** The new homeowners who will live on site will provide prosperity to a site that is in dire need of revitalization. The new residents will shop, meet with friends and family, utilize Lakeside Park, the marina and use the services that existing businesses in the area provide. In addition, new businesses will open as part of the project which will contribute much needed tax revenues and other economic benefits to the community.

BUSINESS NAME	ADDRESS	OWNER NAME	SIGNATURE
Your Boutique		Bayzat Hekher	Bayzat Hekher
SATORI SPA & NAILS		VAN HUYNH	
Glitz & Antiques		Paula Malmström	Paula Malmström

PUBLIC INFORMATION SESSION

September 27, 2017

16 Lock St. - Union Waterfront Zoning By-law Amendment and Official Plan Amendment

COMMENT SHEET

Comments may also be directed to sritchie@stcatharines.ca, and should be submitted no later than October 16, 2017.

Name Suzanne Kavanaugh Email [REDACTED]

I attended the Public Information Session on September 27, 2017. I went with an open mind. I carefully looked at the drawings and I read the handout that was given out at the door.

Upon closer scrutiny I noticed that only 1 parking space was allowed per unit and virtually nothing for a visitor(s). Here are my observations:

- 1) The proposed structure is way out of proportion to the existing land proposal.
- 2) The height of the proposed condo is much too high for the area and surrounding businesses and residential homes.
- 3) A project of this magnitude would shut down PORT DALHOUSIE for a very long time and cause endless amounts of frustration and stress for the taxpayers →

who chose to make PORT DALLHOUSIE
their home.

I am all for "good" development.
Development that is sustainable, reasonable
and adds value to the existing residents.

Look at Burlington: a well planned city
with broad boulevards, dynamic arts
and culture, well-supported by the community.
You must have harmony, a place to grow
and an eye to the future.

Burlington "planned" their city. They are
successful on many fronts. They attract
go getters. I envy the community.

We could have it too here in St. Catharines
but we must be diligent, forward-
thinking with an eye to the future.

Yes, we need more tax revenues but
in the end if you only look at the
bottom line you will be forever doomed
to be swindled out of a unique area
that historically grew slowly. Yes, there
is a need to begin change. But, think
about what made us great in the first
place: a great place to raise a family,
retire, a wonderful climate, the Grape &
Wine industry, the tender fruit industry,
Brock University with outstanding professors,
Niagara College, great roads, wonderful
restaurants and diversity of people.
Folks ART Festival is the culmination
of a coming together, a celebration of people,
food and culture. THANK YOU

RECEIVED
MAYOR'S OFFICE
S. Ritchie
OCT 2 2017

PUBLIC INFORMATION SESSION

September 27, 2017

16 Lock St. - Union Waterfront
Zoning By-law Amendment and Official Plan
Amendment

COMMENT SHEET

Comments may also be directed to sritchie@stcatharines.ca, and should be submitted no later than October 16, 2017.

Name

Bonnie Kattan

Email

[REDACTED]@sympatico.ca

I am very much in favour of the proposal.
I don't care how tall it is. In St. Cath we can no longer build out so we have to build up to increase revenue & tax base.
I was happy to hear about traffic flow & parking. Valid concerns & I feel that they have been addressed. I live near the very large building at 614 Lake St. A lot of people live there. There is never an influx of traffic. A lady mentioned the re-routing of traffic when the QEW was closed due to an accident. This does not happen often (thankfully) and I don't believe that we can hold up this development because of anomalies.

Personally, I don't care about "old" buildings or heritage but for those who do care I think their concerns have been addressed. There were many points made about the new condo at Lock St. & Dalhousie but not everyone drives in and out at the same time.

It sounds like traffic was the main issue. I feel confident that the developers & city planners would not put this forward if it would make Port a driving & parking nightmare. It will be busy during Henley & July 1 (etc.) but I think that's a good thing.

I'm in favour & want to see this move forward.

Port ~~is~~ is a sad mess.

It's time !!

PUBLIC INFORMATION SESSION

September 27, 2017

16 Lock St. - Union Waterfront Zoning By-law Amendment and Official Plan Amendment

COMMENT SHEET

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Name Lesley Hirst

Email [REDACTED]

① Outcomes dashboard sampling needs to happen
to that end.

② Too many units

③ Too tall

④ Love design style - keeping heritage

* ⑤ Parking: there isn't enough parking already

⑥ Necessity - stairs to upper levels are
not noise friendly

⑦ Sustaining for shops/commercial - we don't
want commercial stores sitting empty

⑧ Traffic management concerns

⑨ Water table

* ⑩ People have more than 1 car per unit

but there is only given 1 parking spot.

⑪ Cont. water table management is already
poor with huge odour problems - there
needs to be improvements to this + stop the odour!

⑫ Intervening of late work developments will
be required to accommodate the extra folks.

They are not wide enough.

⑪ - If public meeting spaces are not enough

for 2nd unit.

It is not appropriate to assume people would give up 1 core or country. We are a nation.

⑫ Question - Are the developers prepared to modify the plan if there is a certain number of opposition e.g. over 1 million etc.

Answer:

Sensitive design - to heritage

In general ready for development

Electric vehicle plugin - we have one.

Innovative -

Including Logan's alley

Sluggish design.

Summary

Would like the development ~~road~~ because I want to buy one! but it does need to be solved in unit to height -

Technologies if any entry is best to read!

PUBLIC INFORMATION SESSION September 27, 2017

16 Lock St. - Union Waterfront Zoning By-law Amendment and Official Plan Amendment

COMMENT SHEET

Comments may also be directed to sritchie@stcatharines.ca, and should be submitted no later than October 16, 2017.

Name Bruce Hogg

Email [REDACTED]

Thank you for providing an opportunity to comment. I attended the presentation at the Lion's club and came away with some optimism that revitalization may finally begin to proceed. That said, there are a few aspects of the proposal that concern me, and if they aren't made right from the beginning of the project can not be easily remedied.

I believe that the developer is taking advantage of access to a beautiful public area, and one that the taxpayers have recently spent a fair amount of money upgrading. It isn't clear to me what the developer is giving back to the community in terms of enhanced public spaces, new public recreational opportunities, or facilities. This is our opportunity to ensure a win-win for both the developer and the community and I think we need the approval to be contingent on some improvements....maybe a small community theater like what we lost with the Port Mansion, artificial ice skating rink, nice upper level community accessible room so we could all share the view. This an opportunity for the City to show that it supports enhancements for the residents and not just an enhanced tax base.

My second concern is parking. I don't know a single family who own only one car. There is very little public transportation through Port Dalhousie. Its simply not reasonable to expect the people who buy into the new condos will suddenly decide to get by with one vehicle. It would be hypocritical of me to expect others to live in a way that nobody I know would accept. It would also be nice to attract young families and fill the building with a mixed demographic, very few of which would be able to manage with one car. Unless something more reasonable, (ie: 2 spots/vehicle) is part of the approval the extra cars will end up parked locally, and a never ending battle will ensue. One end result may be a condo filled with a mix of unhappy people constantly battling local parking enforcement, and very old people who are finally at a stage in life where one car is acceptable.

I'm quite optimistic that with some tweaks to address the above points the new development will be a big step forward and a great option to (finally) revitalize the "diamond in the rough" that Port has become.

Thank you in advance...
Bruce Hogg

Ritchie, Scott

From: Sendzik, Walter
Sent: Tuesday, October 24, 2017 1:48 PM
To: [REDACTED]
Cc: Ritchie, Scott
Subject: FW: A crucial issue!

Good Afternoon Stan and Corrie,

Thanks for your email and feedback. The community has been waiting for progress at the development site for years now. As the development proposal is now going through the formal planning process there will be more opportunities for public input. Comments received by the Planning Department will be considered in preparation of a staff report for Council to consider.

By way of this reply, I'm copying Scott Ritchie, planner, who is compiling the public feedback for this proposal.

Thanks again for your input as a Port Dalhousie resident. I look forward to reviewing the report and considering the public comments when the development comes to City Council.

Sincerely,
 Walter

Walter Sendzik
 Mayor
Email: wsendzik@stcatharines.ca
Tel: 905.688.5601 x1540

From: [REDACTED]
Sent: Tuesday, October 24, 2017 9:27 AM
To: Mayor's Administration
Cc: Haywood, David; Harris, Matthew; Britton, Mike; Bellows, Sandie; cgarcia@st.catharines.ca; Williamson, Bruce; Stevens, Jennifer; Kushner, Joe; Sorrento, Sal; Siscoe, Mathew; Phillips, Bill; Elliott, Mark; hbeekhuis@cogeco.ca
Subject: A crucial issue!

Dear Members of City Council:

My wife and I are both in our eighties. We have lived in St. Catharines since 1970. I personally (Stan) arrived from the Netherlands in 1953 and lived for some months in Port Dalhousie. Already then I fell in love with this unique village by Lake Ontario. For years now we have made nearly daily visits to Port, just to enjoy the quiet, the ambience and the lake.

With growing alarm we now read about the plans to greatly expand this quiet place. We read about Port Place, Lincoln Fabrics, the Legion, and also what is happening with the Beaches. To be sure, we are happy with the improvements made over the years, to pretty the place up so to speak. We are not against progress. We are still so pleased that the public still has access to the piers, albeit only in part, to the Lake Side Park, the new walking bridges, the beach, the new pavilion, etc. But what will now happen to this beautiful Heritage Town?

We do not have to remind you that there is only ONE road leading into and out of town. We sincerely hope that you rethink the plans for Port Place, Lincoln Fabrics and the Legion which, if approved, might possibly add close to 500 new condos to this small town. The people who live in these condos drive cars, possibly totalling 700 to 800 cars. All these cars would use only that one road in and out. All these cars need to have parking spaces.

In addition to the "car problem" and growing traffic congestion, have you thought about the effect of nearly doubling the Port Dalhousie's present population, the enormous increase in the number of cars and the huge pressure on the infrastructure?

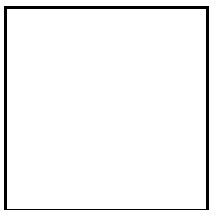
Consider also how the total of these proposed developments will impact the freedoms present visitors have to use the beach, to stroll on the pier and to enjoy the present recreation area for children and their parents.

Both of us hope you will do your best to preserve this jewel we have by the lake. It takes guts to stand up to money-hungry developers. It simply is not the right thing to allow proposals to go forward which will do permanent damage to Port Dalhousie.

Sincerely yours

Stan and Corrie de Jong

 1G8



This email has been checked for viruses by Avast antivirus software.
www.avast.com

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Ritchie, Scott

From: Sendzik, Walter
Sent: Monday, October 30, 2017 9:51 AM
To: [REDACTED]
Cc: Ritchie, Scott
Subject: RE: Fortress Capital's proposal for Port Dalhousie

Hi Mr. Rempel,

Thanks for your email and feedback. The community has been waiting for progress at the development site for years now. While I'm not sure which buildings you are referencing as "historic" you can be assured that any structure or building that is historically designated is protected from demolition. As the development proposal is now going through the formal planning process there will be more opportunities for public input. Comments received by the Planning Department will be considered in preparation of a staff report for Council to consider.

By way of this reply, I'm copying Scott Ritchie, planner, who is compiling the public feedback for this proposal.

Thanks again for your input as weekly visit to Port. I look forward to reviewing the report and considering the public comments when the development comes to City Council.

Sincerely,
 Walter

Walter Sendzik
 Mayor
Email: wsendzik@stcatharines.ca
Tel: 905.688.5601 x1540

From: [REDACTED]
Sent: Wednesday, October 18, 2017 10:17 AM
To: Mayor & Council
Subject: Fortress Capital's proposal for Port Dalhousie

Dear City Council,

I have watched in dismay as Port Dalhousie's historical core was being ravished by self-serving developers.

Sadly, key buildings have already been destroyed for no good purpose, and their historical value lost forever. It's doubly sad that the residents have been unable to preserve their heritage or develop their municipality in the face of well-moneyed and politically favoured outsiders.

As a weekly visitor to Port, my sympathies are with the residents and their vision for Port Dalhousie.

I trust it's not too late,

Abraham Rempel
Author of *The Book of NOTS in Science & Religion*

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Ritchie, Scott

From: Sendzik, Walter
Sent: Monday, October 30, 2017 9:22 AM
To: Elaine Manocha
Cc: Ritchie, Scott
Subject: RE: Proposed Port Tower

Follow Up Flag: Follow up
Flag Status: Flagged

Hi Elaine,

Thanks for your email and feedback. The community has been waiting for progress at the development site for years now. As the development proposal is now going through the formal planning process there will be more opportunities for public input. Comments received by the Planning Department will be considered in preparation of a staff report for Council to consider.

By way of this reply, I'm copying Scott Ritchie, planner, who is compiling the public feedback for this proposal.

Thanks again for your input as a Port Dalhousie resident. I look forward to reviewing the report and considering the public comments when the development comes to City Council.

Sincerely,
 Walter

Walter Sendzik

Mayor

Email: wsendzik@stcatharines.ca

Tel: 905.688.5601 x1540

From: Elaine Manocha [REDACTED]
Sent: Thursday, October 26, 2017 7:41 PM
To: Mayor & Council
Subject: Proposed Port Tower

To Whom It May Concern

I was at the meeting last night and listened to the Heritage Development Committee talk about the secondary proposal for the tower. Not impressed, but they did give the residents availability to write down ideas and concern about Port.

I still feel the tower is too modern in design, that the architects did not put any work into creating a design that would fit in with the style of buildings already in Port

The tower is too large for that area and that it is only a money making situation now and that the more condos available the more money. The people who would want to live in those condos would reconsider living there because of the history and dissatisfaction of the residents of Port. I do not think there would be enough people to fill all the condos they want to build.

The new condo design is too large, too modern and too ugly for that area. Also there is no need for a theatre there since the one in the city has difficulty getting enough shows.

The infrastructure of Port would need to be completely redone to accomodate the increase in population and usage of the various needs of the residents re: hydro, water, sewers etc. Who is paying for this? I do not want my taxes to increase to pay for someone else making money on overcrowding an area with condos so they can have a substantial profit with this project.

I agree that a condo would be very good there in Port but not the one they are proposing. It needs to be smaller in height, 8 floors max or maybe 10 and a better design with enough parking needs for the residents and visitors.

Thank you
Elaine Manocha

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Ritchie, Scott

From: Sendzik, Walter
Sent: Wednesday, November 01, 2017 8:59 AM
To: Tina Summerskill
Cc: Ritchie, Scott
Subject: RE: Serious concerns re: Tower Proposal

Good Morning Tineke,

Thanks for your email and feedback. The community has been waiting for progress at the development site for years now. As the development proposal is now going through the formal planning process there will be more opportunities for public input. Comments received by the Planning Department will be considered in preparation of a staff report for Council to consider.

By way of this reply, I'm copying Scott Ritchie, planner, who is compiling the public feedback for this proposal.

Thanks again for your input as a Port Dalhousie resident. I look forward to reviewing the report and considering the public comments when the development comes to City Council.

Sincerely,
 Walter

Walter Sendzik

Mayor

Email: wsendzik@stcatharines.ca

Tel: 905.688.5601 x1540

From: Tina Summerskill [REDACTED]
Sent: Tuesday, October 31, 2017 2:47 PM
To: Mayor & Council
Subject: Serious concerns re: Tower Proposal

To The Mayor and Council,

I have been a resident of Port Dalhousie since 1978. It has been a privilege to be part of this community and my husband and I have participated in and watched with great interest and enthusiasm, till the Tower Proposal, Port Dalhousie's sustainable development which for the most part in our opinion has been done with integrity and always keeping in mind that Port Dalhousie is special to the citizens of St. Catharines, and visitors from all over Ontario and the

world. It is special because it is a quaint seaside village. We were there during the beginning days of the PDQ and proud to see how local citizens had the best interests of Port Dalhousie in mind and worked together to lay the groundwork for what has become a very desirable part of St. Catharines.

Development is good, but must always be done in keeping with the local environment and needs of the citizens. The citizens of St. Catharines not only need a beach to enjoy but are entitled to it. What I foresee now is that the parking lot and beach area will by de facto be the exclusive domain of the residents of the tower. I say this because with the proposed occupancy these people will of course use the park and parking lot for their extra cars, their guests, and tradespeople required to maintain the tower. There is so little public beach space for the citizens of St. Catharines as it is, that to deprive them of reasonable access and enjoyment of that space for the sake of satisfying builders who want to squeeze every dollar out of their investments is in my opinion short term thinking that can never be undone if the tower is approved.

The approval by the OMB for the height was the assurance of the theatre and hotel, not more condos which now appears is what they want. We all have to abide by laws and can't change them just to suit our wants. The OMB approved the height with a theatre and hotel, and that is what should be constructed and if they don't want to abide by the OMB decision then we start all over again. By starting all over again I do not mean nothing happens, but all buildings should be in keeping with a 19th century heritage village for all citizens to enjoy. Developers I am sure can still make money out of scaled down buildings, but obviously not as much, and therein lies the problem.

I would hazard a guess that the vacant space at the former GM site on Ontario street could very well support the number of condos that the developers want to build, but my guess is they want to build their condos in the most beautiful and special place in St. Catharines because they can sell their condos for more money. It appears to me it is not about condos, but money. As a city we have to decide if we want to sell the birthright of all citizens, now and to come, their reasonable access and enjoyment of the beach area in Port Dalhousie, for money.

Many years ago the Niagara Parks Commission felt it was important to keep the river side of the parkway for the pleasure of the citizen of this area. They could I imagine have decided to sell the waterfront side to developers. Thousand of people enjoy that drive, even Winston Churchill is credited with saying that it was the prettiest Sunday drive in the world. I sincerely hope that you, our Mayor and Council, will consider the future of this precious space in our city for the pleasure of all its citizens.

Respectfully submitted by,

Tineke Summerskill

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Ritchie, Scott

From: Rorison, Julie
Sent: Wednesday, November 01, 2017 11:35 AM
To: Hughes, Julie; Kurek, Leanne
Cc: Ritchie, Scott
Subject: FW: Port Dalhousie Development

Scott Ritchie is planner and is responding to all comments and queries. Perhaps Scott can also explain the planning / comment process that is being followed and that there will be at least one more public meeting.

Julie Rorison
 Chief of Staff Mayor's Office
Email: jrorison@stcatharines.ca
Tel: 905.688.5601 x1567

From: [REDACTED]
Sent: Wednesday, November 01, 2017 11:22 AM
To: Mayor's Administration <mayor'sadministration@stcatharines.ca>
Cc: Garcia, Carlos <cgarcia@stcatharines.ca>
Subject: Fwd: Port Dalhousie Development

Hello,

I sent the below email to Mr. Riddell on October 24, 2017. I have still not received back a response. I would like a response regarding my below email. If the planning department does not feel that they need to respond I will start directing all my questions to the mayors office.

Dana Rogers

Begin forwarded message:

From: [REDACTED]
Date: October 24, 2017 at 7:11:00 PM EDT
To: jriddell@stcatharines.ca
Subject: Port Dalhousie Development

Hello,

I have been hearing chatter about the port dalhousie development. I figured I would contact you directly to determine what is accurate:

1 - is it true there is only one parking space needed per condo unit ?

2 - is it true that there will be paid parking in the Port Dalhousie lot when the fortress condos are built ?

Just a few simple things I want to understand. Also, if there is only one car per condo unit how can the city justify this. I thought it was standard to have 1.25 per residential spot ?

Concerned citizen,
Dana

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Ritchie, Scott

From: Sendzik, Walter
Sent: Wednesday, November 01, 2017 9:06 AM
To: Marilyn M
Cc: Ritchie, Scott
Subject: RE: Port Tower project

Good morning Marilyn,

Thanks for your email and feedback. The community has been waiting for progress at the development site for years now. As the development proposal is now going through the formal planning process there will be more opportunities for public input. Comments received by the Planning Department will be considered in preparation of a staff report for Council to consider.

By way of this reply, I'm copying Scott Ritchie, city planner, who is compiling the public feedback for this proposal.

Thanks again for your input as a Port Dalhousie resident. I look forward to reviewing the report and considering the public comments when the development comes to City Council.

Sincerely,
 Walter

Walter Sendzik

Mayor

Email: wsendzik@stcatharines.ca

Tel: 905.688.5601 x1540

From: Marilyn M [REDACTED]
Sent: Tuesday, October 31, 2017 6:38 PM
To: Mayor & Council
Subject: Port Tower project

Good evening Councillors,

I would like to register a strong no to the new Port proposal, please read why...

1. There are already enough cars trying to find parking at Lakeside Beach, and this condo would find many more people with cars using this public space as their own parking lot. I understand there is only one spot per condo unit, and most people have two cars in their family. My grandchildren would find it very disappointing to come to the beach with their Mom and find it full, all the time. And charging would not be ideal, either. (Not to mention the added cars with all the extra new projects starting.)

2. And very importantly, it is not keeping with the heritage village of a 6-story height. It would add nothing visually for tourists to come here, as it would be more of an "eyesore". Tourists come to places such as Port, for its old, charming features.
3. Traffic in and out of Port is already quite a challenge on most nice days of spring, summer, and fall, especially weekends. Those of us who live here expect that, but don't need to be overwhelmed in this regard. A significant percent of infrastructure would have to be upgraded and maintained, as well.
4. The OMB approved this many years ago, with all the extras of retail and theater. Now these things are phased out, with just an ugly tower to propose. Please vote no.

This just doesn't do anything for this city. I hope you will all listen to the majority of people of St. Catharines (and far beyond this area) who have just begun to voice their no-vote. We had "a poll" door-to-door here in Port last week, asking us to sign a petition to "build something". This was very deceiving, in that they did not talk about height, (which I told them I know it is 14 story) and did not register any "no" votes, like mine, as they left without my name or signature. Please don't be fooled by these types of polls sent your way. Thank you for your time. Marilyn Mason, [REDACTED]

Click [here](#) to report this email as spam.

Ritchie, Scott

From: Sendzik, Walter
Sent: Wednesday, November 08, 2017 1:24 PM
To: Glenys Moss
Cc: Ritchie, Scott
Subject: RE: comment re l4 storey tower

Follow Up Flag: Follow up
Flag Status: Flagged

Good afternoon Glenys,

Thanks for your email and feedback. The community has been waiting for progress at the development site for years now. As the development proposal is now going through the formal planning process there will be more opportunities for public input. Comments received by the Planning Department will be considered in preparation of a staff report for Council to consider.

By way of this reply, I'm copying Scott Ritchie, planner, who is compiling the public feedback for this proposal.

Thanks again for your input as a Port Dalhousie resident. I look forward to reviewing the report and considering the public comments when the development comes to City Council.

Sincerely,
 Walter

Walter Sendzik

Mayor

Email: wsendzik@stcatharines.ca

Tel: 905.688.5601 x1540

From: Glenys Moss [REDACTED]
Sent: Wednesday, November 08, 2017 11:38 AM
To: Mayor & Council
Subject: comment re l4 storey tower

To all:

The 14 storey tower eye-sore proposed for the beach area is so bizarre as to beggar belief. I have been visiting Port Dalhousie for over 20 years and now live in the vicinity. The beach and park used to be a destination. No longer, since the devastation of the area by the avarice of the so-called developers. and the disinclination of political representatives to protect the enjoyment of their constituents.

There isn't a shred of justification for this building to exist other than profits for the perpetrators.

In what was a village with a certain charm, - normally perceived as a benefit for both residents and tourists - we now anticipate this monstrosity dominating and distracting from any enjoyment of the area.

The traffic jams will be horrendous - as they are now in the summer. This, without considering what will happen with the development of the Lincoln and Legion buildings. The residents of the tower will not be restricted to one car per unit as there is no public transit to shopping areas which are accessible..

The parking lot will be jammed, unquestionably. So where else can people park?

The proposed building should be slapped in the middle of downtown, not sitting in a park and beach area, a scar on the landscape. And probably of less concern to your group - it's such very bad taste.

G.Moss. St. Catharines..

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Ritchie, Scott

From: Hughes, Julie
Sent: Monday, November 06, 2017 4:31 PM
To: [REDACTED]
Cc: Ritchie, Scott
Subject: RE: Condo units, Lakeside Park

Follow Up Flag: Follow up
Flag Status: Flagged

Hi John,

On behalf of Mayor Sendzik, I'd like to acknowledge your email.

The community has been waiting for progress at the development site for years now. As the development proposal is now going through the formal planning process there will be more opportunities for public input. Comments received by the Planning Department will be considered in preparation of a staff report for Council to consider.

I am copying the project planner, Scott Ritchie, who will ensure your comments are part of that preparation.

Thanks again for your feedback. The Mayor and Council forward to reviewing the report and considering the public comments when the development comes to City Council.

Best regards,
 Julie

Julie Hughes
 Administrative Assistant - Mayor's Office
Email: jhughes@stcatharines.ca
Tel: 905.688.5601 x1542

From: [REDACTED] **On Behalf Of** John vanderZalm
Sent: Friday, October 27, 2017 4:12 PM
To: Mayor's Administration
Subject: Condo units, Lakeside Park

As someone born and bred in Port Dalhousie, I am concerned with this cities affiliation with the condo development under Fortress Development. My search shows that they are in a legal battle with 4 billion \$ being thrown around. As a citizen and potential consumer, I am quite concerned. I do agree a development is in order there but with the other projected condos being built at the old Lincoln Fabrics and I hear on the opposite side between both proposals, perhaps we can down size the first one to perhaps 150 units allowing the other builds

and not getting this huge traffic problem that everyone is talking about. Anyway, what do you feel about the lawsuit and Fortress?

Origin: <https://www.stcatharines.ca/en/governin/Mayor-Walter-Sendzik.asp>

This email was sent to you by John vanderZalm [REDACTED] through
<http://www.stcatharines.ca/>.

Ritchie, Scott

From: Sendzik, Walter
Sent: Thursday, November 09, 2017 2:11 PM
To: victor cassisi
Cc: Ritchie, Scott
Subject: RE: Port Dalhousie new building

Good afternoon Mr. Cassisi,

Thanks for your email and feedback. The community has been waiting for progress at the development site for years now. As the development proposal is now going through the formal planning process there will be more opportunities for public input. Comments received by the Planning Department will be considered in preparation of a staff report for Council to consider.

By way of this reply, I'm copying Scott Ritchie, planner, who is compiling the public feedback for this proposal.

Thanks again for your input as a Port Dalhousie resident. I look forward to reviewing the report and considering the public comments when the development comes to City Council.

Sincerely,
 Walter

Walter Sendzik
 Mayor
Email: wsendzik@stcatharines.ca
Tel: 905.688.5601 x1540

From: victor cassisi [REDACTED]
Sent: Thursday, November 09, 2017 1:13 PM
To: Mayor & Council
Subject: Port Dalhousie new building

From: victor cassisi [REDACTED]
Sent: November 7, 2017 6:36 PM
To: mayorcouncil@stcatharines.ca
Subject: Port Dalhousie new building

To the mayor and city council:

My wife and I have lived in Port Dalhousie since 1980, originaly from Toronto. On September 25, 2017 I did attend the meeting and presentation by the city and builder in regard the new revised tower. Most people

including me were against the new apartments. It looks like a monster of building! It does not belongs in Port Dalhousie but Toronto.

Yes, we all want to have some thing build on the site, not that! also I heard the city wants to put parking meters at lake side parking lot.

when citizen of St. Catharines go to the beach or the park don't have to worry to feed the meters. the parking lot is for all of us, not for the use of the residents of the new project.

the builder must provide enough parking spaces .

Therefore, my wife and I are urging the mayor and council to vote against the revised project.

sincerely,

Victor and Karin Cassisi

[REDACTED]

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November 14, 2017

Mayor Walter Sendzik and Members of Council
50 Church St.
St. Catharines L2R 7C2

Re: Fortress Developments Proposal for a Condo Tower in Port Dalhousie

Dear Mayor and Councillors:

I write on behalf of Architectural Conservancy of Ontario to express our grave concern and strong opposition to the Fortress Developments Proposal for a Condominium Tower in Port Dalhousie.

Through education, advocacy and direct action, Architectural Conservancy Ontario has championed the preservation of Ontario's architectural and environmental heritage since 1933. We are a grassroots organization with 25 branches across Ontario, including an active branch in your municipality.

A similar development proposal was opposed by the community over a decade ago. In 2006, ACO wrote to both your Council and Regional Council in opposition to the tower proposal then before you. We also submitted a letter and spoke in opposition at the public meeting the City held. The submission from former ACO President Patricia Robinson Malicki, who was born and raised in Niagara, stated:

Council has an obligation and responsibility, under the Ontario Heritage Act and the 2005 Provincial Policy Statement, to preserve its cultural heritage. Port Dalhousie is special and very unique -- not just in Canada, but in the world. It is one of the few remaining Canal towns left in the world. Again, it is Council's obligation and responsibility, under the Ontario Heritage Act and the 2005 Provincial Policy Statement, to preserve this unique town.

Despite widespread opposition in the community and from heritage organizations across the Province, the Council of the day narrowly approved that proposal. Subsequently, a new Council was elected that appreciated the value of Port Dalhousie's heritage and the developer's heritage permit was denied. Regrettably and despite the valiant efforts of your City and numerous volunteers, the OMB Board, at that time, approved the tower proposal. All these years later however, the tower the OMB approved has not been built.

403-10 Adelaide Street East Toronto, ON M5C 1J3
T 416.367.8075 TF 1.877.264.8937 F 416.367.8630 E manager@arconserv.ca

www.arconserv.ca

The past. Our present. Your future.



Now Council has another proposal that calls for almost triple the density of condominiums. We find the current design even more inappropriate. We believe its size and proposed density will overwhelm your cherished 19th century village and impede resident access to the village and the park.

Thus, we urge you to, once again, reject this proposal and seek an alternative that fits in with the heritage values of Port Dalhousie and provides community benefits.

Sincerely,

A handwritten signature in black ink that reads "F. Leslie Thompson". The signature is written in a cursive, flowing style.

F. Leslie Thompson
President

cc: ACO St. Catharines Branch

Ritchie, Scott

From: Sendzik, Walter
Sent: Tuesday, November 14, 2017 2:19 PM
To: 'Stacey'
Cc: Ritchie, Scott
Subject: RE: port Towers

Good afternoon Stacey

Thanks for your email and feedback. The community has been waiting for progress at the development site for years now. As the development proposal is now going through the formal planning process there will be more opportunities for public input. Comments received by the Planning Department will be considered in preparation of a staff report for Council to consider.

By way of this reply, I'm copying Scott Ritchie, planner, who is compiling the public feedback for this proposal. The parking ratio is one element of the development proposal which will be given consideration by Council.

Thanks again for your input as a Port Dalhousie resident. I look forward to reviewing the report and considering the public comments when the development is presented at a Public Meeting at City Council on December 12th. That meeting will start at 6pm at the FirstOntario Performing Arts Centre. All members of the public are welcome to attend.

Sincerely,
 Walter

Walter Sendzik

Mayor

Email: wsendzik@stcatharines.ca

Tel: 905.688.5601 x1540

From: Stacey [REDACTED]
Sent: Saturday, November 11, 2017 3:04 PM
To: Mayor & Council
Subject: port Towers

Hello,

I have been informed that Port Towers area is zoned for 1.75 parking places per unit but with the current plan they have 1.17 parking places. How is this possible?

Stacey Richter, a concerned citizen

Ritchie, Scott

From: Sendzik, Walter
Sent: Tuesday, November 14, 2017 2:16 PM
To: 'John Hewitt'
Cc: Ritchie, Scott
Subject: RE: Union Waterfront Condo Proposal

Follow Up Flag: Follow up
Flag Status: Flagged

Good afternoon John,

Thanks for your email and feedback. The community has been waiting for progress at the development site for years now. As the development proposal is now going through the formal planning process there will be more opportunities for public input. Comments received by the Planning Department will be considered in preparation of a staff report for Council to consider.

By way of this reply, I'm copying Scott Ritchie, planner, who is compiling the public feedback for this proposal.

Thanks again for your input as a Port Dalhousie resident. I look forward to reviewing the report and considering the public comments when the development comes to City Council on December 12th. That meeting will start at 6pm at the FirstOntario Performing Arts Centre. All members of the public are welcome to attend.

Sincerely,
 Walter

Walter Sendzik

Mayor

Email: wsendzik@stcatharines.ca

Tel: 905.688.5601 x1540

From: John Hewitt [REDACTED]
Sent: Sunday, November 12, 2017 11:16 AM
To: Mayor & Council
Subject: Union Waterfront Condo Proposal

To City Council:

I believe most people realize that change is needed in the heritage district of Port Dalhousie, but hopefully the changes that do come about are the result of collaboration and co-operation among the developers, the city planners and the collective voice of

Port Dalhousie. The current plan of Fortress' condo proposal certainly looks like the 220 condos will bring with them parking and traffic problems. There are three other condo developments in the works in this area (perhaps totalling 450 units) so there must be planned development which is suitable for this village community and its historical and cultural history. Let's make common sense a priority when making changes in Port Dalhousie. Big is not always better.

Thank You,
John Hewitt,
St. Catharines

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Ritchie, Scott

From: Sendzik, Walter
Sent: Tuesday, November 14, 2017 2:26 PM
To: 'tanya pavelko'
Cc: Ritchie, Scott
Subject: RE: Port Dalhousie

Good afternoon Tanya,

Thanks for your email and feedback. The community has been waiting for progress at the development site for years now. As the development proposal is now going through the formal planning process there will be more opportunities for public input. Comments received by the Planning Department will be considered in preparation of a staff report for Council to consider.

By way of this reply, I'm copying Scott Ritchie, planner, who is compiling the public feedback for this proposal. The parking ratio is one element of the development proposal which will be given consideration by council.

Thanks again for your input as a Port Dalhousie resident. I look forward to reviewing the report and considering the public comments when the development comes to the Public Planning meeting City Council on Tuesday December 12th. That meeting will start at 6pm at the FirstOntario Performing Arts Centre. All members of the public are welcome to attend. If you are interested in speaking, please contact Deanna Haine, dhaine@stcatharines.ca

Sincerely,
 Walter

Walter Sendzik
 Mayor
Email: wsendzik@stcatharines.ca
Tel: 905.688.5601 x1540

From: tanya pavelk [REDACTED]
Sent: Thursday, November 09, 2017 4:51 PM
To: Mayor & Council
Subject: Port Dalhousie

It has recently come to my attention that the new development in Port Dalhousie is going to infringe on our community to the point of impacting the entire city. Port Towers area is zoned for 1.75 parking places per unit with the current plan they have 1.17 parking places. We need to actively write council and the mayor so they do not allow this to happen . .
 . Why? When I asked the city planning department if the Port Dalhousie Lot would become paid parking the

response, "There are no plans for the City to implement paid parking in Lakeside Park at this time. Changes to Lakeside Park parking operation, such as paid parking or time restrictions, may however be considered in the future as a means of better ensuring parking availability for visitors to Port Dalhousie." The best way to ensure parking availability for visitors to Port Dal and ensure our investment of 3 million in tax dollars spent on Lakeside Park and money meant for the Piers remains a community focus is to stop the size of this development. It is too large for this space. WE DO NOT WANT OUR PARK PRIVITIZED !!!

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Ritchie, Scott

From: Sendzik, Walter
Sent: Tuesday, November 14, 2017 2:24 PM
To: [REDACTED]
Cc: Ritchie, Scott
Subject: FW: We Don't Want Our Park Privatized

Follow Up Flag: Follow up
Flag Status: Flagged

Good afternoon Stephen,

Thanks for your email and feedback. The community has been waiting for progress at the development site for years now. As the development proposal is now going through the formal planning process there will be more opportunities for public input. Comments received by the Planning Department will be considered in preparation of a staff report for Council to consider.

By way of this reply, I'm copying Scott Ritchie, planner, who is compiling the public feedback for this proposal.

The parking ratio is one element of the development proposal which will be given consideration by council.

Thanks again for your input. I look forward to reviewing the report and considering the public comments when the development comes to City Council on December 12th. That meeting will start at 6pm at the FirstOntario Performing Arts Centre. All members of the public are welcome to attend and speak to the application. If you would like to register as a speaker, please contact Deanna Haine at dhaine@stcatharines.ca

Sincerely,
 Walter

Walter Sendzik
 Mayor
Email: wsendzik@stcatharines.ca
Tel: 905.688.5601 x1540

From: Stephen Good [REDACTED]
Sent: Thursday, November 09, 2017 5:03 PM
To: Mayor's Administration; Mayor & Council
Subject: We Don't Want Our Park Privatized

Sent from my iPad

Begin forwarded message:

From: Stephen Goode [REDACTED]
Date: November 9, 2017 at 10:45:06 AM EST
To: Michael Grant [REDACTED]
Cc: [REDACTED]
Subject: Fwd: Port Dal

Hello Everyone, It has recently come to my attention that the new development in Port Dalhousie is going to infringe on our community to the point of impacting the entire city. Port Towers area is zoned for 1.75 parking places per unit with the current plan they have 1.17 parking places. We need to actively write council and the mayor so they do not allow this to happen . . . Why? When I asked the city planning department if the Port Dalhousie Lot would become paid parking the response, "There are no plans for the City to implement paid parking in Lakeside Park at this time. Changes to Lakeside Park parking operation, such as paid parking or time restrictions, may however be considered in the future as a means of better ensuring parking availability for visitors to Port Dalhousie." The best way to ensure parking availability for visitors to Port Dal and ensure our investment of 3 million in tax dollars spent on Lakeside Park and money meant for the Piers remains a community focus is to stop the size of this development. It is too large for this space. WE DO NOT WANT OUR PARK PRIVITIZED Please using this email: mayorcouncil@stcatharines.ca

Click [here](#) to report this email as spam.

Ritchie, Scott

From: Suzie [REDACTED]
Sent: Thursday, November 16, 2017 10:49 AM
To: Hughes, Julie
Cc: Ritchie, Scott
Subject: Re: Condo parking requirements

Follow Up Flag: Follow up
Flag Status: Flagged

Hi Julie, thank you for your note. I am talking in general about Condo Parking requirements. When a Developer submits a proposal to the City, the existing requirements for parking per unit is 1.25 even though I understand the parking requirements for the proposal at Port Dalhousie is 1.75 parking spaces per unit and I hope the City does not vary from that requirement for the new build in Port Dalhousie. it would be a disaster if the city allowed less parking per unit. I want to know how we can get the parking requirements for all new Condos to be upgraded to 1.75 per unit. The existing parking requirements for Condos is 1.25 and that is inadequate. I would like Condos buildings and Apartment buildings to be considered totally separate. Condos can not be and should not be compared to Apartment buildings. Thank you,
 Suzie

Sent from my iPad

On Nov 16, 2017, at 9:44 AM, Hughes, Julie <jhughes@stcatharines.ca> wrote:

Good Morning Suzie,

A public meeting has been set for Tuesday, Dec. 12 at 6 pm to consider a completed application by Fortress to build a new 14-storey waterfront project in St. Catharines. This public meeting is the first step in the process to consider the new development proposal for Port Dalhousie.

The meeting will be held at the FirstOntario Performing Arts Centre in Partridge Hall instead of the usual location in Council Chambers at City Hall. The bigger venue is to accommodate the members of the public as we know there is a lot of public interest in this proposal. All are welcome to attend.

We appreciate that many of you have already reached out with your own thoughts and comments about the proposal and want to make sure you are aware that this meeting is taking place.

Council will not be making decisions at this meeting. It is an opportunity for members of the public to comment on the proposal. Staff will take comments, hear feedback and prepare a report for Council to consider early in the new year.

Thank you for sharing your comments on the proposal.

Sincerely,
Julie

Julie Hughes

Administrative Assistant - Mayor's Office

Email: jhughes@stcatharines.ca **Visit:** www.stcatharines.ca

Phone: 905.688.5601 x1542 **TTY:** 905.688.4TTY (4889)

Mail: PO Box 3012, 50 Church Street, St. Catharines, ON L2R 7C2

[<image549f31.PNG>](#) [<imagecfb4cc.PNG>](#) [<image8c9ad0.PNG>](#) [<image912d0d.PNG>](#)

From: Suzie [REDACTED]
Sent: Wednesday, November 15, 2017 6:57 PM
To: Riddell, Jim; Mayor's Administration
Subject: Condo parking requirements

I would like to lobby for the City Council to change the existing parking requirements from 1.25 per unit to be 1.75 per Unit in a Condo going forward.

I would also request to have the City Council look at separating Condo's from apartments. Condos can not be compared to Apartment Residents and I understand that at sometime in the past the City Council lumped the two together and I think these should be totally separate.

Suzie Neff

[REDACTED] St. Catharines
Sent from my iPad

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Ritchie, Scott

From: Louise Hastings [REDACTED]
Sent: Wednesday, November 29, 2017 3:56 PM
To: Ritchie, Scott
Subject: Fwd: Union Development in Port Dalhousie

Follow Up Flag: Follow up
Flag Status: Flagged

Scott - I had an incorrect email for you when I first sent this,

Louise Hastings.

Begin forwarded message:

From: Louise Hastings <[REDACTED]>
Subject: Union Development in Port Dalhousie
Date: November 29, 2017 at 3:52:28 PM EST
To: Carlos Garcia <cgarcia@stcatharines.ca>, Bruce Williamson
 <bwilliamson@stcatharines.ca>, Scott Ritchie <sritchie@stcatharines.com>
Cc: Jeff Mackie [REDACTED] Marion Markarian
 <[REDACTED]>

My husband and I came to Port Dalhousie in 1999 from Toronto - and have asked ourselves many times why we hadn't come sooner. It is wonderful and we have been fortunate to have been part of the community, particularly the St. Catharines Rowing Club.

We have seen many changes over that almost 20 years - downtown is now, quite simply, very sad and the current proposal will breathe much needed new life into the community. Marion Marakian has kindly shared her recent correspondence with me - and I absolutely agree with everything she has said.

We can not go back to what was and change is always challenging and difficult but we are being given an opportunity to move forward in a very exciting way and to watch Port Dalhousie come alive again. I agree that retaining Port's heritage components is vitally important but what we have now is not attractive or welcoming. What is being offered can be.

Respectfully Submitted,

Louise Hastings
 [REDACTED]
 St. Catharines.

Ritchie, Scott

From: Haine, Deanna
Sent: Thursday, November 23, 2017 12:32 PM
To: [REDACTED]
Cc: Nistico-Dunk, Bonnie; Alfieri, Rebecca; Sullivan, Kristen; Riddell, Jim; Ritchie, Scott
Subject: RE: Public Council Meeting on Fortress Condo Development December 12, 2017

Follow Up Flag: Follow up
Flag Status: Completed

Hello Colin.

By copy of this email, I am forwarding your comments to the Director of Planning, Jim Riddell, and the Planner, Scott Ritchie, for inclusion in their report.

Below, you state, "In order for us to be able to provide proper input...", are you requesting to be added to the speakers list as a representative for the Conservancy? If so, I do not want to assume so I ask, will you speak in opposition to or in support of the proposal?

Thank you

Deanna Haine
 Council and Committee Coordinator
Email: dhaine@stcatharines.ca
Tel: 905.688.5601 x1524

From: Colin Johnston [REDACTED]
Sent: Thursday, November 23, 2017 11:50 AM
To: Nistico-Dunk, Bonnie <bdunk@stcatharines.ca>
Cc: Mayor & Council <mayorcouncil@stcatharines.ca>
Subject: Public Council Meeting on Fortress Condo Development December 12, 2017

Bonnie Nistico-Dunk
 City Clerk
 St. Catharines

Dear Ms. Nistico-Dunk, Mayor and Council,

Re: Public Council Meeting on Fortress Development Union Waterfront Condo Project in Port Dalhousie on December 12, 2017

Along with many interested residents, we have attended previous 'Open Houses' on this proposed development, and while we have asked many questions, we find it very

difficult to visualize how this very large building complex will look, in our small 19th century heritage village.

Now, the City is holding an official public Council meeting regarding the Fortress Union Waterfront Condo Project and are seeking our input.

In order for us to be able to provide proper input regarding the proposed development, we need to be able to visualize the proposed project; and so, at the meeting, we are requesting that the City provide accurate scale renderings of street level elevations on all four sides: Lock St., Lakeport Rd., Main St. and Lakeside Park, that accurately show the proposed building relative to other buildings in the Commercial Core and the adjacent residences.

Please make sure that this letter is available for City Councillors at the November 27th, 2017 Council Meeting.

Thank you,

Colin Johnston

President

Architectural Conservancy of Ontario - St. Catharines Branch



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Hi Mayor and Councilors, I would like to express my opinion regarding the Port Dalhousie development. There are 3 major things that concern me.

1. I believe the size of this building and the number of units is way too much for our little village. It doesn't fit in with the aesthetics of our community. We are one of the last little waterfront communities in Niagara with that small town charm and that will be taken away with this massive building. We can use NOTL for an example as all buildings follow a strict guideline.
2. Traffic will be excessive. The study that was done by the developer will not reflect the real world. This was a bad year for Port's visitors/tourists as we had no beach, the piers were closed and the weather was unfavorable. Even when there are no rowing activities and it is a beautiful Saturday or Sunday, traffic lines up through Port. It would be unthinkable to me, to have 400 plus vehicles plus condo visitors using the roadway as well. There's also the fact that all condo users from both developments, Beaches of Port and Union Waterfront will be sharing a small roadway (north end of Main) with people leaving the beach parking area. Then add delivery and garbage vehicles backing in from Main street.
3. Parking will be a huge issue regardless of what I've heard from the developer. This development was designed similar to projects in Toronto where there are shops, grocery stores, gas stations and convenient stores close by or within walking and biking distance. Although, we are told that people moving there will have one parking space per unit unless they combine units. I am of, what I believe to be rational thinking, that a lot of these people will have more than one vehicle. I'm sure there are ways to limit parking in Lakeside Park and surrounding areas but I'm also sure people will park their second vehicle and their visitor's vehicles in the residential streets nearby. This will likely cause a great deal of frustration for residents resulting in complaints to the city and ultimately lead to more signage in their front yards saying either 'no parking' or a 'time limit'. That's not fair to the residents living in close proximity of the downtown core.

In closing, I agree that we need development of some kind but I would like to see it kept to the height laws currently in place. We need to use our secondary plan to make these decisions. These are plans the residents decided upon and provided to the city in meetings at the Lion's Club. It doesn't make sense to me and I feel like we were misled by doing those workshops for a secondary plan and apparently are not using. I'm certain the city knew the developer would have a plan by July so why was the secondary plan delayed until after July? Now there's a final secondary plan meeting in October! I don't like the smell of that one.

Anyway, I hope this has some merit with the city and please let's develop responsibly.

Thank you for reading,

Ed Buchan

[REDACTED]

St. Catharines

[REDACTED]

Ritchie, Scott

From: Sendzik, Walter
Sent: Thursday, November 23, 2017 10:38 AM
To: [REDACTED]
Cc: [REDACTED] Ritchie, Scott
Subject: FW: Union Waterfront

Follow Up Flag: Follow up
Flag Status: Flagged

Hi Marion,

Thank you for taking the time to connect with me and council regarding the Fortress development proposal. Your comments are appreciated.

A public meeting has been set for Tuesday, Dec. 12 at 6 pm to consider the completed application by Fortress to build a new 14-storey waterfront project in St. Catharines. This public meeting is the first step in the process to consider the new development proposal for Port Dalhousie.

The meeting will be held at the FirstOntario Performing Arts Centre in Partridge Hall instead of the usual location in Council Chambers at City Hall. The bigger venue is to accommodate the members of the public as we know there is a lot of public interest in this proposal.

We appreciate that many of you have already reached out with your own thoughts and comments about the proposal and want to make sure you are aware that this meeting is taking place.

Sincerely,
Walter

Walter Sendzik
Mayor
Email: wsendzik@stcatharines.ca
Tel: 905.688.5601 x1540

From: Jeff Mackie [REDACTED]
Sent: Wednesday, November 22, 2017 8:57 PM
To: Sendzik, Walter
Cc: Mayor & Council
Subject: Fwd: Union Waterfront

Dear Mr Mayor,

In advance of Dec 12th's public meeting I was asked by my mother-in-law to forward this letter to yourself and council. If you have already received it from Scott Ritchie, my apologies for doubling up. She felt it was important to ensure that it was circulated well in advance.

Please see below.

Cheers,
Jeff Mackie

From: Marion Markaria [REDACTED]
Sent: Wednesday, October 04, 2017 10:45 AM
To: Ritchie, Scott <sritchie@stcatharines.ca>
Subject: Union Waterfront

This is regard to the Union Waterfront proposal in Port. Given the information that this where to send comments so hopefully you are the right one.

My husband and I have been residents of Port for 40 years now and are active in our community (St. Catharines rowing club, Port Beautification) Our daughters and their families both live in Port so I feel we have a vested interest in what happens in our community.

I will start out by saying that I would love for Port to maintain its small beach community feel that it currently has. I love living in this community. But having said that we cannot live in the past forever. The commercial area is not viable as stands. Small family owned businesses cannot survive with only summer patronage, which was sorely hurt by this year's high water in addition to the closure of the piers. If we do not do something to breathe life into the area it will surely continue to die it's slow death and Port will go to sleep again. Those buildings that some want to maintain for their heritage value will continue to decline.

We have a chance here to make new beginnings and integrate the developments into our community. Far better that we work in concert with those who wish to develop the area to try to mitigate issues that could arise such as the dilemma about traffic flow and parking. The city and region should be working with the developer to come up with a plan mutually beneficial to all. Indeed we should be working with all the potential new developers together in creation of this plan. I believe this can happen.

There are some who continue to challenge the need to go forward. They are right that this development will change the face of Port forever. I respect their opinions in terms of heritage buildings and ambiance of the town. I too agree that this should be forefront in discussions with the developers. But this does not mean that we do not develop. It means that we work together to

develop. Another lengthy and costly OMB hearing is counterproductive to that end.

Let's be a community that understands and appreciates it's past but embraces the future. Let's show that we are open for business..... and a side note... that we are worthy of the federal government restoring our piers because we have many people here who will appreciate and utilize them.

My two cents worth... but I would not want only the voice of the naysayers to be heard. I would hope that you share this with Bruce and Carlos as I cannot seem to find their emails at this moment.

Respectfully,
Marion Markarian



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Ritchie, Scott

From: Sendzik, Walter
Sent: Thursday, November 23, 2017 9:40 AM
To: 'Mary'
Cc: Ritchie, Scott
Subject: RE: Port Towers

Follow Up Flag: Follow up
Flag Status: Flagged

Good Morning Mary and Rob,

Thanks for connecting. Your comments on the Fortress development proposal are appreciated.

A public meeting has been set for Tuesday, Dec. 12 at 6 pm to consider the completed application by Fortress to build a new 14-storey waterfront project in St. Catharines. This public meeting is the first step in the process to consider the new development proposal for Port Dalhousie.

The meeting will be held at the FirstOntario Performing Arts Centre in Partridge Hall instead of the usual location in Council Chambers at City Hall. The bigger venue is to accommodate the members of the public as we know there is a lot of public interest in this proposal.

We appreciate that many of you have already reached out with your own thoughts and comments about the proposal and want to make sure you are aware that this meeting is taking place.

Have a good day,
 Walter

Walter Sendzik

Mayor

Email: wsendzik@stcatharines.ca

Tel: 905.688.5601 x1540

From: Mary [REDACTED]
Sent: Wednesday, November 22, 2017 11:21 AM
To: Mayor & Council
Subject: Port Towers

Good morning,

I am writing to voice my concerns re the newest development plan for Port Dalhousie.

As longtime residents of St. Catharines, we have watched a vibrant city attraction deteriorate to an embarrassing eyesore.

This small community cannot support a building of this size, or the traffic and parking nightmare it will create in the area. The tax revenue that will be created by the additional residential units does not justify the damage that will be done to this community.

The attractive features left in port are the beach, the carousel, and the few quaint and easily accessible shops and restaurants that remain.

The current proposal will ensure the end of all of these. Access to enough close and free parking is a necessity to ensure people will still bring their families to the area, and to the beach and carousel, and this will in turn keep the stores and shops open.

The building size and parking numbers that that are proposed set a precedent for future development in Port Dalhousie, which could also be allowed to proceed without ensuring the necessary parking is available.

My family has enjoyed growing up in the area, and the memories we have of time spent in Port. Please consider the Port Dalhousie that is being created for future generations.

Sincerely,

Mary and Rob D'Ilario
Lakeshore Rd.