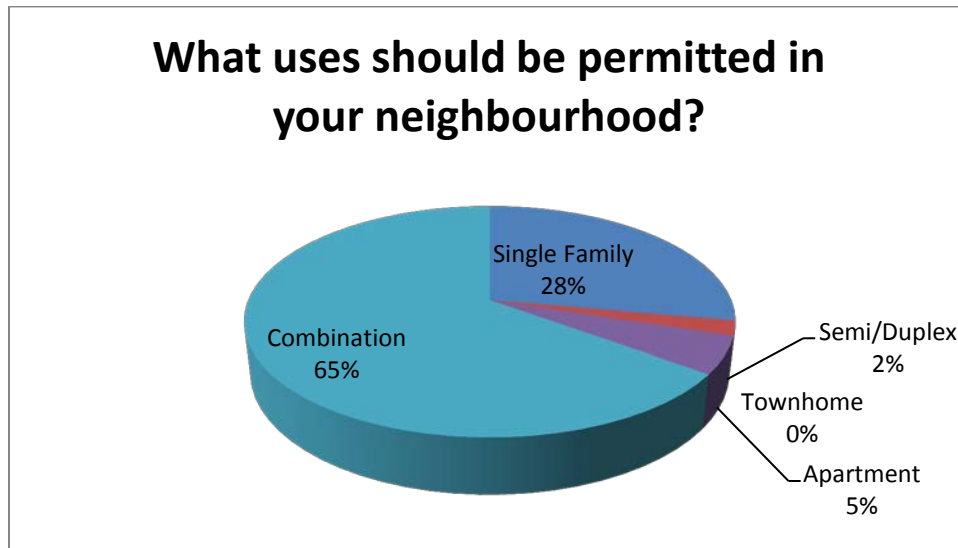
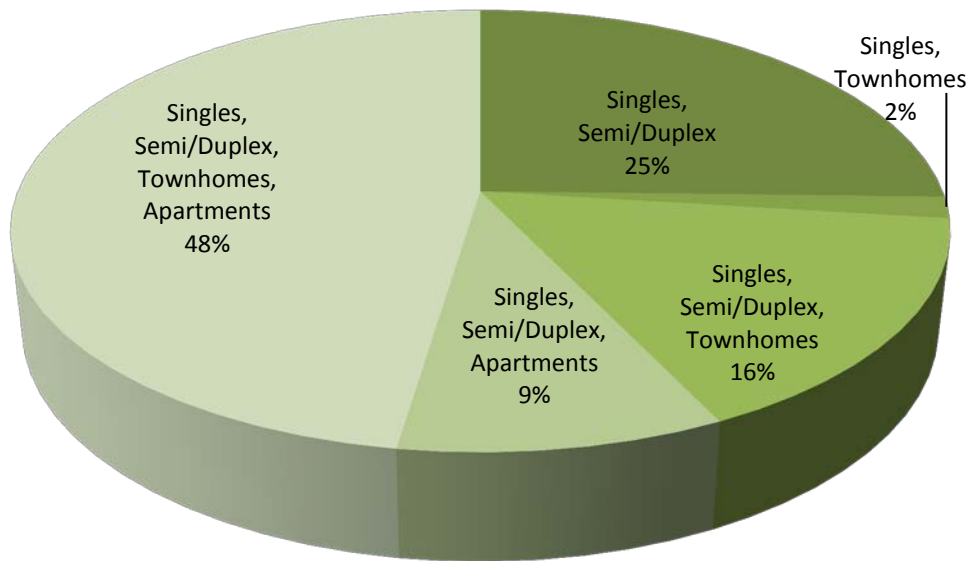


## Residential Survey Results

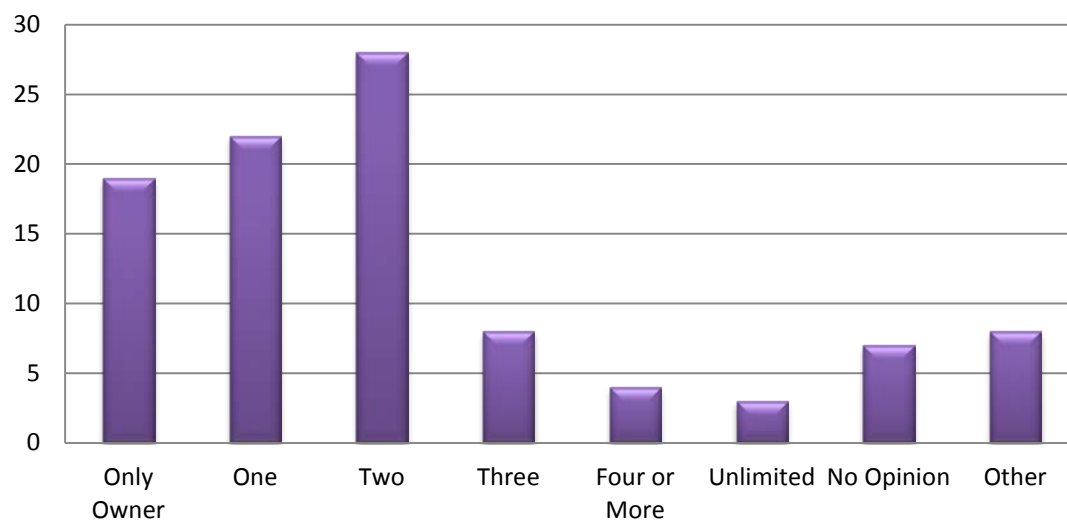
99 Surveys completed



## What Combination of Uses Should Be Permitted in Your Neighbourhood?

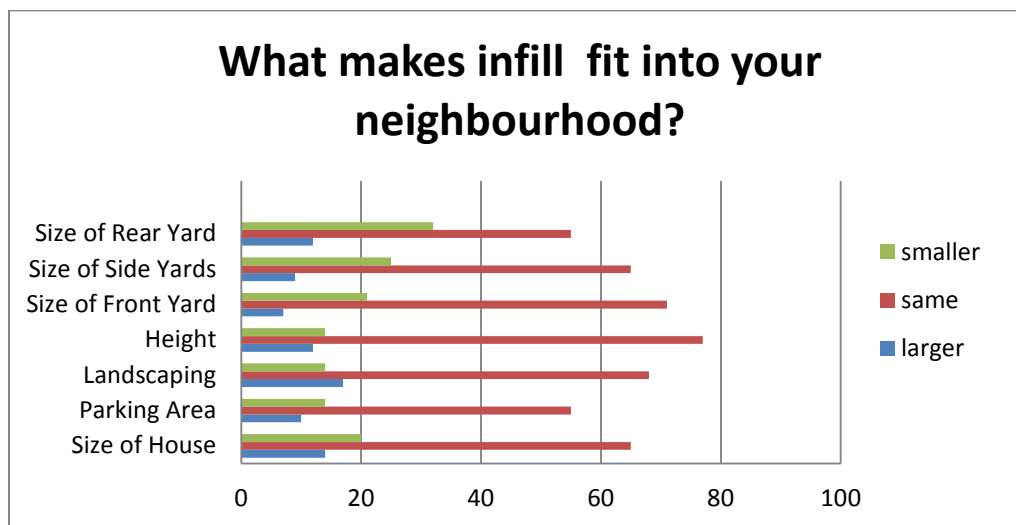


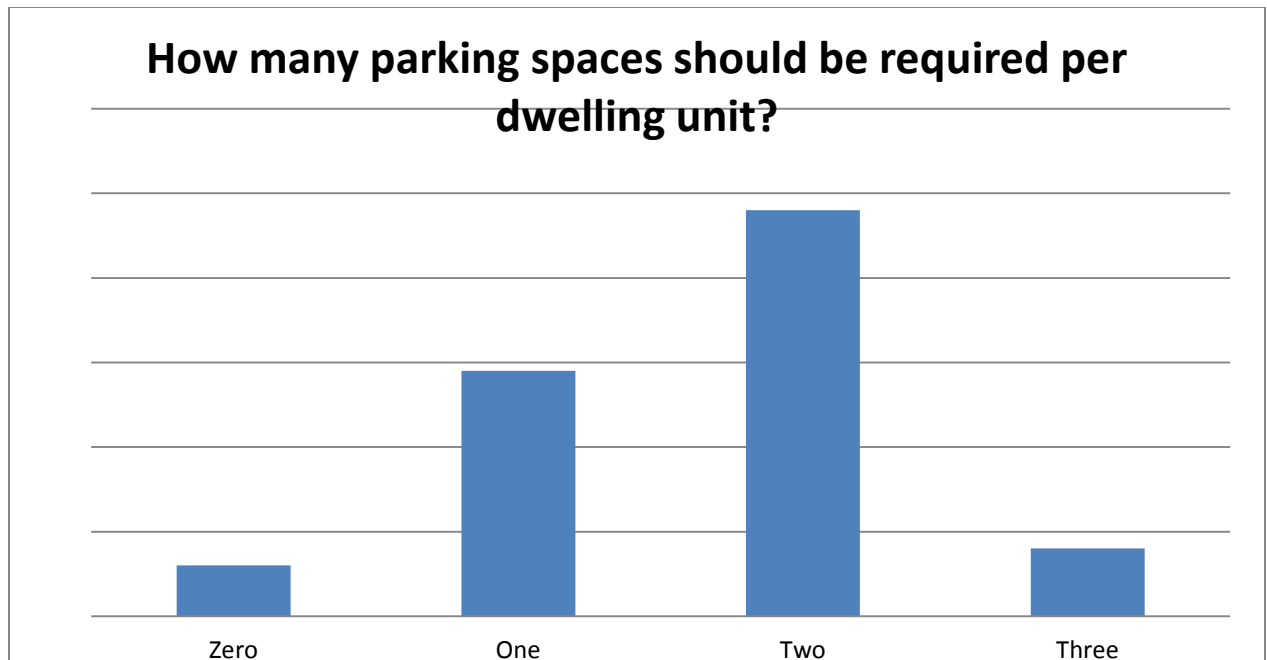
## If you or your neighbour run a business out of your home, how many employees should be allowed?



### What you said about the number of employees that should be allowed with a home based business:

- As few as possible to reduce paving of yards
- depends on the area disturbance the business creates
- Depends, if there is a lot of business traffic, they should be in a commercial area, not a residential area.
- If the Business does not have customers coming to the house, then one per employee
- Not sure, they might want to consider car sharing or having a dedicated business vehicle or use transit to get there.
- One per employee plus one for clients.
- This depends on the type of business. I would not want employee parking to inconvenience neighbours.. for example, parking a large truck and trailer on the street taking up 2-3 actual spots is no way to impress your neighbours who then need to find elsewhere to park their vehicles.
- Whatever the driveway can accommodate. If they don't have enough parking, they'll have to park on a side street, or use public transportation

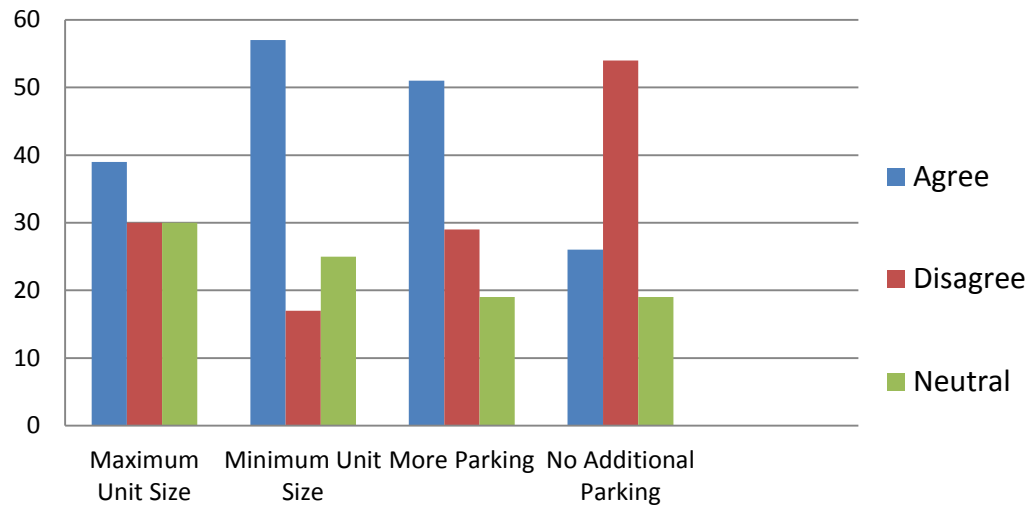




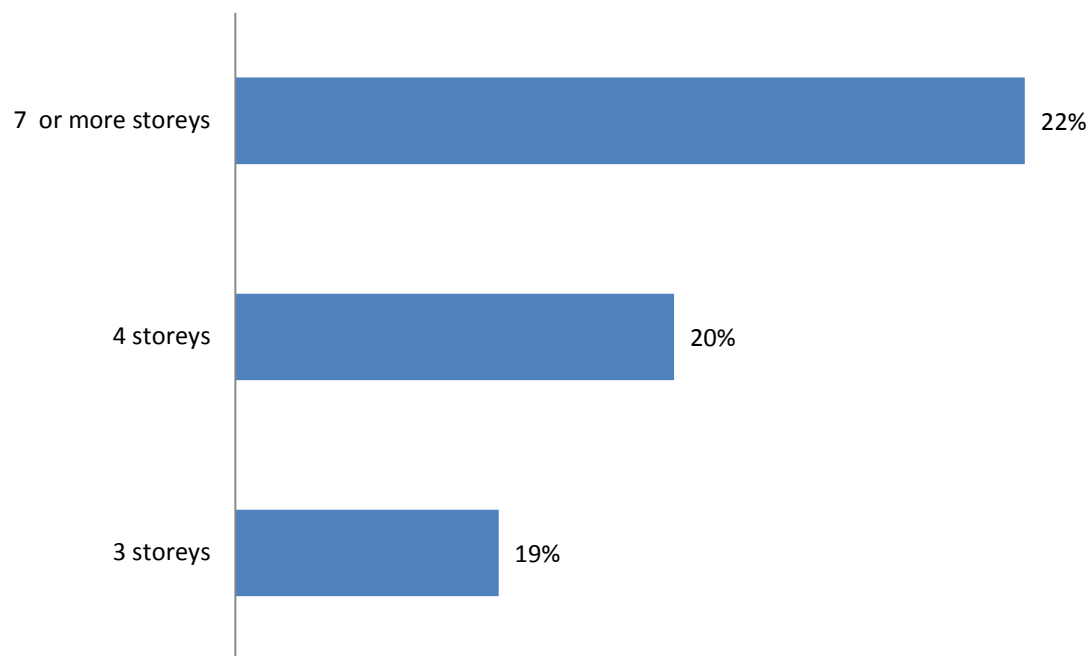
**Comments on the number of parking spaces for dwelling units:**

- I don't think there should be a minimum, if a house doesn't have enough parking then a tenant can choose to live elsewhere. It may be possible they use public transportation anyway, so it wouldn't matter.
- In a university/college town, restricting the amount of parking seems really stupid.
- It depends on the area of the city and how it is served by public transit and bike lanes
- This depends on the use: i.e. a student house with 4 rental rooms should have room for upwards of 4 cars.

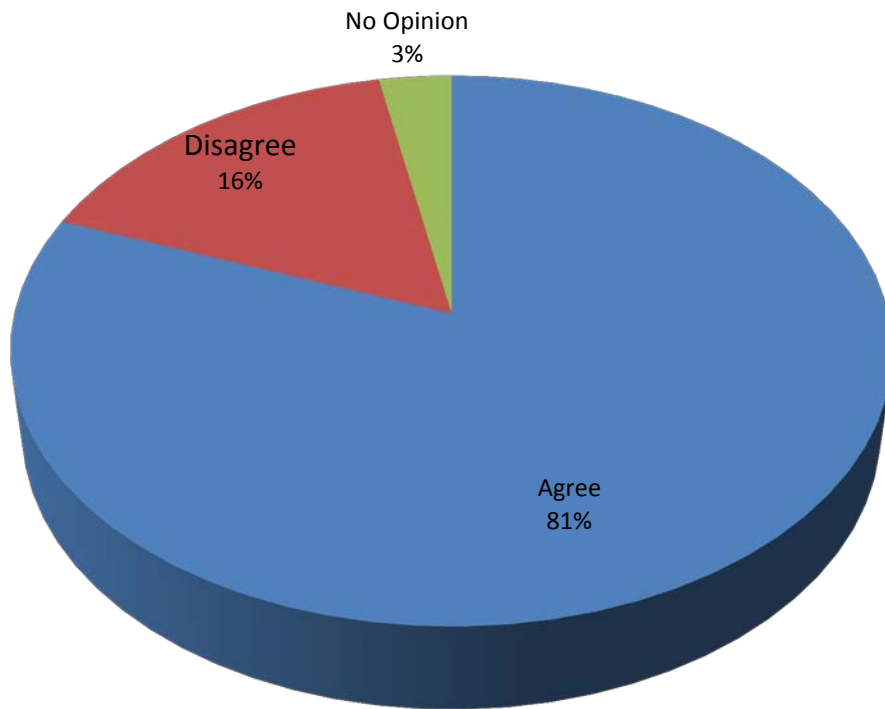
## What should the requirements be to add a secondary unit?



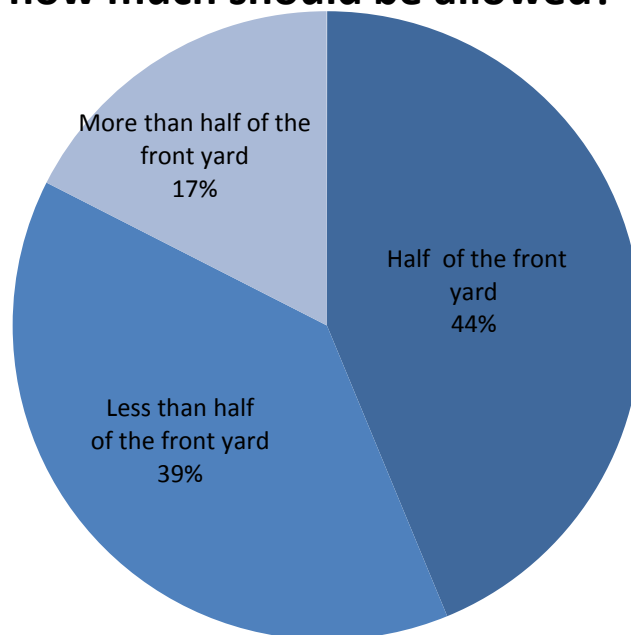
## How tall should apartment buildings be in residential neighbourhoods?



### Should the amount of parking be restricted in the front yard?



### If the amount of parking is restricted in the front yard, how much should be allowed?



## More Comments and Suggestions:

- Hire more people, takes forever to deal with zoning & planning staff. Be progressive - city needs all kinds of housing.
- In a university/college town, restricting the amount of parking seems really stupid. Everyone should be able to fit their car. It is also understandable that the whole lawn should not be littered with cars.
- Interesting question regarding height - I checked 4 storeys as my daughter owns a town home in Toronto that is very narrow yet 4 storeys in height. The lowest level is mostly a tandem garage for two vehicles as there is no parking lot at all.
- Don't allow developers to cut down mature trees on or near property lines, thus changing/damaging the environment of neighbouring homes/yards.
- Certain areas of the city, notably along the 406 corridor, are subject to MTO approval on any changes to property. Will the MTO continue to be involved in city zoning issues?
- Requiring parking for secondary units (basement apartments, etc) is important as it is not desirable to other residents of the area for a resident of a secondary dwelling to always park on the street.
- parking of large RVs & large boats should be restricted to behind the lot line. Should not be allowed on city section of front lawn or driveways. Units parked near the curb compromise sight lines for drivers. As well the situation detracts from the aesthetics of the area
- If you limit the amount of parking on a property, then you increase the amount of parking on the streets. This can create an unsafe environment for motorists, cyclists and pedestrians. Secondly, depending on your neighbourhood, vandalism can be an issue if the parking areas are landscaped to block security lights, clear viewing from the street, etc. I believe that the city has done an excellent job at keeping up with the needs of residents. As more people have home based businesses, I think there may be issues with the confusion between residential/commercial zoning. That should be watched carefully. Thanks, Darryl
- Increased height in buildings is an excellent way to use more space without removal of green space
- Parking should be a maximum allowed, not minimum required. If they want additional parking, a variance should be required proving WHY they need over the allowed maximum. Anything over should have to be permeable or LID.
- More trees need to be planted to replace the recent decline due to wind storms and aging tree populations, and in most cases they should be Carolinian trees, given the fact that we are in a unique (for Canada) Carolinian biosphere (e.g., tulip trees, cucumber trees).
- RV parking needs to be figured out. Need restrictions on parking boats, skidoos, jetskis, and trailers in residential areas. Perhaps one per dwelling.
- Some of my answers were selected half-heartedly, since I don't think that the same zoning rules should apply in all areas of the city. There needs to be sensitivity to a variety of factors (heritage, neighbourhood character, demographics, proximity to amenities, etc) in determining how to proceed with infill.
- Basement suites and infilling is a great way for St. Catharines homeowners to improve their homes and at the same time allow for family to move in, or create affordable housing for people while at the same time providing some income for the home owner or investor.
- Landscaping should be paid for by any developer as new developments arise, rather than the taxpayer.
- Why does the city care about this when they have just broadened the ability to store RV's in driveways? Why is the city going after people who work from their homes when all they are trying to do is earn a living? The city gives the Niagara Airport, a private company our tax dollars, but they attempt to thwart the poor taxpayer who is trying to make ends meet. We have no jobs in the region, just look at the statistics, and nothing that city hall has ever done has changed this. If someone can provide employment city hall should welcome this, not try to thwart it.
- Minimum lot frontage variance was obtained for my lot. Only 25 feet wide, variance also was obtained for side yard setbacks. This was done for developer to squeeze more houses into corner of subdivision. Both of these variances make snow removal in winter next to impossible

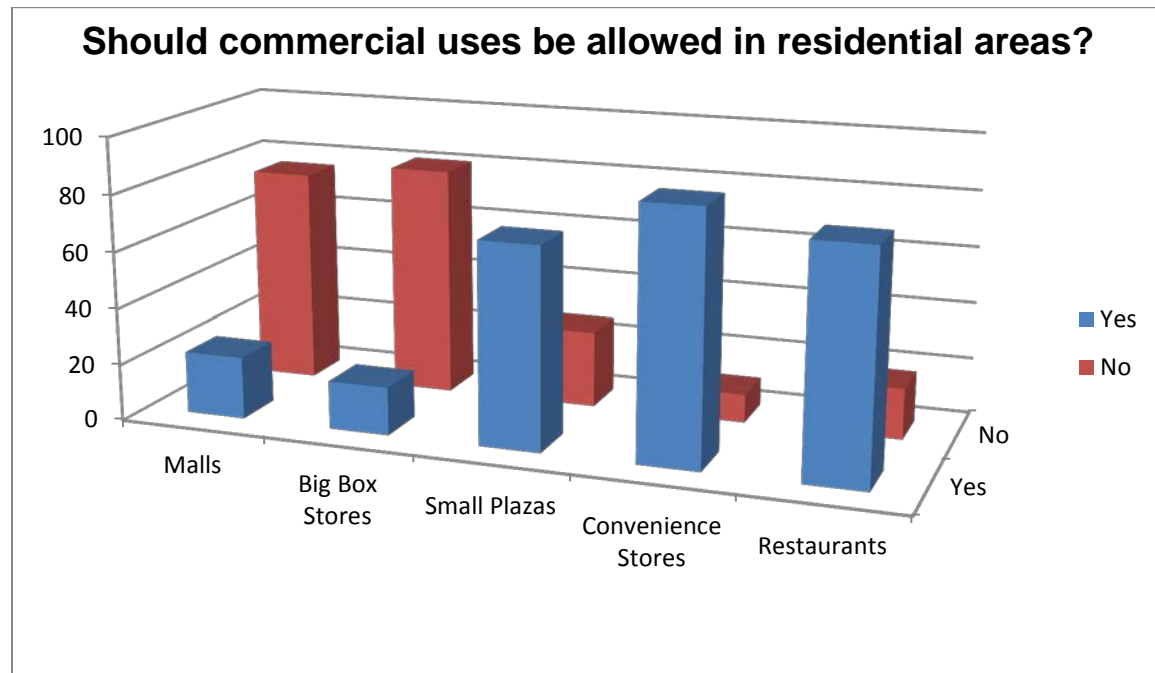
and also causes concern with children playing on sidewalks with so many driveways within a small distance. Plus lack of street parking. Around my house there are four lot frontages within 100 feet. That is ridiculous. I can be reached at 905-687-9961 if you would like to discuss further. Carl Strohak

- Property owners who do not provide enough parking spaces for the number of tenants per unit should be subject to a daily fine until they comply with the by-law. On street parking allowed only if the property owner obtains a monthly parking permit for each extra vehicle with restrictions on number of hours vehicle may be parked on the street.
- Community gardening use and explicit permission for food gardening in front yards should be included.
- Please provide incentives for green building. It doesn't have to be monetary but could be a reduction in fees or possibly even taxes. We need to consider smaller building lots for single family dwellings. New Urbanist type building with large front porches close to the street and parking in the back if possible. Parking requirements should always be a maximum instead of a minimum especially in commercial areas. If they want more parking they have to make it permeable.. parking reductions should be encouraged, not restricted in transit oriented development.
- An infill should not be allowed to drain onto a neighbouring property, nor should an existing place be allowed to regrade so the water drains to the neighbour. Trees should fit the lot or close to it and not shade 5 other lots. Tree of heaven and weeping willows should be banned.
- I believe that any construction should match what is already existing in the area of build. there should be exception for lawn parking on front lawn for disabled persons with permits showing. especially in shared driveway situations.
- I feel that multi-generational or duplex house's are a good direction for St. Catharines. This will be a new trend in the future for people to afford housing due to the erosion of good paying middle class job's. Allowing duplex's will help young professional's afford home ownership while trying to enter the work force and start a family.
- No overnight parking on street Street Parking by permit only We do not want cars to park and block street sidewalk or no parking on city sidewalk
- Fencing behind front face of dwelling should be min 6.5'. I believe St. Catharines is the only remaining municipality, in Niagara, that has a max 6'.
- The city cannot follow the laws they have created now without abuse. Trying to enforce by laws that are in dispute is a costly affair to the city.
- I think parking restrictions for any type of vehicles should be relaxed, especially in areas where lot sizes are small to begin with.
- I think that as land gets scarcer how we utilize it is more important. We must also plant more trees so each home in St. Catharines should have at least 1 tree if there's room.
- Provide bylaws which make duplex/triplex legal or less restrictive. I have turned down purchasing many multi-family dwellings because the units were not zoned properly. Affordable housing helps everyone.
- Leave things well enough alone
- The less restrictions the better. Generally, people should be allowed to do what they want on their own property.
- Have more relaxed requirements for major arterial roads in the neighbourhood. Glendale Ave from the PenCentre to the 12 mile creek (and Glenridge from Brock hill, to railroad) should allow for higher intensification (higher building heights, less setbacks and restrictions) compared with a subdivision off Marsdale or Riverview.
- Mature trees along an existing property line should not be removed in order to stuff a new house in. Consideration for the impact to the neighbours (gardens, shade, sun) must be a priority.

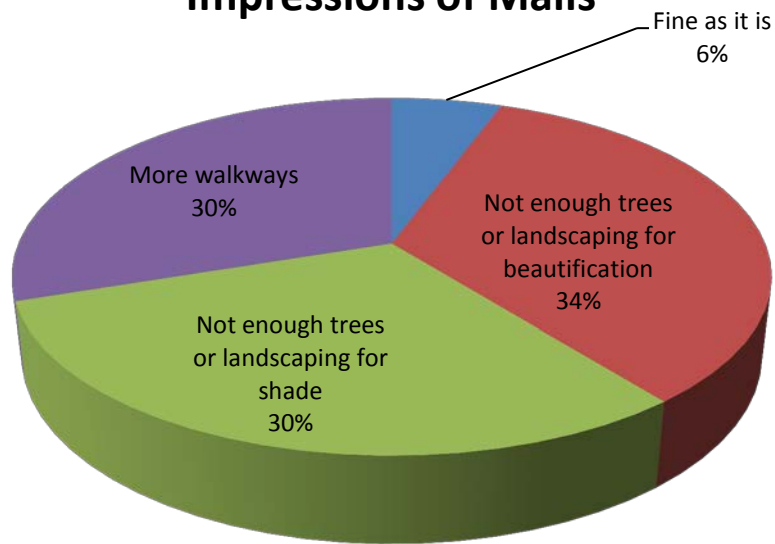


## Commercial Zoning Survey-

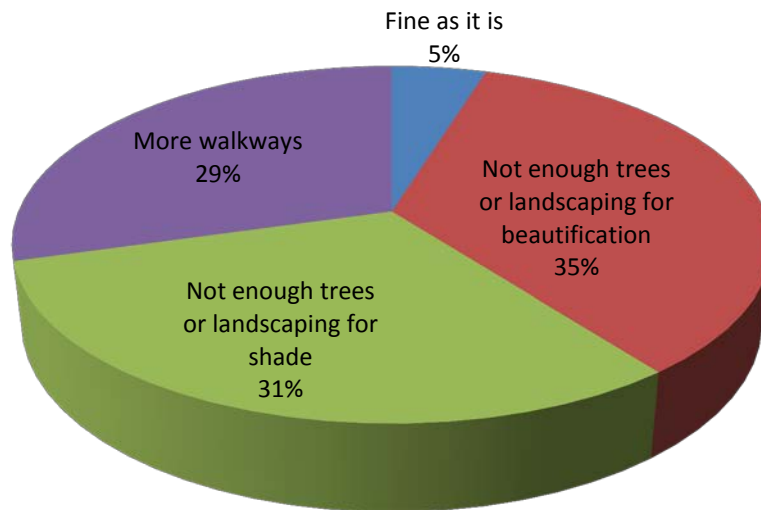
- 98 surveys complete



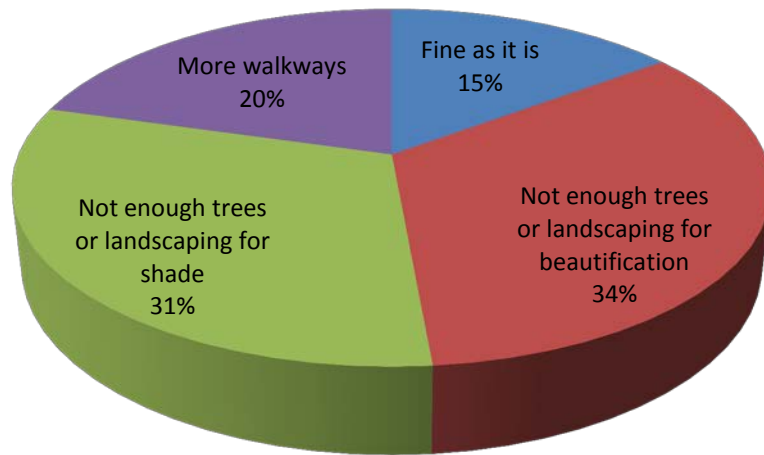
## Impressions of Malls



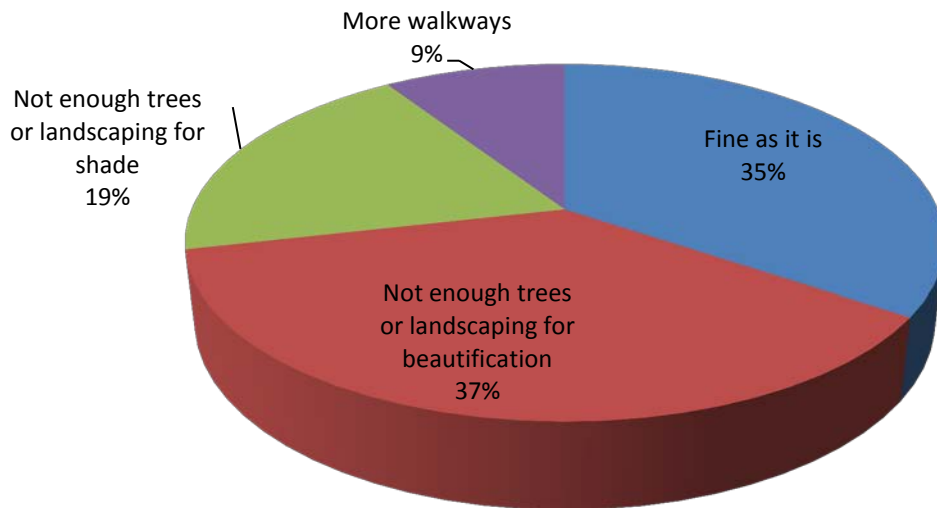
## Impressions of Big Box Stores



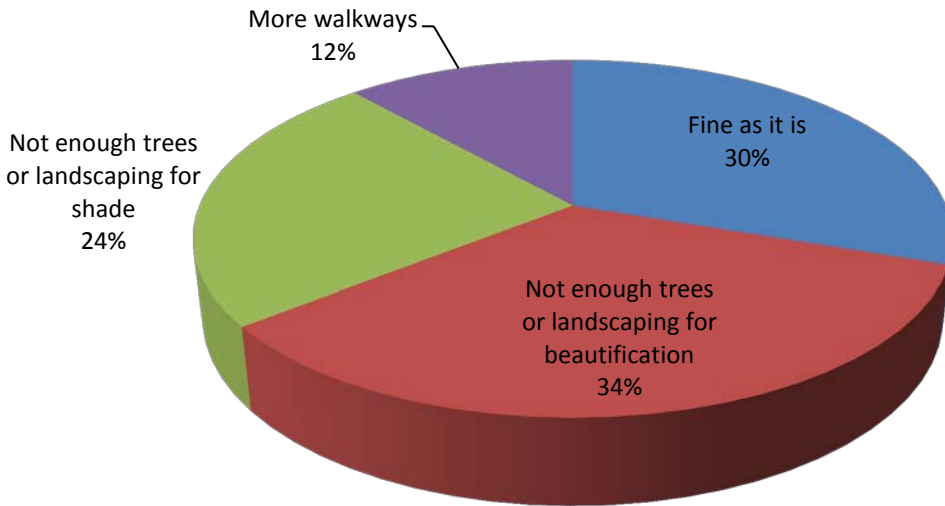
## Impressions of Small Plazas



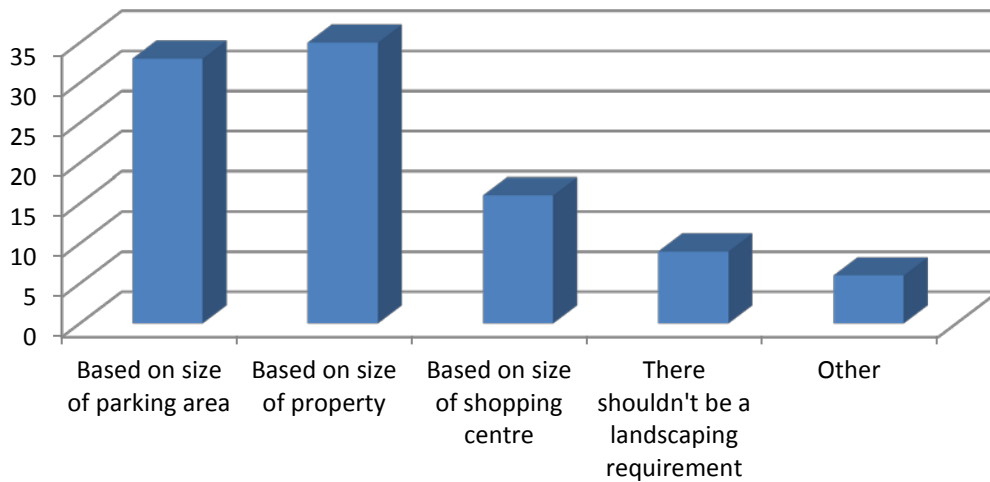
## Impressions of Convenience Stores



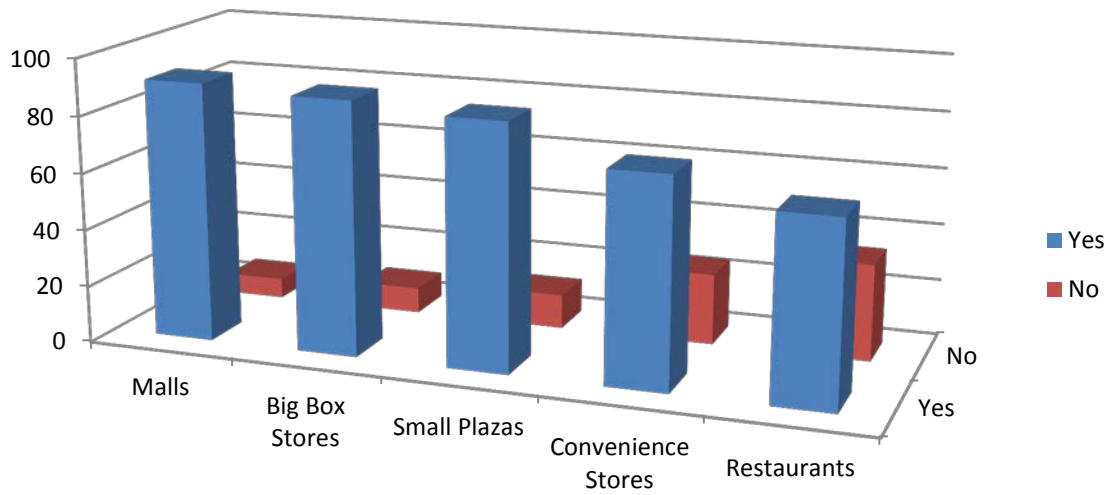
## Impressions of Restaurants



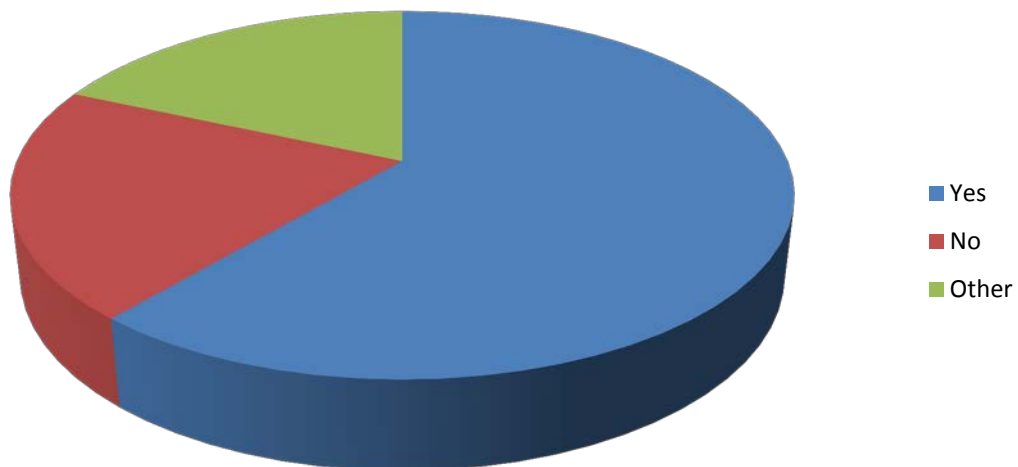
## How should landscaping requirements be determined for shopping centres?



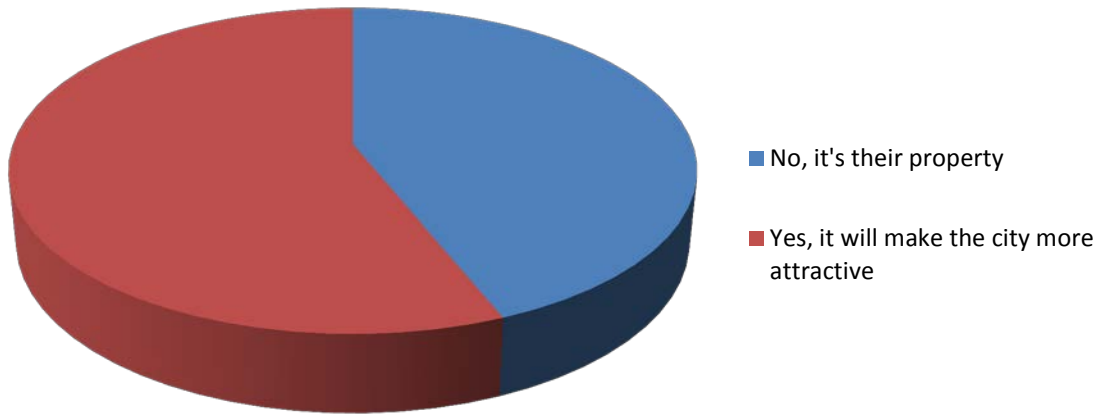
### Should there be a requirement to provide bike parking?



### Is there enough space in drive-thru lines for vehicles waiting?



## Should there be a landscaping requirement to hide drive-thru line-ups?

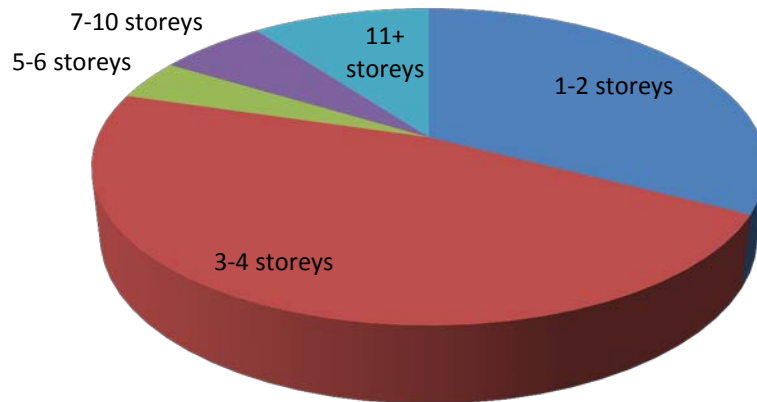


### What you said about drive thrus:

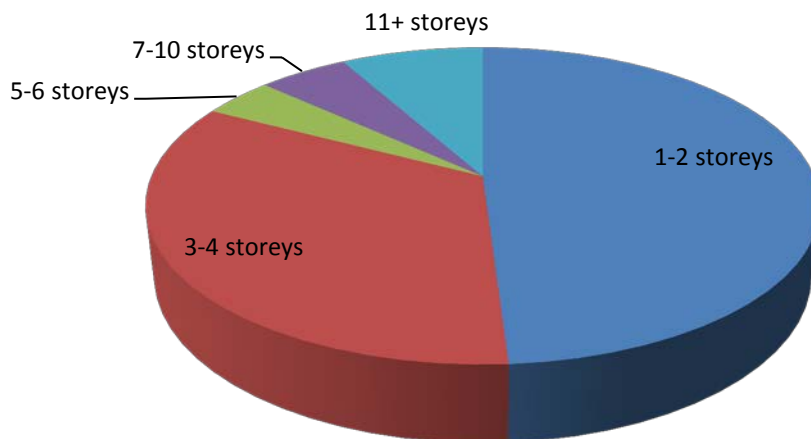
- Not as relevant as having a drive-thru in the first place and I'm not aware of this even being an 'on-going' issue
- Pick any tim hortons;No
- Remove drive-throughs
- sometimes, and sometimes not - it really depends on the location.
- THERE ARE TOO MANY DRIVE THRUS ALREADY, GET PEOPLE OUT OF THEIR CARS AND INTO THE FACILITY
- Width wise and sharp turns are difficult for a full size truck owner.
- Ban drive thrus entirely.
- Ban them. Too many fat people. Not good for the environment
- check out the mess at the grantham plaza Tim horoton's where traffic sometimes backs up all the way back to the street. Also, what I find most annoying is that in most drive thrus once you are in line, if you were in need to get out of line should you change your mind or in case of emergency - there is no way out past concrete curbs;No
- depends on the nature of the drive through. Service indistry such as banks require less space for lining up that say a restaurant.
- Drive thrus should be eliminated
- Drive-thrus are a source of pollution and should be eliminated where possible.
- For most places yes, but I've seen places the line-up for drive-thru was all the way into the mall parking lots.
- gdt out of the cars. tim hortons lines in the street are unacceptable.
- Get rid of drive-thrus
- I never go in drive thrus

- in some plazas they need more room
- Most Tim Hortons don't have enough space. Eliminate food service in drive thru. Line ups would go faster.

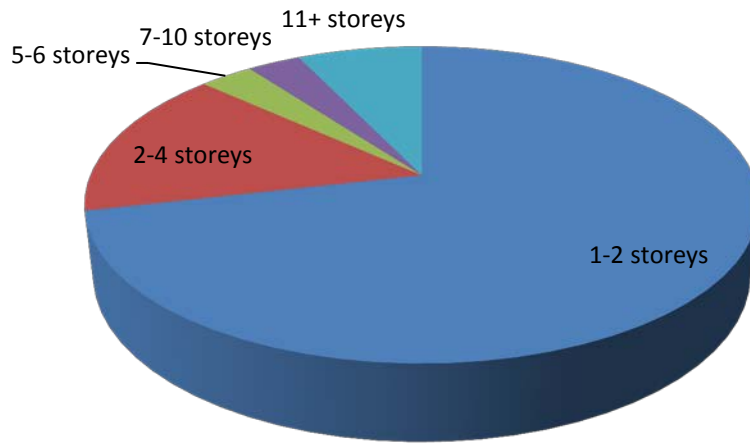
### **What building height (in storeys) should be permitted for malls?**



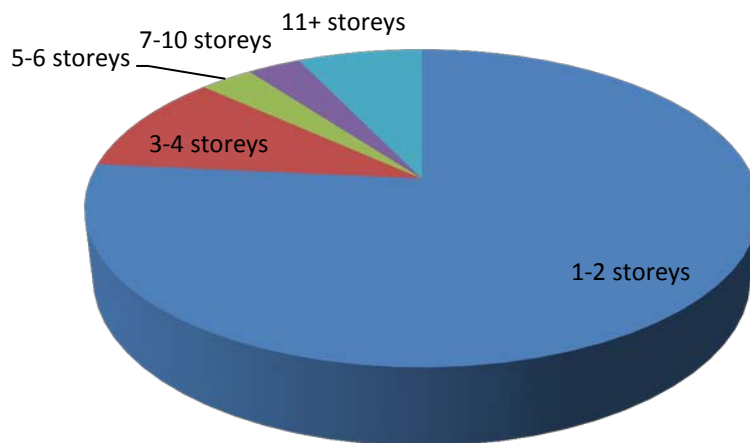
### **What building height (in storeys) should be permitted for big box stores?**



**What building height (in storeys) should be permitted for small plazas?**

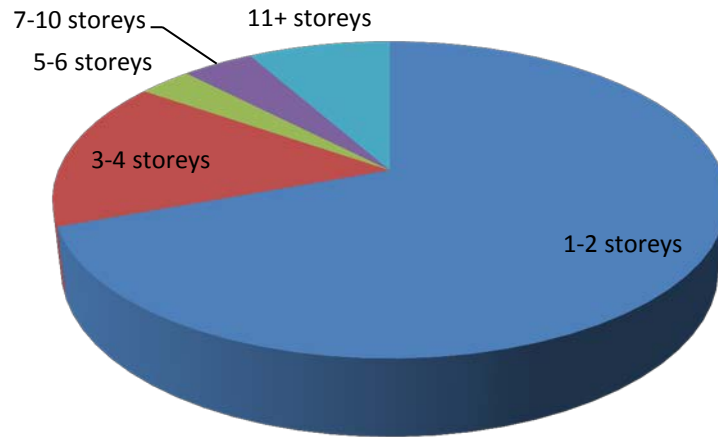


**What building height (in storeys) should be permitted for convenience stores?**

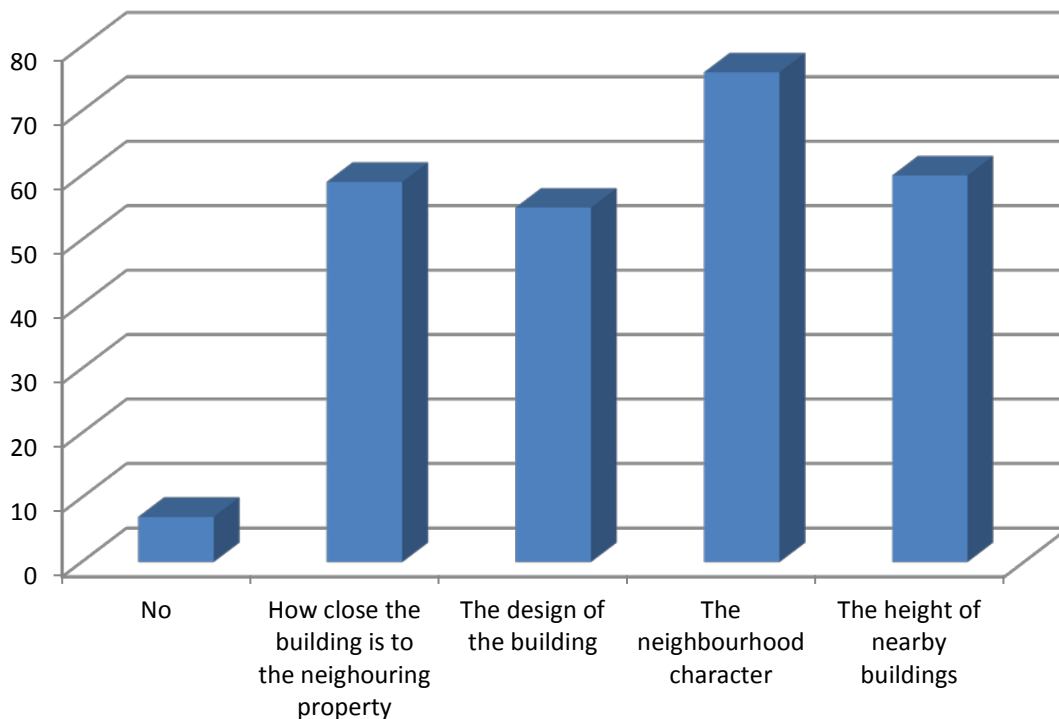




## What building height (in storeys) should be permitted for restaurants?



## Are there other factors that should be used to determine height of buildings?



### **More Comments and Suggestions on Commercial Zoning:**

- Malls and big box stores could provide underground parking. It would help the look of the city. In general I feel that St. Catharines has far too much retail / commercial space and not enough residential. The City has allowed there to be new development when there is unused / vacant commercial space throughout the city. The population isn't growing - how can it support more commercial development?
- It would be nice to see more mixed-use zoning. I would love to be able to shop, eat, and be entertained in my neighbourhood instead of having to drive into another part of town. The size of parking lots, particularly at the big box centres on Fourth Avenue, should be reduced. There is too much parking. I have never once seen them full to even a third capacity. They are disgusting eye soars.
- More bicycle lanes in down town core and throughout the city. Stop building small plazas and use the ones we have that are vacant.
- Commercial plazas, malls and big box stores should incorporate multi story residential uses
- Having been brought up in Germany, I find it important to have bicycle parking in all the above listed locations. In the Spring, Summer and Fall many people tend to use their bicycles more often than their cars, with the exception of grocery shopping. Keep up the good work City of St. Catharines!!!
- Landscaping should include deciduous trees to clean the air and beautify year round
- Any new small plazas should have mixed commercial and residential use e.g. the new plaza on Martindale should have been forced to have apartments above the stores. They could have been condo apartments with a beautiful view down 12 mile creek and would have brought in high taxes.
- Please, no more single storey buildings. Hiding drive through a can be done with greenery for shade, not just beautification.
- I think the over all look of the city is very in kept. We are supposed to be the garden city and the gardens are terrible. Port Dalhousie is supposed to be our tourist area and it is an embarrassment. The downtown core needs to be cleaned up, no body wants to be there, it's ugly and doesn't feel safe.
- Electric charging stations for electric vehicles.
- Make sure there is focus on keeping St Catharines sustainable by helping local business and being careful of corporate big box sprawl.
- Keep up the good work. The questions and surveys being generated suggests this is going to be a very interesting, unique document when it's complete.
- Drive thrus should be phased out. People need to get out of their car.
- Minimizing parking and targeting increased density would be a big step forward
- Bikes, walking, bus access should be a consideration and drive thrus should no longer be allowed and existing ones should be phased out over an appropriate and fair length of time. Recently saw a place of business that banned bikes on its property. Inexcusable.
- If you are thinking about drive-thrus, I would strongly recommend taking a serious stand against their proliferation. These are an ecological disaster, and contravene any anti-idling by-law the city will ever enact. From an

environmental perspective, I would suggest adding a \$0.25 tax to every drive-thru purchase and use that money to increase public transit (an LRT to Brock and to the train station from downtown) and bike lanes. It may sound like political suicide, but there is a strong case to be made for curtailing drive-thru culture and remaking St. Catharines into an environmental leader.

- Don't try to generalize the entire city into one law. Downtown, for example, shouldn't be limited in terms of height of buildings, whereas Port Dalhousie should never have permitted giant structures.
- 1) Why would you have a height restriction on a restaurant or convenience store? 2) Some of the malls already have bike parking any I rarely see any bikes parked there. I doubt it is much of an issue except for a tiny fraction of the population. Who would ride their bike to a big-box store anyway. Doesn't make any sense. 3) The parking lots at the Malls are never full except for a few weekends before X-Mas (and I am sure that even if the parking lots were doubled for X-Mas they would still be full).
- Preserve history, respect the current neighbourhood.
- Further to questions 9/10, given the format of 9, it's hard to just determine or decide appropriate storey limitations. Convenience stores, for example, in a normal residential neighbourhood, would not have to be any higher than the neighbourhood. But then again, if a convenience store or restaurant were to be built/open on the ground level of an apartment building or mixed-use development, it need not be restricted/forbidden. In the same vein, malls, big box stores and even small plazas should no longer be stand-alones, but rather part of mix-use intensification/densifications.
- Instead of large malls such as the Pen and Fairview Mall being forced to close off sections of their basements to reduce the amount of rentable space thus helping to reduce parking needs mall parking should be based on sales/revenue space. Customers can not shop in a storage area so why use this in the calculations. Larger malls such as the Pen and Fairview Mall should be encouraged to grow to help to reduce the need for additional malls to be built.
- Zoning by-laws need to account for future growth. I.e. existing roads in St. Catharines are for the most part only 1 lane each way, whereas cities like Burlington or Mississauga have 2-3 lanes going each way in most cases, therefore it facilitates traffic and therefore encourages business. In St. Catharines, new businesses can't really open up without limiting access to that area.